



**ORDINARY COUNCIL MEETING OF
THE HUME CITY COUNCIL**

MONDAY, 8 FEBRUARY 2021

7:00PM

**TO BE LIVESTREAMED VIA
WWW.HUME.VIC.GOV.AU**

OUR VISION:

Hume City Council will be recognised as a leader in achieving social, environmental and economic outcomes with a common goal of connecting our proud community and celebrating the diversity of Hume.

An audio recording of this meeting of the Hume City Council will be recorded and published in accordance with Council's Audio Recordings of Council Meetings Policy. The live stream of this meeting will not be recorded or published.

HUME CITY COUNCIL

Notice of an

ORDINARY COUNCIL MEETING OF THE HUME CITY COUNCIL

to be held on Monday, 8 February 2021

at 7:00PM

to be streamed via www.hume.vic.gov.au

To: a: Council	Cr Joseph Haweil Cr Jack Medcraft Cr Jarrod Bell Cr Trevor Dance Cr Chris Hollow Cr Jodi Jackson Cr Naim Kurt Cr Sam Misho Cr Carly Moore Cr Jim Overend Cr Karen Sherry	Mayor Deputy Mayor
b: Officers	Mr Domenic Isola Ms Kristen Cherry Mr Michael Sharp Ms Roslyn Wai Mr Peter Waite Mr Daryl Whitfort	Chief Executive Officer Acting Director Community Services Director Planning and Development Director Communications, Engagement and Advocacy Director Sustainable Infrastructure and Services Director Corporate Services

ORDER OF BUSINESS

1. ACKNOWLEDGEMENT OF THE TRADITIONAL CUSTODIANS OF THIS LAND

"Hume City Council recognises the rich Aboriginal heritage within the municipality and acknowledges the Wurundjeri Woi Wurrung, which includes the Gunung Willam Balluk clan, as the Traditional Custodians of this land.

Council embraces Aboriginal and Torres Strait Islander living cultures as a vital part of Australia's identity and recognises, celebrates and pays respect to the existing family members of the Wurundjeri Woi Wurrung and to Elders past, present and future."

2. PRAYER

Hume City's religious diversity strengthens and enriches community life and supports the well-being of the citizens of Hume City. Hume City Council acknowledges the importance of spiritual life and the leadership offered by the Hume City Inter Faith Leaders Network. In recognition of the religious diversity of residents in Hume City, Council has invited the Inter Faith Leaders Network to take responsibility for the opening prayer at Council meetings. This evening's prayer will be led by Abdelhay Hafiz, Imam, of the Roxburgh Park Worshipping Group."

In the name of God, the most gracious, the most merciful.

All praise is due to God, the lord of the universe.

The beneficent, the merciful.

Lord of the day of judgement.

You alone we worship, and to you alone we turn for help.

Protect us from all diseases and ailments.

Guide us to the right path, the path of those you have blessed.

Amin

3. APOLOGIES**4. DISCLOSURE OF INTEREST**

Councillors' attention is drawn to the provisions of the *Local Government Act 2020* and Council's Governance Rules in relation to the disclosure of conflicts of interests. Councillors are required to disclose any conflict of interest immediately before consideration or discussion of the relevant item. Councillors are then required to leave the Chamber during discussion and not vote on the relevant item.

5. CONDOLENCE MOTIONS**6. INFORMAL MEETINGS OF COUNCILLORS**

In accordance with Council's Governance Rules, the Chief Executive Officer must ensure that a summary of the matters discussed at an informal meeting of Councillors are tabled at the next convenient council meeting and recorded in the minutes of that council meeting.

There are no records of assemblies to report on this section of the Agenda.

1. SUMMARY OF REPORT:

1.1 In accordance with Council's Governance Rules, the Chief Executive Officer must ensure that a summary of the matters discussed at an Informal Meeting of Councillors is tabled at the next convenient Council meeting.

1.2 In keeping with this requirement, this report provides a summary of matters discussed at the following meeting which was conducted under the auspices of Council:

- (a) **Monday 14 December 2020** –Strategy and Policy Briefing Session.

RECOMMENDATION:

THAT Council notes the summary of matters discussed at a meeting conducted under the auspices of Council, being the Strategy and Policy Briefing Session, which was held on Monday 14 December 2020.



Summary of matters discussed at a MEETING CONDUCTED UNDER THE AUSPICES OF COUNCIL

MEETING TITLE: STRATEGY AND POLICY BRIEFING SESSION

Date of Meeting: Monday 14th December 2020

Time of Meeting: 6:33pm

Place of Meeting: Virtual Meeting

The ground or grounds for any matter discussed at this meeting which was considered to be confidential is recorded in this meeting summary.

NOTICE OF MEETING**ORDINARY COUNCIL MEETING OF COUNCIL**

COUNCILLORS PRESENT		OFFICERS PRESENT	
Cr Joseph Haweil (Mayor)	Yes	Mr Domenic Isola	Yes
Cr Jack Medcraft	Yes (6:37pm)	Mr Hector Gaston	Yes
Cr Jarrod Bell	Yes	Ms Roslyn Wai	Yes
Cr Trevor Dance	Yes (left 11:12pm)	Mr Michael Sharp	Yes
Cr Chris Hollow	Yes	Mr Peter Waite	Yes
Cr Jodi Jackson	Yes	Mr Daryl Whitfort	Yes
Cr Naim Kurt	Yes	Ms Megan Kruger	Yes
Cr Sam Misho	Yes	Mr Joel Farrell	Yes
Cr Carly Moore	Yes	Ms Kristen Cherry	Yes
Cr Jim Overend	Yes	Mr David Fricke	Yes
Cr Karen Sherry	Yes	Mr Joel Kimber	Yes
		Mr Bruce Fordham	Yes
		Ms Brooke Watson	Yes
		Ms Peta Farquar	Yes

MATTERS CONSIDERED:

Order	Description	Names of Councillors who disclosed conflict of interest	Did the Councillor leave the meeting?	Confidential ground of grounds
Items for Discussion by exception:				
1	Review of the Fencing of Council Reserves			
2	2021/2022 Community Grants Program			
3	Hume Neighbourhood House Partnership and Funding Agreement	Cr Overend	Yes	
4	Contract 30 20 3122 - Provision of Delivered Meals			Private Commercial Information
5	Sustainability Taskforce Meeting Minutes from 8 September 2020			
6	Register of Councillor Delegates and Representatives (2020-2024)			
8	COVID-19 Community Survey Results			
9	Briefing by Mr Frank McGuire MP, Member for Broadmeadows			
10	COVID Recovery and Reactivation Plan			
11	Draft Community Engagement Policy			
12	Council Planning Weekend			

NOTICE OF MEETING**ORDINARY COUNCIL MEETING OF COUNCIL**

13	Chief Executive Officer's use of Temporary Increase in Delegation 25 October - 7 December 2020 and Temporary Change to the Chief Executive Officer's Delegation 21 December 2020 to 1 February 2021			
15	CEO and Divisional Updates			
Other matters dealt with:				
7	Allocation of Councillor Portfolios			
14	Hume Indoor Cricket Training Centre			
16	Mahoney's Road, Campbellfield			
Items not dealt with:				
	Sunbury West Local Area Traffic Management Study			Not Applicable
	Chief Executive Officer – Application for Annual Leave			Not Applicable
	Somerton Industrial Local Area Traffic Management Study			Not Applicable
	State of Council Assets Report			Not Applicable
	Correspondence received from or sent to Government Ministers or Members of Parliament – November 2020			Not Applicable

Assembly Closed at: 11:30 PM

RECORDED BY:

Daryl Whitfort
Director Corporate Services

7. CONFIRMATION OF COUNCIL MINUTES

Minutes of the Ordinary Council Meeting of 21 December 2020, including Confidential Minutes.

RECOMMENDATION:

THAT the Minutes of the Ordinary Council Meeting of 21 December 2020, including Confidential Minutes, be confirmed.

8. RECEIPT OF COUNCIL AND COMMUNITY COMMITTEE MINUTES AND RECOMMENDATIONS TO COUNCIL TO BE ADOPTED

8.1 Minutes of the Audit and Risk Committee Meetings of the Hume City Council held on 28 August 2020, and 4 December 2020.

RECOMMENDATION:

THAT the Minutes of the Audit and Risk Committee Meetings of the Hume City Council held on 28 August 2020, and 4 December 2020, be noted.

9. PRESENTATION OF AWARDS**9.1 Departing Manager – Jason Summers****10. NOTICES OF MOTION****NOM406 Notice of Motion to Rescind Council Resolution HE119**

I, Cr Trevor Dance, hereby request that pursuant to Council's Governance Rules and Code of Conduct for Councillors that the following motion be included in the Agenda of the next Ordinary Meeting.

RECOMMENDATION:

THAT Council rescind the resolution made on council report HE119 – Hume Indoor Cricket Training Centre at the Hume Council meeting 21 December 2020.

NOM407 Notice of Motion to Rescind Council Resolution GE488

I, Cr Trevor Dance, hereby request that pursuant to Council's Governance Rules and Code of Conduct for Councillors that the following motion be included in the Agenda of the next Ordinary Meeting.

RECOMMENDATION:

THAT Council rescind the resolution made on council report GE488 – Arbiter Report at the Hume Council meeting 21 December 2020.

11. PUBLIC QUESTION TIME**12. OFFICER'S REPORTS**

The Mayor will ask the Councillors and gallery at the commencement of this section, which reports they wish to speak to. These reports will then be discussed in the order they appear on the notice paper. Reports not called will be dealt with in a block resolution at the end.

<u>Item No</u>	<u>Title</u>		<u>Page</u>
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CULTURE AND COMMUNITY

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SUSTAINABILITY AND ENVIRONMENT

SU534	Council Submission to the DELWP Planning for Green Wedges and Agricultural Land Discussion Paper		17
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13. PETITIONS AND JOINT LETTERS**PJL296 PJL296 - Joint Letter - Coopers Hill Drive - Objection to proposed footpath**

A joint letter has been received containing 21 compliant signatures, regarding an objection to the proposed footpath in Coopers Hill Drive, Westmeadows.

RECOMMENDATION:

That the joint letter be received, circulated to Councillors, and the first named signatory of the joint letter be advised that the matter has been referred to the Manager Assets.

14. DEPUTATIONS**15. URGENT BUSINESS****16. DELEGATES REPORTS****17. GENERAL BUSINESS**

18. CONFIDENTIAL MATTERS

The Meeting may be closed to members of the public to consider confidential matters.

RECOMMENDATION:

THAT Council close the meeting to the public pursuant to section 66(2) of the *Local Government Act 2020* to consider the following items:

Report No.	Title	Reason for Confidential
COSU178	Contract - Provision of Landfill Services	(g(ii)) private commercial information, being information provided by a business, commercial or financial undertaking that if released, would unreasonably expose the business, commercial or financial undertaking to disadvantage. The specified grounds apply because the report contains commercial in confidence information.
COSU179	Contract No. 30 20 3169 - Wildwood Road South, Wildwood - Road Rehabilitation and Widening	(g(ii)) private commercial information, being information provided by a business, commercial or financial undertaking that if released, would unreasonably expose the business, commercial or financial undertaking to disadvantage. The specified grounds apply because the report contains commercial in confidence information.
COGE305	Capital Works Report 2020/21 - December 2020 Update	(a) Council business information, being information that would prejudice the Council's position in commercial negotiations if prematurely released.

19. CLOSURE OF MEETING

DOMENIC ISOLA
CHIEF EXECUTIVE OFFICER

4/02/2021

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REPORT NO:	CC124
REPORT TITLE:	Update on the trial removal of overdue library fines
SOURCE:	Mieke Mellars, Coordinator Libraries
DIVISION:	Community Services
FILE NO:	HCC
POLICY:	Social Justice (2014)
STRATEGIC OBJECTIVE:	3.1 Foster socially connected and supported communities.
ATTACHMENTS:	Nil

1. SUMMARY OF REPORT:

This report is to update Council regarding the trial removal of overdue library fines (CC104 / 28 October 2019).

The trial was initiated in response to General Business item KUR059, which requested:

“That Council officers explore the possibility for a trial which would look at removing overdue fines and other fees from our library services, with the intended aim of increasing borrowing rates and library membership uptake at Hume libraries. With a report provided to a future meeting of Council”.

Report CC104 recommended that Council:

- (2.1) Undertake a trial removal of overdue fines across Hume Libraries from 1 January 2020 to 30 June 2021 inclusive.
- (2.4) Considers a report evaluating the progress of the trial in early 2021.

This report responds to recommendation 2.4, as shown above.

2. RECOMMENDATION:

That Council:

- 2.1 notes the results of the first 12 months of the trial removal of library overdue fines.**
- 2.2 endorses the immediate and permanent removal of overdue library fines for Hume Libraries, based on the positive feedback and evaluation of the first 12 months of the trial.**

3. LEGISLATIVE POWERS:

- 3.1 The *Local Government Act (2020)* identifies that “the primary role of a Council is to provide good governance in its municipal district for the benefit and wellbeing of the municipal community”. Furthermore, “priority is to be given to achieving the best outcomes for the municipal community, including future generations” and “Innovation and continuous improvement is to be pursued”.
- 3.2 Council’s libraries provide inclusive, accessible, and welcoming spaces for people to learn, create, socialise, and share ideas and resources. They contribute to lifelong learning and support Council’s strategic objective of a well-educated and employed community.

4. FINANCIAL IMPLICATIONS:

- 4.1 Prior to introducing the trial, Council budgeted approximately \$30,000 per annum for overdue fines.

REPORT NO: CC124 (cont.)

- 4.2 The recommendation to permanently remove library fines will have a \$30,000 impact on the Hume Libraries income budget.

5. ENVIRONMENTAL SUSTAINABILITY CONSIDERATIONS:

- 5.1 The removal of overdue fines for library items encourages borrowing rather than purchasing of items. Sharing of resources is beneficial to our environment.

6. CLIMATE CHANGE ADAPTATION CONSIDERATIONS:

- 6.1 Hume Libraries' diverse programs and activities ensure that the organization delivers services and activities that aim to redress disadvantage and increase participation in community life by building community resilience. It is well documented that communities that are more engaged, connected, and resilient in general are also more resilient to climate change impacts.
- 6.2 Activities and programs delivered by all service points of Hume Libraries include services and programs that engage and educate community members about actions to reduce the incidence of inefficient and costly home energy use, encouraging improved thermal comfort despite increased impacts of heatwaves and rising energy costs.

7. CHARTER OF HUMAN RIGHTS APPLICATION:

- 7.1 Hume Libraries is committed to upholding the rights of the Hume community.
- 7.2 The provision of universal access to information, delivery of early literacy and lifelong learning programs, ensuring inclusivity of all communities and provision of safe spaces for all community members demonstrate how Council's commitment to the application of the Charter of Human Rights and Responsibilities Act Vic (2006) is put into action.

8. COMMUNITY CONSULTATION:

- 8.1 Community consultation regarding the trial occurred organically via Hume Libraries' Facebook and Instagram social media platforms.

9. DISCUSSION:

- 9.1 At its meeting on 28 October 2019, Council supported the recommendation to trial the removal of overdue fines at Hume Libraries from 1 January 2020 to 30 June 2021.
- 9.2 Library overdue fines tend to discourage people from lower socio-economic backgrounds to use the library due to financial hardship. These fines therefore undermine the core social justice principles of libraries, where access to library services and materials are freely available to all.
- 9.3 Within report CC104 on 28 October 2019 four monitoring and evaluation measures were identified to measure the impact of the trial. These were:
 - (a) Customer feedback (including an anticipated reduction in the number of complaints relating to outstanding overdue items and fines).
 - (b) Library memberships, loans, renewals and collection turnover (anticipated to increase).
 - (c) Number of items that are never returned (anticipated to decrease).
 - (d) Number of lapsed members that re-commence using the service.

REPORT NO: CC124 (cont.)

9.4 The table below outlines the collected data in relation to each measure:

Evaluation measure	2019	2020	Comments
Customer feedback (including an anticipated reduction in the number of complaints relating to outstanding overdue items and fines).	N/A	N/A	<p>Customer feedback on the trial has been positive.</p> <ul style="list-style-type: none"> •Initial Facebook and Instagram posts announcing the removal of overdue fines elicited positive reactions from over 500 people and 110 positive comments. •63 people shared the news with their networks. <p>Early concerns were voiced about waiting times for reserved library items. These were mitigated by purchasing additional copies of items in high demand. The resulting wait times are shorter than before the trial began.</p> <p>Staff have not needed to have any difficult conversations with patrons around outstanding overdue items and resulting fines.</p>
Library membership numbers (expected to increase)	1,842	3,149	+71%
Loans and renewals of library items (expected to increase)	134,775	149,836	+11%
Collection turnover expected to increase (expressed as percentage of items on loan)	35%	42%	+20%
Number of items never returned (expected to decrease)	N/A	N/A	Data was invalid as loan periods were extended from March to November 2020 due to COVID
Return of lapsed customers	Unknown	258	

9.5 The following conclusions can be drawn from the data:

- All evaluation measures followed the expected trend.
- Library membership increased by a higher percentage than expected. This figure was also influenced by the opening of the Hume Global Learning Centre – Sunbury in December 2019.
- It is to be noted that due to the interruption of the COVID-19 pandemic, only short data periods were able to be compared. However, significant increases in library memberships, loans, renewals and collection turnover indicate a positive trend in community behavioral responses following the removal of overdue fines.

9.6 Based on the results of the first 12 months of this trial, it is recommended that Council permanently remove the application of overdue fines for Hume Libraries.

REPORT NO: CC124 (cont.)

10. CONCLUSION:

- 10.1 The Hume community responded positively to the removal of overdue fines for library items. Community feedback was overwhelmingly positive, and analysis of the data collected shows a trend in line with expectations.
- 10.2 In line with the Social Justice Charter 2014, the permanent removal of overdue library fines is an important measure to improve equitable access to the services and opportunities offered by Hume Libraries and thereby supporting the learning, social, cultural and recreational aspirations of all residents.

REPORT NO:	CC125
REPORT TITLE:	Dulap Wilim – Sunbury Shared Community Hub
SOURCE:	Cathy Marshall, Executive Officer Community Services
DIVISION:	Community Services
FILE NO:	HCC
POLICY:	-
STRATEGIC OBJECTIVE:	3.1 Foster socially connected and supported communities.
ATTACHMENTS:	Nil

1. SUMMARY OF REPORT:

- 1.1 This report provides Council with an update on the completion of the new Dulap Wilim Sunbury Hub and to seek approval to:
 - 1.1.1 be an active partner and member of the governance committee for the Hub, overseeing the effective operation of the Centre.
 - 1.1.2 allocate \$30,000 annually for the initial first three years of the Centres operation to support the activation and operation of the new Centre.

2. RECOMMENDATION:

That Council

- 2.1 **refers \$30,000 annually (for an initial period of three years commencing in 2021/22) to contribute to the operating costs of the new Dulap Wilim Hub Sunbury.**
- 2.2 **develops a suitable funding agreement to be jointly signed off between Council and Sunbury Heights Primary School.**
- 2.3 **nominates a Jackson's Ward Councillor to participate on the newly formed Dulap Wilim Hub governance committee.**
- 2.4 **appoints the Director Community Services (or nominee) to participate on the Dulap Wilim Hub governance committee.**

3. LEGISLATIVE POWERS:

In accordance with the Victorian Local Government Act 2020, a function of Council is to plan and deliver services to the community in accordance with service performance principals. This proposal seeks to improve service delivery, and ensure services are responsive to the diverse needs of the community.

4. FINANCIAL IMPLICATIONS:

- 4.1 The development of Dulap Wilim Hub Sunbury was fully funded by the Victorian State Government at a total cost of \$7.2M.
- 4.2 This report seeks Council's approval to refer a total of \$30,000 per annum for an initial three-year period representing Council's share of the administration and overall costs associated the operation of Dulap Wilim Hub in Sunbury. The funding will be used specifically to support operating costs including staffing, power, water, electricity, cleaning, consumables, security, ground and building maintenance.

REPORT NO: CC125 (cont.)

5. ENVIRONMENTAL SUSTAINABILITY CONSIDERATIONS:

There are no environmental sustainability implications in respect to this report.

6. CLIMATE CHANGE ADAPTATION CONSIDERATIONS:

There are no direct implications for climate change adaptation as a result of this report.

7. CHARTER OF HUMAN RIGHTS APPLICATION:

Dulap Wilim Hub Sunbury will support improved access to a wide range of community services and programs thereby helping to strengthen and foster socially connected and supported communities.

8. COMMUNITY CONSULTATION

8.1 There has been no direct community consultation in relation to this matter.

9. BACKGROUND – DULAP WILIM HUB SUNBURY

9.1 Council at its meeting on 10th July 2017 (Report No.CC051) agreed to partner with Sunbury Downs College (SDSC) and Sunbury Heights Primary School (SHPS) in the development of a shared-use community facility on land owned by the Victorian Government – (co-located on land of both schools) to service the needs of the current and expanding Sunbury West community.

9.2 At the time a \$4.2M funding submission was submitted to the 2016/17 Shared Facility Fund (SFF) to deliver a shared community facility which would include kitchens for classroom teaching and community use, multi-purpose community rooms that will function as classrooms, meeting rooms, and training rooms, a playgroup room, and consulting suites for both school and community use.

9.3 In August 2017, Council together with both school representatives (school principals) signed a Common Joint Use Proposal (CJUP). The purpose of the CJUP was to provide key information about the proposed joint use agreement and to outline the basis on which the parties propose to enter into a legally binding joint use agreement with the Minister of Education.

9.4 In November 2017, Council was informed that the funding proposal had been successful, and as part of the funding advice it was proposed the Community Joint Use Agreement (CJUA) would not be entered into due to the funding model agreed to by all parties in the CJUP. In the absence of a formal agreement, Hume City advised State Government that it wished to uphold the agreed position outlined in the CJUP, which acknowledged that Council will contribute to the cost of the facility and this would be either through an annual allocation towards the running of the facility (e.g. maintenance, utilities, cleaning, security) or through booking fees.

9.5 In March 2018, the State Government announced a further \$3m funding allocation for the construction of a double unit Preschool to be integrated into the community hub development, resulting in a total project budget of \$7.2m.

9.6 Following this additional funding announcement, Sunbury Heights Primary School wrote to Council inviting Council to operate and manage the preschool service on their behalf.

9.7 Council at its meeting on 25th June 2018 resolved to operate the SHPS, preschool service upon completion of the centre, and agreed to enter into an agreement with the school for the delivery of the preschool service.

9.8 Dulap Wilim Hub Sunbury was completed in August 2020 however due to Covid 19 related restrictions the centre has been unable to open its doors to the public. The new multipurpose facility has an internal footprint of approximately 1400m² and includes the following:

REPORT NO: CC125 (cont.)

- 9.8.1 Shared Community spaces: comprising a dedicated playroom and outdoor space, three meeting rooms, a flexible multipurpose space that can be configured into 1, 2 or 4 rooms, a community kitchen, toilets and informal meeting spaces. This shared community space will be managed and operated by Sunbury Heights Primary school (SHPS) with programs commencing in 2021.
- 9.8.2 Two room Preschool, with the service to be delivered from 2021, by Hume City Council (HCC), under a lease agreement between the Department of Education & Training (DET) and Hume City Council. The term of the Lease is up to 26 years.
- 9.8.3 Two room teaching kitchens, administered by Sunbury Downs College (SDC) to deliver the secondary school, Home Economics program. This space forms part of the school's entitled teaching space, however, was envisaged as having some potential for community use out of school hours. The teaching kitchens will be managed by the Sunbury Downs College.
- 9.9 Whilst shared school – community facilities are not uncommon, Dulap Wilim Hub is distinguished by its multi- purpose elements and the breadth of uses that can be accommodated in the Hub. The development of this facility has been made possible by the Victorian State Government who have contributed a total of \$7.2M towards the development of this project providing the local community of Sunbury with access to much needed community infrastructure to support both current and future community needs and aspirations.

10. DISCUSSION:

- 10.1 Since 2016, Hume City Council has continued to be an active partner in the planning and development of this new facility, and has over the past 3 years worked alongside the Victorian School Building Authority and the two local schools to support the successful delivery of this project. Whilst Council was not required to contribute any capital funding towards the delivery of this building it has remained active partner in all stages of the planning, ensuring that the building design met contemporary service standards and responded to both current and future community needs and aspirations.
- 10.2 The three partner organisations, Sunbury Heights Primary School, Sunbury Downs Secondary School and Hume City Council have all indicated an ongoing commitment to supporting the activation of the hub including participation in a governance committee to promote the Hub's vision and purpose. A Memorandum of Understanding is currently being drafted to capture the spirit of cooperation, the commitment to the hub vision and the operational responsibilities that each partner has accepted.
- 10.3 Currently, the three partners each have discrete areas of the Hub which they will operationalize:
 - 10.3.1 Sunbury Downs College will occupy the teaching kitchens for the school programs and will meet the associated running costs and ensure compliance with safety standards. Community use of these teaching kitchens to be determined by the College.
 - 10.3.2 Hume City Council will occupy and run the preschool program and meet the associated operational costs under a lease agreement with DET which clarifies responsibilities and liabilities.
 - 10.3.3 Sunbury Heights Primary School will oversee the balance of the facilities including accommodation for the Community Hubs Australia (CHA) program which has been run by the school for 10+years. The intent of the shared community facilities is to offer a diverse program of education, health and community services that engage the local community and build community strength. SPHS will manage and operate the community facilities.

REPORT NO: CC125 (cont.)

10.4 Governance Committee

- 10.4.1 Council has been invited by the SHPS to nominate an elected Councillor from the Jacksons Creek Ward and a senior Council officer to participate in the Governance Group.
- 10.4.2 It is proposed that the governance and management of the Hub comprise of a seven (7) member Governance group with membership drawn from Sunbury Heights Primary School, Sunbury Downs College, Hume City Council, each with 2 members and 1 independent community member, who would Chair the Committee, supported by Sunbury Heights Primary School Principal who would convene the Group.
- 10.4.3 The key roles of the Governance Group include:
 - (i) To champion the Hub Vision.
 - (ii) To review operational reports including program delivery and financial reports.
 - (iii) To provide an annual report to the community.
- 10.4.4 Once established, it is anticipated that the Governance Group would meet 3 times per year.
- 10.4.5 Council's participation on the governance committee will provide valuable support and expertise and will help to ensure the needs of the local community are well represented and reflected in through the programs and activities offered from the hub.

10.5 Operational Funding

- 10.5.1 The SHPS have requested that Council provide allocation of \$30,000 per annum for a three-year period 2021 to 2023, to support the Dulap Wilim Hub during the period of initial establishment.
- 10.5.2 An annual operating budget has been prepared by the SHPS as part of the operational planning for the new Centre. The budget has been drawn upon available income (including a conservative estimate of income from hire fees) and expenditure associated with the operations of the current Community Hub program operated at the school, including revenue and expenditure and other school operating costs.
- 10.5.3 Council is reminded that the original grant application back in 2016/17 was built upon an operating model that would generate revenue to meet the Hub's annual operating costs and that on an annual basis Council will need to contribute towards the cost of the facility, acknowledging this could be either through an annual allocation or via booking fees.
- 10.5.4 During the initial establishment period (3 years) of the Hub, allowing time for promotion, securing regular users and potentially tenants, the underpinning of the Hub's budget via an annual allocation by Council will provide guaranteed upfront income / revenue during the establishment phase to underpin and support with confidence the operation of the Centre.

11. CONCLUSION:

- 11.1 Dulap Wilim (*meaning Proud Place*) represents a new approach to shared school/community facilities and presents a unique opportunity to demonstrate the success of Council working closely with local schools to achieve a facility that brings together students, parents and the neighbourhood community. This multi-purpose facility provides children, teenagers, adults, parents and seniors; to meet, build relationships and strengthen social capital, essential for resilient communities and to harness local talents and create opportunities for all.

REPORT NO:	SU534
REPORT TITLE:	Council Submission to the DELWP Planning for Green Wedges and Agricultural Land Discussion Paper
SOURCE:	David Hajzler, Strategic Land Use Planner
DIVISION:	Planning and Development
FILE NO:	HCC65/65
POLICY:	-
STRATEGIC OBJECTIVE:	4.1 Facilitate appropriate urban development while protecting and enhancing the City's environment, natural heritage and rural spaces.
ATTACHMENTS:	<ol style="list-style-type: none">1. <i>Proposed Council Submission</i>2. <i>Melbourne's green wedges and Discussion Paper project scope</i>

1. SUMMARY OF REPORT:

The Department of Environment, Land, Water and Planning (DELWP) released the *Planning for Melbourne's Green Wedges and Agricultural Land Discussion Paper* for consultation in November 2020. The Discussion Paper seeks to manage and protect green wedge and agricultural land around Melbourne through legislative and planning scheme changes. Officers consider that the Discussion Paper is limited in its capacity to meaningfully address the challenges experienced by Hume's green wedge community due to its narrow focus on agriculture and a lack of any significant policy interventions or actions other than relatively minor changes to town planning controls. It is recommended that Council puts forward a submission to the Discussion Paper that highlights the limitations of DELWP's approach to facilitating a viable green wedge in Hume and provides recommendations for other opportunities and practical support measures that the State Government in line with the draft Hume Rural Strategy.

2. RECOMMENDATION:

That Council resolves to forward a submission to the Planning for Melbourne's Green Wedge and Agricultural Land Discussion Paper, outlining the matters discussed in this report, in accordance with the draft submission included as Attachment 1.

3. LEGISLATIVE POWERS:

The Discussion Paper proposes changes to the *Planning and Environment Act 1987*, should these changes be pursued they will be enacted by DELWP.

4. FINANCIAL IMPLICATIONS:

There are no direct financial considerations to Council in lodging a submission to the Discussion Paper.

5. ENVIRONMENTAL SUSTAINABILITY CONSIDERATIONS:

The draft submission to the Discussion Paper in Attachment 1 recommends actions for the State Government to undertake that if adopted would result in better environmental sustainability outcomes in green wedge areas.

6. CLIMATE CHANGE ADAPTATION CONSIDERATIONS:

The draft submission to the Discussion Paper in Attachment 1 recommends actions for the State Government to undertake that if adopted would result in better climate change outcomes in green wedge areas.

REPORT NO: SU534 (cont.)

7. CHARTER OF HUMAN RIGHTS APPLICATION:

There are no human rights implications to Council in lodging a submission to the Discussion Paper.

8. COMMUNITY CONSULTATION:

- 8.1 DELWP released the *Planning for Melbourne's Green Wedges and Agricultural Land Discussion Paper* for Council and other government stakeholder consultation on 3 June 2020.
- 8.2 DELWP initially planned to release the Discussion Paper publicly in August 2020, however due to the introduction of Stage 4 COVID-19 restrictions the release of the Paper was postponed until November 2020. The public consultation for the Discussion Paper will now close on 5 February 2021.
- 8.3 The development of the draft Hume Rural Strategy March 2020 that has informed the draft submission in Attachment 1 was preceded by two rounds of community consultation. Currently, the community consultation for the draft Strategy itself is still open. The draft submission at Attachment 1 reflects the challenges highlighted to officers during these consultations that are confronting Hume's rural community.

9. DISCUSSION:

Background

- 9.1 Metropolitan Melbourne has large areas of non-urban land that fall outside the urban growth boundary, these are known as Melbourne's green wedges – see Attachment 2.
- 9.2 Melbourne's 12 green wedges each exist for different reasons that are based on their specific contexts and the unique roles they play for metropolitan Melbourne. Some of the common roles that each green wedge might play that are highlighted in the purpose of the Green Wedge Zone and Plan Melbourne 2017-2050 state planning policy include:
 - 9.2.1 The provision of rural buffers between the urban spread of Melbourne.
 - 9.2.2 The protection of valuable agricultural land.
 - 9.2.3 The protection of significant state infrastructure assets.
 - 9.2.4 The protection of land for environmental, biodiversity conservation, tourism and landscape features.
- 9.3 Hume's green wedge comprises approximately half the municipality. It separates the Hume Growth Corridor and Sunbury Township urban areas, and also separates Sunbury from outer areas of Melbourne within Melton City Council and townships to the north in Macedon Ranges Shire.
- 9.4 While Hume's green wedge fulfills many of the roles outlined above, its existence is owed primarily to its unique role in protecting the operations of Melbourne Airport by limiting the scale and density of noise sensitive uses and development, notably housing. Hume's green wedge provides a rural buffer around the airport that allows flights to be operated through the day and night without a curfew, such as those imposed on every other major airport on Australia's eastern coast.
- 9.5 Compared with other green wedges, such as Mornington Peninsula and the Yarra Ranges, Hume's green wedge has relatively low agricultural viability. This creates an enormous challenge for Hume's green wedge community, as there is a prevailing perception is that all green wedge land is protected for agricultural land uses. Given this perception, when agriculture cannot establish in Hume or it struggles to remain financially viable, many landowners logically argue that Hume's green wedge cannot fulfill its purpose as a green wedge and should therefore be rezoned or subdivided to enable more viable urban or residential land uses.

REPORT NO: SU534 (cont.)

- 9.6 The difficulties for agriculture in Hume combine with the challenges of effective and sustainable land management and the impacts of land speculation and climate change. These issues further compound the perception that some green wedge areas are not viable for any future use or purpose other than residential housing.
- 9.7 There are no single or easy solutions to address the challenges confronting Hume's green wedge and the struggles of the community who live and work there.
- 9.8 The draft Hume Rural Strategy seeks to begin to address some of them by offering the green wedge community a shared vision for the rural areas to strive towards in partnership with Council and attempt to foster a viable green wedge. Reflective of this holistic and partnership approach, the actions of the draft Hume Rural Strategy are encompassed by three themes:
 - 9.8.1 Certainty – on the future planning for the rural areas, particularly future urban development and subdivision in order to assist the rural community to make investments in their land with a greater level of confidence.
 - 9.8.2 Support – to help enable landowners to continue to pursue current activities, diversify into new activities, maximise opportunities associated with a growing urban population within the UGB, and to effectively manage land.
 - 9.8.3 Engagement – to inform the rural community of potential future changes and to grow community connectedness and capacity to realise future opportunities and manage challenges.
- 9.9 Council is limited in its capacity to act in many respects due to its inability to make changes to green wedge planning controls and the significant investment in infrastructure, compliance and support programs that are necessary to create a viable green wedge.
- 9.10 Officers believe that to achieve the outcomes of the draft Rural Strategy and respond to the challenges confronting Hume's green wedge community requires a partnership approach with the State Government and for the State Government to particularly address these challenges where Council cannot.

The DELWP Discussion Paper

- 9.11 The project has its genesis in the State Labor election commitment to protect the right to farm and the emphasis in the Discussion Paper remains very strongly on protecting agricultural land. It also responds to actions in Plan Melbourne associated with protecting agricultural land, reviewing green wedge planning provisions and Green Wedge Management Plans.
- 9.12 The overarching aim of the Discussion Paper is to protect Melbourne's green wedge and peri urban areas and manage land use to support their continued long term agricultural land use. In order to achieve this the Discussion Paper proposes increased protection for agricultural land uses in green wedge and peri-urban areas through changes to legislation and planning controls.
- 9.13 Specifically, some of the directions and options proposed in the Discussion Paper include:
 - 9.13.1 Introducing legislative changes to strengthen the protection of green wedges in the *Planning and Environment Act 1987*, including a requirement for councils to prepare Green Wedge Management Plans (i.e. the draft Rural Strategy) and for them to require Ministerial approval.
 - 9.13.2 Introducing State backed regional policy for green wedges to reflect the unique qualities of each green wedge.
 - 9.13.3 Introducing changes to the State Planning Policy Framework to encourage agriculture and supporting infrastructure and to recognise the economic contribution of agricultural land.

REPORT NO: SU534 (cont.)

- 9.13.4 Introducing planning guidance and potential transition areas on the urban fringe to better manage the interface between urban and rural areas.
- 9.13.5 Introducing agent of change principles into legislation and planning policy that require development proposals to consider and minimise their impact on agriculture.
- 9.13.6 Amending the Green Wedge Zone controls to require that schools and places of worship are located adjoining a Road Zone (i.e. a major arterial road) and the Urban Growth Boundary.
- 9.13.7 Introducing new design requirements to manage the impact of new development on the landscape and visual qualities of the green wedge.
- 9.14 Officers consider that the Discussion Paper represents a very significant missed opportunity to address the overarching challenges that are confronting Hume's green wedge, outlined above, in two key respects:
 - 9.14.1 Its focus on agriculture at the expense of other appropriate land uses that could achieve a more viable green wedge.
 - 9.14.2 Its lack of consideration of much needed complimentary practical non-town planning related tools to support viable green wedges.

A detrimental focus on agriculture

- 9.15 The Discussion Paper's generalised need to protect agricultural land uses in all green wedge areas partially stems from the scope of DELWP's review encompassing all land in Melbourne's green wedges and all non-urban land to a radius area of 100km from Melbourne – see attachment 2.
- 9.16 The geographic scope of the Discussion Paper is expansive and diverse. It includes highly viable green wedge areas like the Mornington Peninsula and Yarra Valley where there are concerns of the overdevelopment of tourism facilities around areas of intensive commercial agriculture and animal husbandry. These challenges are vastly different from those experienced in Hume's green wedge.
- 9.17 The great diversity of the areas included in the Discussion Paper is significant as the proposed town planning and legislative changes are generally sweeping and uniform with little consideration of the spatial variation. While the diversity of Melbourne's green wedges are acknowledged in the Discussion Paper, any specific challenges or unique features are proposed to be addressed only through regional policy and Green Wedge Management Plans.
- 9.18 The Discussion Paper's focus on primarily protecting green wedge land for agricultural purposes results in perpetuating the unrealistic and reductive view that all green wedge land exists solely for its agricultural viability. Thereby fueling the pervasive argument that if green wedge land is not agriculturally viable it has no value as a green wedge and should become urban.
- 9.19 The Discussion Paper's emphasis on agriculture as a land use comes at the near exclusion of consideration that any other land uses might be appropriate in a green wedge. Indeed, much of the discussion of non-agricultural land uses is focused on either reducing their impact on agriculture, minor tweaking of their definitions in the planning scheme or further restricting their establishment within the Green Wedge Zone. This is of particular concern for tourism and recreational land uses, such as restaurants, function centres and accommodation, that officers believe should be encouraged within Hume's green wedge as a viable alternative to agriculture.

REPORT NO: SU534 (cont.)

The lack of practical support actions

- 9.20 The Discussion Paper aligns with Council's draft Rural Strategy in recognising the interconnected challenges of decreasing agricultural viability due to climate change and the impacts of increasing land speculation and land management difficulties that are occurring in many green wedges including Hume.
- 9.21 To address these challenges the Discussion Paper states very clearly that changes to the urban growth boundary and changes to minimum lot sizes are not part of the scope of the project. This seeks to dispel any prospect of urban development or subdivision to provide a more certain planning arena for green wedge landowners, consistent with Council's draft Rural Strategy.
- 9.22 Unfortunately, other than acknowledging these issues and the State Government's commitment to relative certainty on planning controls around the urban growth boundary and subdivision sizes, the Discussion Paper is deliberately and narrowly focused on legislative and planning solutions to address these social, economic and environmental issues.
- 9.23 The scope of the Discussion Paper does not consider or propose any actions to support landowners in green wedge areas through education and business programs, grants or infrastructure investment, when it is such types of State Government support that is critically needed to achieve a viable green wedge in partnership with the community.

Proposed Council's submission – key elements

- 9.24 The draft submission to the Discussion Paper in Attachment 1 reflects the position proposed in the draft Hume Rural Strategy and the challenges that the green wedge community has consistently highlighted to Council that informed its development.
- 9.25 While supportive of the vision of the Discussion Paper and the aspiration for certainty on issues such as the urban growth boundary, the draft submission is critical of DELWP's reductive focus on agriculture and its limited scope of town planning solutions. The draft submission expresses that the Discussion Paper represents a significant missed opportunity to review, understand and meaningfully engage with the complex challenges confronting Melbourne's and Hume's green wedge.
- 9.26 The draft submission highlights that a key role of Hume's green wedge is to support the operation of Melbourne Airport rather than protect highly agricultural land. The submission proposes that greater consideration should be given to what are appropriate land uses for Hume's green wedge that might be viable and complementary to this role, such as tourism and recreational land uses, rather than simply agriculture.
- 9.27 The draft submission is critical of the Discussion Paper's narrow focus on town planning solutions as an approach that is limited and not particularly helpful for Hume's green wedge community. Whilst town planning has a crucial role in managing change in green wedges, it needs to be complimented by practical support measures for landowners to facilitate and enable the change that it is trying to achieve.
- 9.28 The draft Hume Rural Strategy identifies that in order to begin to move towards a viable green wedge there is a need to provide certainty, support and engagement with the rural community. The Discussion Paper suggests that the State Government sees its role to be limited to providing certainty, with any support and engagement programs solely in the domain of councils.
- 9.29 The draft submission states that Council does not consider this to be acceptable and is insufficient to realise a viable future for Hume's green wedge. It argues for the need of a holistic, integrated and partnership approach between State and Local Government with the rural community to find a viable long-term future for the green wedge.

REPORT NO: SU534 (cont.)

- 9.30 Such an approach is necessary because of the vital role of Hume's green wedge in protecting the operation of Melbourne Airport. Unless a realistic long-term future is facilitated for Hume's Green Wedge, pressure will continue to mount for the land under the flight paths to be subdivided and urbanised.
- 9.31 The draft submission expresses that DELWP needs to support the rural community in Hume and for the State Government to help them facilitate change. Specifically, the draft submission includes the following comments to DELWP:
- 9.31.1 That DELWP must acknowledge that not all green wedges serve a primarily agricultural role and they need to be considered and planned for accordingly.
 - 9.31.2 That the unique features and role of Hume's green wedge cannot be adequately addressed through the proposed regional policy.
 - 9.31.3 That Council supports greater guidance on schools and places of worship in the green wedge, but raises concerns that limiting them to the immediate edge of the urban growth boundary is potentially too restrictive. There is the need to support these restrictions with measures to make it more financially viable and convenient for these facilities to locate within the urban growth boundary.
 - 9.31.4 There is the need for councils to have flexibility to vary maximum patron and capacity limits to promote and facilitate key tourism and recreational uses in the Green Wedge Zone.
 - 9.31.5 That the State Government must commit to meaningful and ongoing engagement with rural landowners throughout the life of infrastructure projects.
- 9.32 The submission also recommends additional support actions that the State Government should undertake, including:
- 9.32.1 Actively facilitating secure access to recycled water to green wedge areas.
 - 9.32.2 Education programs and business support for farmers to transition to new forms of farming and complementary land uses.
 - 9.32.3 Assistance programs to landowners to investigate and switch to alternative crops or agribusinesses.
 - 9.32.4 Advice for businesses on how to access new and growing markets for their produce.
 - 9.32.5 Support and promote tourism through a coordinated regional approach and ensure that all green wedge areas are within regional tourism areas.
 - 9.32.6 Better weed compliance programs.

10. CONCLUSION:

DELWP has released the *Planning for Melbourne's Green Wedges and Agricultural Land Discussion Paper* for community consultation. The Discussion Paper proposes legislative and planning scheme changes to manage and protect green wedge areas across Melbourne. While the Discussion Paper's emphasis on planning certainty is useful, it is ultimately limited by its narrow focus on agriculture and proposes only town planning responses to address the many and complex challenges that are confronting green wedge areas. The proposed solutions come at the expense of other much needed opportunities and solutions that could support Hume's rural community. It is proposed that Council puts forward a submission to the Discussion Paper that calls for DELWP to take a more holistic approach to the project and recommends alternative actions that could foster a more viable green wedge in Hume.



HUME CITY COUNCIL
**SUBMISSION TO
THE PLANNING
FOR MELBOURNE'S
GREEN WEDGES AND
AGRICULTURAL LAND
CONSULTATION PAPER**

February 2021

www.hume.vic.gov.au



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Introduction

Council welcomes the opportunity to comment on the *Planning for Melbourne's Green Wedges and Agricultural Land Consultation Paper* (the Consultation Paper). Hume City Council has a strong interest in protecting Hume's green wedge and supporting the community who live and work there.

To do this Council released a draft Rural Strategy that offers the green wedge community a shared vision for the rural areas to strive towards in partnership with Council and to ensure that Hume's green wedge can continue to fulfil its various roles, particularly the ongoing protection of Melbourne Airport.

To achieve the outcomes of the Rural Strategy requires a partnership approach with the State Government and it is in this context that Council provides the comments in this submission to the Consultation Paper.

While Council supports the intention of the Consultation Paper to provide planning certainty for Melbourne's green wedges and generally supports the State Government's vision, Council is concerned that most of the options proposed in the Paper are not relevant for Hume's green wedge and will continue to inhibit its viability.

Council believes that the Consultation Paper does not consider or address the challenges that are confronting a green wedge with low agricultural viability, such as experienced in Hume's green wedge. The function of Hume's green wedge and the key challenges that its community are facing are outlined in Part A of this submission.

Part B of this submission highlights what Council has identified as the key deficits in the Consultation Paper. Namely: a narrow focus on agriculture as the primary land use for all green wedge land; the absence of any non-town planning related tools to support a viable green wedge; and the urgent need to facilitate the provision of recycled water to the green wedge.

Part C of this submission responds to the options proposed in the Consultation Paper and provides recommendations for additional measures that must be pursued by the State Government if the vision described in the Consultation Paper might be achieved and Hume's green wedge can begin to be seen as viable.

Part A – Hume’s green wedge and its community

Hume’s green wedge spans approximately 250 square kilometres, comprising half the municipality. Council recognises the need to provide clear guidance and certainty in the planning future for green wedge areas as they serve an important role both locally, for metropolitan Melbourne and the State.

The majority of Hume’s green wedge is affected by the Melbourne Airport Environs Overlay, reflecting its vital role in protecting the curfew free status of Melbourne Airport. This unique factor necessitates a different consideration of the Hume green wedge – see Box 1.

This imperative also allows Hume’s green wedge to achieve its other important functions, namely the:

- Maintenance of a non-urban/rural break around Sunbury to keep it separate from the urban development of Melbourne and the regional townships to the north and west.
- Protection of areas with significant environmental and biodiversity assets, in accordance with State Government strategies such as Biodiversity 2037 and the Healthy Waterways Strategy.
- Protection of areas identified with potential future mineral resources and extractive industries, waste and resource facilities and future State infrastructure projects.
- Protection of areas with valued landscape and open space features.
- Protection of areas of aboriginal and post-European cultural heritage.

Whilst agriculture is a land use occurring in parts of the Hume green wedge and should be encouraged, it is critical to recognise that a greater variety of land uses are required to viably sustain these roles now and into the future. As will be outlined in this submission, achieving this mix is very challenging and requires a range of interventions.

In 2016, Council began the preparation of a rural strategy to guide the future of Hume’s green wedge areas for the next 20 years and to offer a shared vision for Council and the community to strive towards. This followed previous attempts to commence a strategy for the rural areas following Melbourne 2030, which were abandoned due to the changes to the urban growth boundary that created continuing uncertainty.

In March 2020, Council began community consultation on the draft Rural Strategy and due to the current COVID-19 restrictions the consultation is still ongoing.

The draft Rural Strategy was developed after numerous background studies were conducted and two rounds of community consultation. This work and the ongoing consultation with Hume’s green wedge community have given Council a deep understanding of the challenges that these areas have been confronting for many decades.

Box 1 - Hume's green wedge – Protection of Melbourne Airport

Rather than protecting agricultural land, the primary basis for reserving the majority of land in Hume as green wedge is to protect the curfew free operation of Melbourne Airport as a State-significant infrastructure asset. The need to protect Melbourne Airport from inappropriate development is acknowledged in State Government Planning Policy Framework at Clause 18.04-1R and Direction 3.4 of Plan Melbourne, as well as the 2018 Melbourne Airport Master Plan. This is an extremely important and unique role that distinguishes Hume's green wedge from other green wedges across Melbourne.

Council is committed to protecting the operation of Melbourne Airport, the integrity of Hume's green wedge, and the myriad of roles it plays as a green wedge. Yet it must be acknowledged that the landowners within Hume's green wedge who are protecting assets for the benefit of all Victorians are burdened in their capacity to respond to the challenges that confront green wedge land (as highlighted in Figure 1).

These residents need to be supported in order to enable the viable and ongoing use of their land so that the pressure to relax the burden of the planning controls for the protection of Melbourne Airport does not grow.

Ensuring the ongoing viability of Hume's green wedge is therefore of fundamental strategic significance to the curfew free status of Melbourne Airport and in turn the State. Hume's green wedge requires special consideration and tailored solutions rather than a one size fits all approach.

The challenges of Hume's green wedge

While some issues are localised to certain areas of Hume's green wedge and can be addressed specifically by Council, we have consistently heard from the community that there is a set of interconnected challenges that are felt by landowners across the green wedge. See Box 2 below for the key findings of the 2018 consultation to Council's *Emerging Issues and Options Paper* for the rural areas.

Figure 1 demonstrates the interconnected nature of these challenges that reinforce a cycle of land speculation and land banking that makes agriculture and land management increasingly difficult, these stem primarily from:

- limited and declining viability for agricultural uses on many properties, and
- a recent history of continued expansion of the urban growth boundary.

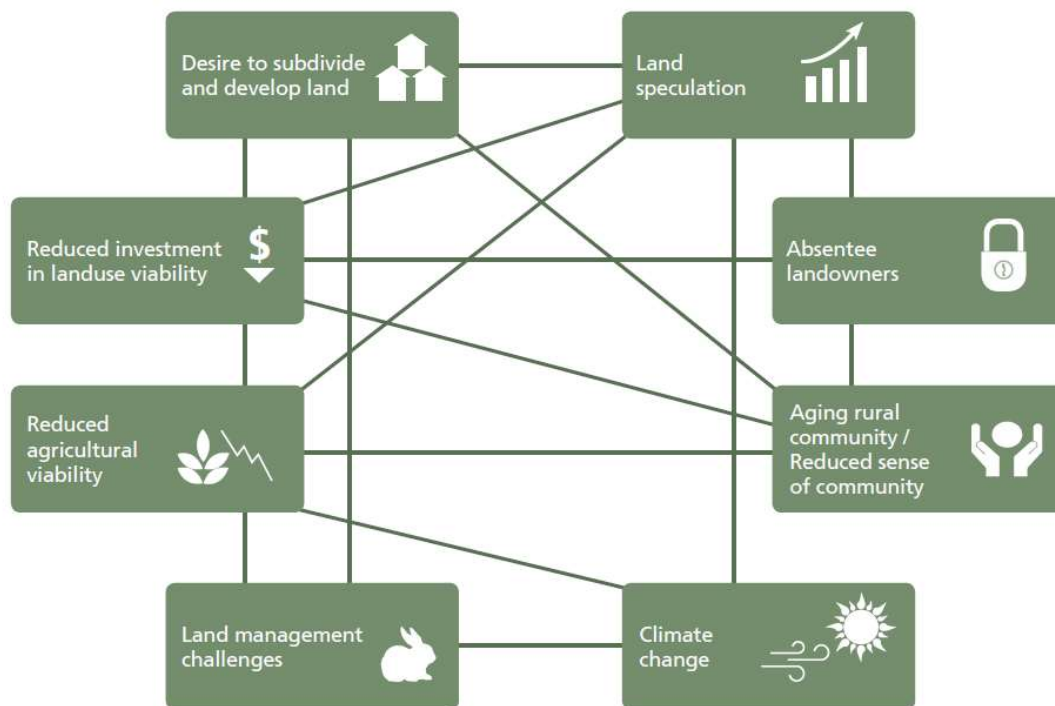


Figure 1 Interconnected challenges in Hume's green wedge

The green wedge community has been vocal for many years about the difficulties associated with agriculture in Hume. Hume's green wedge sits in the rain shadow of the Macedon Ranges and experiences relatively lower levels of rainfall compared with other green wedges. There is currently no recycled water infrastructure in place and it is cost prohibitive for farmers to construct such infrastructure. This lack of water is combined with relatively poor quality rocky soil which has seen Hume's green wedge areas experience declining viability since the 1970s¹.

The Keilor Market Gardens in Hume's south was the exception to this trend, however extensive periods of drought in the 2000s and limited water allocation for irrigation from the Maribyrnong River has seen agriculture in this area also become challenging.

This assessment of Hume's agricultural viability by the community is supported by the technical reports that accompanied the 2019 consultation for the *Protecting Melbourne's Strategic Agricultural Land* project. These materials identified that the majority of Hume's green wedge areas consist of land which has a moderate capability for agriculture at best, with vast swaths that have low capability and are highly constrained. As such, that project did not identify any land in Hume's green wedge that would be considered strategic agricultural land.

Given the limited capacity for agriculture, many landowners are dependant on income from non-farming and off land sources. The need for off land employment impacts the time and capacity for these landowners to undertake meaningful agricultural and land management

¹ *Rural Areas Strategy Plan: Toward 2011*, Shire of Bulla, June 1993.

activities on their land. These social, economic and environmental factors combine to see an increasingly diminished capacity for viable agriculture to occur in Hume's green wedge. Consequently, many landowners within the green wedge community argue that their land cannot fulfil its purpose as a green wedge.

The only value that many residents can therefore envision for their land is for Council and the State Government to either rezone their land for residential development or to reduce the minimum subdivision sizes to enable more rural living subdivisions.

For many residents and land speculators this perception is fuelled by the recent history of continued expansion of the urban growth boundary. It is the core of the damaging perception identified in Section 3.3.1 of the Consultation Paper – the expectation that rural land is simply “urban land in waiting”.

The challenges of agriculture, and the perceived promise of urban development on green wedge land, exacerbate the cycle of land speculation, land banking and the increasing struggle for viable land uses and land management. The result of this for those wanting to live and enjoy the green wedge areas is a very challenging lifestyle that into the future will see the continued degradation of green wedge values that those living on the land initially sought. It is within this context that urban development (including signing long term sales contracts to land speculators) or further subdivision are seen as the solution to many of these issues.

Box 2 –Key findings of the Emerging Issues and Options Paper 2018 consultation

The community consultation for Council's *Emerging Issues and Options Paper 2018* revealed that there is no clear consensus among the community regarding how to approach the challenges that are affecting the green wedge. There were submissions both for and against changes to the urban growth boundary and changes to minimum subdivision controls as potential solutions.

Key findings of the consultation are as follows:

- The urban growth boundary was discussed in 60% of submissions to the Options Paper. Opinions on it were mixed, with approximately two thirds of submitters seeking changes to the urban growth boundary, the remaining third not wanting to see it change and the green wedge further degraded.
- Approximately three-quarters of submissions discussing housing were seeking a reduction in minimum subdivision controls, most citing this as the only way that agriculture and good land management could be achieved in Hume's green wedge.
- Approximately two-thirds of submissions discussing agriculture stated that agriculture is not viable in Hume due to water and soil limitations combined with the rising cost of land management. Many of these submitters acknowledged that while they consider agriculture to not be viable in current conditions, their preference would be to keep farming within the green wedge with more government support to do so, including new government grants and rebates and other programs discussed later in this submission.
- The majority of submissions expressed that the above issues contributed towards increasing land management challenges and burdens that has increased their desire to sell their land to land speculators.

Hume's draft Rural Strategy

Through the development of the draft Hume Rural Strategy it has been clear that there is no silver bullet for resolving the challenges of Hume's green wedge. Rather a holistic, integrated and partnership approach from both local and state government across town planning, environmental planning, resource planning, land management, economic development and community development is required to:

- achieve a more financially viable long-term land use future for Hume's green wedge including but not exclusively based on agriculture.
- maintain and support the rural community's interest and capacity to safeguard the natural and cultural values of the green wedge.

It will also require and depend very heavily on the capacity and the motivation of rural landowners to invest money, time and resources into their land. To this end, the draft Hume Rural Strategy seeks to enable the green wedge community to envision a positive future for Hume's green wedge and encompasses three themes that reflect this holistic and partnership approach, see figure 2:

1. **Certainty** – on the future planning for the rural areas, particularly future urban development and subdivision in order to assist the rural community to make investments in their land with a greater level of confidence.
2. **Support** – to help enable landowners to continue to pursue current activities, diversify into new activities, maximise opportunities associated with a growing urban population within the UGB, and to effectively manage land.
3. **Engagement** – to inform the rural community of potential future changes and to grow community connectedness and capacity to realise future opportunities and manage challenges.

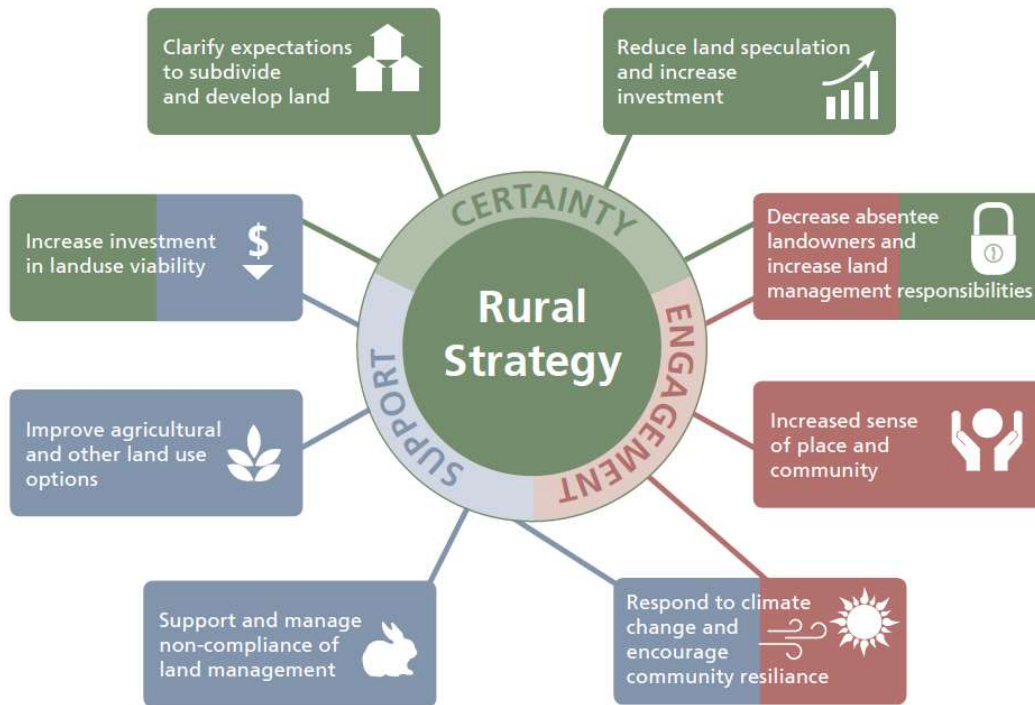


Figure 2 Addressing the challenges of the green wedge by offering certainty, support and engagement

Part B – the Planning for Melbourne’s Green Wedges and Agricultural Land Consultation Paper

Council considers that the Consultation Paper includes a highly suitable Vision but is concerned that the proposed options and actions to achieve it are limited and reductive in three major respects as it relates to Hume’s green wedge:

- The pervasive focus on agriculture at the expense of other appropriate land uses that could achieve a more viable green wedge.
- A lack of consideration of much needed complimentary practical non town planning related tools to support viable green wedges.
- A lack of urgent new action or measures to achieve a reliable cost effective water supply to enable viable agriculture.

A detrimental focus on agriculture

Council understands that this project was born out of a political commitment prior to the last election to protect the right to farm. It supports the desire to protect agriculture and address pressures and activities that undermine viable agriculture areas around Melbourne.

Council also supports the subsequent broadening of the scope of the project to consider measures to address other green wedge issues. This is vital if a viable long term future for Hume’s and Melbourne’s green wedges are to be realised.

However, it is evident that the Consultation Paper’s genesis has resulted in a project that is overly focused on protecting agriculture to the near exclusion of other important green wedge issues and opportunities.

While the Consultation Paper acknowledges that each green wedge has its own unique role and functions, outside of agriculture most of these roles are rarely more than acknowledged in the Paper. This imparts the impression that protecting and enabling agriculture is not one of the primary roles in Melbourne’s green wedge areas, rather it is the only role.

As discussed in Part A of this submission, agriculture in Hume’s green wedge is very challenging and many of our landowners believe that it is not viable. While Council will continue to support and encourage agriculture within the municipality, it should not be seen as the only means to achieving the important role that Hume’s green wedge plays in protecting the curfew free status of Melbourne Airport or fulfilling its other roles.

A continued focus on agriculture as the primary purpose for Hume’s green wedge will continue to fuel the perception that green wedge land in Hume cannot fulfil its purpose and so it’s future as urban land is inevitable. Such belief reinforces the destructive cycle of land banking and land management challenges experienced by Hume’s green wedge community.

Tighter controls to the urban growth boundary and subdivision standards are insufficient to combat this alone.

Council believes that encouraging non-agricultural viable land uses within Hume's green wedge is an important way to begin to reduce the destructive cycles associated with these challenges. It allows people to imagine and explore new uses for their land that will change how their land has been seen in the past.

Unfortunately, such uses including tourism and recreation – which are also included as key function of green wedge areas – appear to be discouraged within the Consultation Paper in favour of agriculture.

Continuing to see and respond to all green wedge land as primarily for agricultural land uses will limit the extent to which Council and the green wedge community can create a viable and vibrant green wedge.

It does not provide a realistic vision for Hume's green wedge.

Specific recommendations on how this could be achieved in the Consultation Paper are discussed in Part C of this submission. Underpinning these recommendations Council submits that DELWP must:

- Recognise that non agricultural land uses are a legitimate and vital uses in green wedge areas, particularly in Hume's green wedge where conditions for agriculture are very challenging.
- Better consider how it can support non agricultural land uses to ensure that Hume's green wedge has a viable long-term future.

Viable green wedges require more than town planning controls

Council welcomes the State Government's commitment to provide certainty on the future development and subdivision potential of green wedge areas. This is very consistent with Council's draft Rural Strategy theme of 'certainty' as outlined above.

Council is, however, disappointed that the Consultation Paper is so deliberately and narrowly focused on legislative and planning solutions. Indeed, on page 34 of the Discussion Paper, it recognises that a planning response is insufficient to realise a food bowl around Melbourne but then proposes no new or meaningful directions that are not planning related. This is not useful when attempting to address key matters like recycled water - see below.

Council contends that whilst the planning scheme has a role in managing inappropriate development in green wedge areas, it has a very limited role in facilitating desired development, particularly where a permit is not required (as is already the case for many of the uses that the Consultation Paper identifies as desired land uses, such as agriculture).

Hume's green wedge does not only need certainty of its protection from urban expansion, it needs holistic and integrated support measures to make it viable now and in the longer term.

Council already undertakes a number of support actions and measures aligned to the needs of the green wedge and is committed to expand these in line with the draft Rural Strategy. Council expects that the State Government also play its role, as outlined further in Part C below. There is very little in the Consultation Paper that targets these support needs of green wedge communities.

If the State Government is serious about realising its Vision for green wedges, in order to achieve this vision for Hume's green wedge and to protect the on going curfew free status of Melbourne Airport, it needs to broaden its focus beyond town planning. It needs to adopt the holistic, integrated and partnership approach across town planning, environmental planning, resource planning, land management, economic development and community development proposed by the draft Hume Rural Strategy.

Recommendations on how this could be achieved are discussed in Part C of this submission.

Reliable cost effective water - a critical requirement for agriculture

As outlined in Part A, Hume's green wedge suffers from low and unreliable rainfall. This currently undermines the viability of agriculture and is anticipated to worsen as a result of the impacts of climate change.

Whilst the Discussion Paper recognises this issue and makes "recycled water and stormwater" and "fit for purpose water" a key pillar of its Vision, there is no actions or options proposed to realise this beyond comments about continuing to work with water authorities.

Council has been actively participating in projects with water authorities aimed at securing recycled water and stormwater to its green wedge for the best part of two decades. Whilst some important progress has been made in this time, fundamental barriers remain that urgently need to be addressed by State Government to avoid a further two decades of worsening agricultural viability due to a lack of reliable and cost effective water.

Recommendations on how this could be achieved are discussed in Part C of this submission.

Part C – Proposed Options

Strengthening the Legislative and Policy Framework

Legislative and policy framework for Melbourne’s green wedges

Section 3.1.1 acknowledges the many and varied roles that different green wedges play across Melbourne and therefore that some use and development outcomes might be appropriate in some green wedge areas but not in others.

Council welcomes recognition of the diversity of roles the green wedge play and the recognition that the appropriateness of different land uses change based on the context of the specific green wedge. Tourist uses which have proliferated in some green wedges, that for instance might be inappropriate within Yarra Ranges due to a further loss of commercial farming land, might be suitable within Hume’s green wedge where such constraints do not apply.

Council questions the value of this recognition being limited to an introduction of regional policy as discussed in Section 3.1.1 and 3.1.2. While some green wedges may have enough commonalities that would allow a regional policy to adequately address their requirements, Hume Council considers that the unique role and constraints of Hume’s green wedge cannot adequately be addressed through regional policy.

Map 5 in the Consultation Paper identifies that Hume is to be located within the northern regional group that includes Macedon Ranges, Mitchell, Whittlesea and Nillumbik councils. Beyond their northern geography, Hume’s green wedge does not share many common functions with these areas that have high value agricultural land and an established visitor offering. As outlined in Parts A and B of this submission, the features and challenges that need to be strengthened and engaged within any policy for Hume’s green wedge include:

- The imperative to protect Melbourne Airport
- A lack of agricultural viability
- Urban development pressures from two growth fronts
- a legacy of urban growth boundary changes.

Council is concerned that the need for a tailored response for Hume’s green wedge would be lost in a regional policy process, thereby exacerbating the challenges outlined in Part A. As this could result in regional policy that speaks to the wider agricultural viability of the northern region that is not realistic or applicable to Hume.

It is acknowledged that Sections 3.1.1 of the Consultation Paper indicates that specific features, constraints and strategies for individual green wedges should be highlighted in a Green Wedge Management Plan (GWMP).

Given that the Paper proposes nothing but restrictive changes to the legislation and the planning scheme, Council is concerned that any relevant local context for Hume’s green wedge in a GWMP will be constrained by its subordinate status to overarching State-wide or regional policies.

Green Wedge Management Plans (GWMP)

Hume City Council does not currently have a GWMP due in the most part to the constantly changing urban growth boundary since 2003 and the resulting uncertainty of future changes. Indeed, Hume's green wedge has seen 75km² of land moved inside the urban growth boundary (equivalent to 23% of the 1995 green wedge area) over four variations to the urban growth boundary.

Despite not having a GWMP, Hume has undertaken many of the same actions as councils with GWMPs, including support programs for landowners. These have been well received by the rural community.

Hume City Council is now well advanced with its Rural Strategy as advised above. This has demonstrated the value in engaging with the rural community on their challenges, but it is too early to confirm its lasting value. As the preparation of GWMPs are time consuming and resource intensive, it is considered important that before measures are taken to make GWMPs mandatory that a full review of their value be undertaken.

Specifically, the review should confirm the absolute need for a GWMP and their value and effectiveness in addressing the issues in green wedges and highlight those elements that are most important. This would explore what actions identified by GWM plans have been pursued because of the plan.

Supporting agricultural land use

Section 3.2 of the Consultation Paper captures many of the challenges that Council has heard from the green wedge community are confronting agriculture today, including climate change, high operational and land management costs and the impact of increased land speculation.

The State Government has the capacity to support the green wedge community and create, if not an agriculturally viable area, then at least a productive and valued one complementary to its specific function as a green wedge. In addition to the support measures highlighted in the Consultation Paper, Council recommends that the State Government commit to practical actions such as:

- Actively facilitating secure access to recycled water.
- Increased flexibility for landowners to diversify into non-agricultural uses.
- Providing farmers business support to transition to new forms of farming and non-agricultural complementary uses.
- Advice on how to access new and growing markets for agricultural produce.
- Non-agricultural land use support to promote tourism.
- Support and promotion of tourism through a coordinated regional approach, such as ensuring all green wedge areas are within regional tourism areas.

- Green wedge landowner support to manage bushfire risks, manage environmental degradation and commence sustainable land management practices. This could be through financial assistance and education programs.
- Initiate weed compliance programs under the CALP Act to counteract the impact of weed and pest spreading from poorly managed land.

It is unsatisfactory that any support measures might be investigated later at an unspecified date. These measures are urgently needed now to support the Hume green wedge community.

Council has advocated without success for the State Government to invest in noxious weed compliance. The green wedge community has continually expressed the need for compliance programs to address the ongoing burden of land management that is continually exacerbated by noxious weeds spreading from neighbouring mismanaged land. In the absence of State Government action, Council has begun investigating its capacity to act within this space. It is crucial that the State Government also commit to undertaking compliance programs in Hume. Such action demonstrates the commitment of the State Government to support green wedge communities and to make the green wedges viable.

Council encourages the provision of additional resources and guidance to assist Council officers, as proposed in Sections 3.2.1 and 3.2.2 of the Consultation Paper, such as to assess the impact of dwellings and other proposed uses on agriculture. At the same time, Council is again concerned that this contributes to the unrealistic expectation that the primary use of all green wedge land is agriculture.

Indeed, the current wording within the proposed options that a practice note will provide guidance on “how to determine whether a discretionary use will lead to loss of agriculture as the primary use of land”, reinforces the ongoing, anachronistic assumption that the primary use of all land in every green wedge is agriculture.

Should such guidance be produced, this needs to be properly contextualised with an acknowledgment and discussion of the non-agricultural roles and functions of green wedges.

Council also questions the status and statutory weight of any agricultural referral or expert advisor would hold, as proposed in Section 3.2.2 of the Consultation Paper. Council suggests that it would be of benefit for such advisors to be engaged to develop proactive programs to better support farmers and green wedge residents.

Supporting agricultural diversification, value-adding and innovation

Section 3.2.5 of the Consultation Paper notes that the planning scheme encourages the diversification and value adding of complimentary land uses with agriculture, which has occurred in Yarra Ranges and Mornington Peninsula, before proceeding to discuss the risks associated with such diversification. Namely, that agricultural diversification which pairs agriculture with restaurants, accommodation and other visitor experiences displaces commercial farming and ongoing agricultural uses of land to the level where agriculture might become a subordinate land use.

Council considers that these challenges of agricultural diversification highlighted in the Consultation Paper are not indicative of the challenges experienced across all green wedge areas. While they might be a concern in the Yarra Ranges and Mornington Peninsula, Hume Council would encourage such uses to establish within Hume's green wedge to either support the viability of existing agricultural uses or provide a viable land use in their own right.

This section again is indicative of the assumption that all green wedge land is valuable for agriculture that is constraining the options in Hume's green wedge by considering all other land uses subordinate and less desirable.

Further work should be undertaken to broaden the scope of what appropriate land uses could be considered as value adding to agriculture. Council considers that this is a missed opportunity for the Consultation Paper, as enabling these uses would encourage the agriculture that is occurring within Hume's green wedge to remain financially viable for the farmers. As green wedge land could then become to be seen as viable, this would thereby start to reduce the land lost to land speculators.

Future proofing Melbourne's food bowl

Section 3.2.3 of the Consultation Paper discusses the need to secure alternative water access to agricultural areas. While Council is supportive of this objective, we are again disappointed in the Consultation Paper's lack of meaningful commitment to achieving this.

The proposed options again only seek to alter the planning scheme and do not offer any tangible outcomes that will assist the provision of recycled water to the green wedge.

Notwithstanding the important achievements that have occurred within the integrated water management (IWM) space, Council believes the State Government needs to take increased ownership of the issue and facilitate the provision of recycled water to the green wedge. Without this level of support and leadership, it has been frustrating for Council to achieve in principle support for high order IWM outcomes, even in greenfield sites that directly adjoin the green wedge, which are not realised due to regulatory and funding constraints. Despite the many IWM forums and background reports that propose exciting IWM opportunities within greenfield development sites, we are unable to achieve high order outcomes that could see the provision of recycled water across the single road that often separates a greenfield site and Hume's green wedge.

In practice, the well intentioned policy imperatives are constantly being undermined by rushed and repetitive approaches, instead of future focussed and innovative initiatives.

Council also requests the State Government review how the water authorities are structured and operate, and to explore a shift away from the current user pay system. The cost burden of constructing infrastructure that takes recycled water to farmland should not be placed on the end users alone. Under the current system the provision of recycled water infrastructure to Hume's green wedge is prohibitively expensive to those who need it.

The urban spread of Melbourne is creating an increasing amount of hard surfaces and a corresponding increase in the volume of stormwater runoff that needs to be safely and sustainably disposed of in a manner that does not degrade waterways. Currently, the cost

burden of transferring this surplus water to green wedge areas is mostly imposed on the farmers and landowners within Hume who cannot justify the cost of the infrastructure nor the cost of the water itself. Yet the environmental cost of placing this excess water into urban waterways and thereby degrading environmental values, is not borne by anyone.

Should Hume's green wedge ever begin to be suitable for agriculture, Council believes the State Government must actively facilitate its provision in Hume.

Specifically, Council seeks:

- The provision of alternative water to green wedge areas be written into water authorities' Statement of Obligations.
- The cross subsidisation of recycled water projects from metro Melbourne water users to the green wedge areas should be seriously considered by the State Government. If supported, then water authorities should be directed to incorporate this into their business cases, rather than the current model which relies on the need to prove the willingness of the urban community to do this on an individual case by case basis.
- Further review and guidance should be given to water authorities regarding extraction licences for surface or groundwater. This includes allowing additional consideration of alternative local water sources, such as the presence of any nearby recycled water pipelines. Licences must also be dependent on current productive use.
- Consideration should be made in the design and delivery of Melbourne Water's Development Services Schemes in order to identify and implement IWM outcomes, particularly where this can support securing water supply from excess urban stormwater flows to the adjacent or nearby green wedge.

Managing use of green wedge and peri-urban land

Managing the urban-rural interface

Section 3.3.1 of the Consultation Paper highlights many of the challenges of proximity to urban development that Council has heard are being experienced by Hume's green wedge community. Perhaps owing to the extensive rural-urban interface of Hume's green wedge between two separated growth fronts, these challenges are not confined to discrete interface areas, but are experienced across a large proportion of Hume's green wedge.

Council encourages guidance and policy on the transition from the urban to rural interface as a method of supporting a fixed urban growth boundary. As the Consultation Paper correctly states, consideration and guidance needs to be provided for what is also occurring on the urban side of the urban-rural interface. This is appropriate as the burden of providing a transition between urban to rural, which is also necessary for bushfire and kangaroo management of urban areas, should not fall to rural landowners alone.

Hume's green wedge is mostly adjoined by areas with either current precinct structure plans (PSPs) or PSPs currently being prepared by the Victorian Planning Authority (VPA). Any changes to address this issue on the urban side need to occur immediately to guide PSPs that are currently under preparation or it will be too late. Consideration should also be given to the

VPA then applying these changes to approved PSPs through a State wide amendment where such land is not already developed or subject to an approved planning permit. If this change is left to individual Councils to pursue on individual PSPs this will be too slow.

Planning for future infrastructure and energy needs

Council's Rural Strategy acknowledges that green wedges serve an important role in providing land for important infrastructure projects to support metropolitan Melbourne. In addition to protecting Melbourne Airport as a State significant asset, Hume's green wedge is home to several quarries, waste and resource recovery facilities, an Extractive Industry Interest Area (EIIA) and the future Outer Metropolitan Ring Road (OMR).

Notwithstanding the need for these infrastructure assets, Hume's green wedge is often seen as the default location for any infrastructure project or asset. The convenience of locating infrastructure that serves the wider metropolitan area in green wedge areas must come with an awareness of the community that is being impacted.

Whilst it is understood that the State Government follows its minimum statutory requirements for such projects, it is apparent from our rural community that the lived experience of these processes and the implementation of the projects is far from certain or satisfactory.

Landowners and businesses in Hume's green wedge face considerable uncertainty for the future of their land as their properties become layered with different overlays and easements for major projects. With the example of the OMR, the timing, exact alignment and requirements for compulsorily acquiring land for many of these proposed infrastructure projects, are often unknown and creates further uncertainty for landowners who landholdings will be bisected by the future freeway.

Whilst Council will continue to advocate and consider the impact of such proposals on the green wedge community, there must be a commitment from the State Government for meaningful engagement with affected communities throughout the life of these projects.

Council stresses the need for this engagement to form part of the support mechanisms that the State Government provides to green wedge communities.

To encourage investment in viable land uses on green wedge land, it is crucial that certainty is provided not only on the urban growth boundary but on these large infrastructure projects.

The Consultation Paper also acknowledges the need to balance rural land uses against waste and resource recovery facilities and quarries, yet the Paper does not provide further guidance on how this might be achieved. Additional guidance should be provided for these facilities within a green wedge context to ensure that their impact on the other uses and the natural values of the green wedge are managed.

Council supports further consideration of refreshing the EIAs and the consideration of an overlay to ensure their presence on an affected property is known and understood by landowners.

Council is disappointed in a lack of further consideration of renewable energy generation facilities. Renewable energy offers a potential opportunity for a viable land use within Hume's green wedge. Given the innovation that is rapidly occurring in the renewable energy space, further consideration should be given to these land uses to ensure they are not unduly restricted in a green wedge context. Furthermore, the State Government should actively facilitate the exploration of these uses, in keeping with the role of the green wedges to provide infrastructure and renewable energy needs to Melbourne, and in so doing enable a potentially viable green wedge in Hume.

Managing discretionary uses - schools and places of worship

Since schools and places of worship became allowed uses in the green wedge in 2013, Council has seen a number of these uses seek to establish in Hume's green wedge.

In the absence of guidance previously from the State Government regarding how to consider their appropriateness in a rural context, Council has taken a precautionary approach to these applications and has discouraged their establishment in the green wedge. This is particularly necessary when they have proposed to be located under the Melbourne Airport flight paths and would compromise the curfew free operation of the Airport.

Council considers that schools and places of worship could be suitable within Hume's green wedge, subject to appropriate conditions. The draft Rural Strategy proposes less restrictive conditions on where schools and places of worship could be appropriate compared to those proposed in the Consultation Paper, with the following accessibility attributes in preference order:

1. a declared arterial road with public transport services on the immediate edge of the urban growth boundary
2. a declared arterial road with public transport services
3. a declared arterial road without public transport services
4. a local road within 1km of a declared road and with a full lane width in each direction and public transport services (minimum 7 metres wide)
5. a local road within 1km of a declared road and with a full lane width in each direction
6. where the existing and forecast road volumes have capacity to accommodate any likely traffic volumes.

Such facilities would also be discouraged in locations that do not have these transport attributes and will generally not be supported under the Melbourne Airport Environs Overlay.

The above conditions do not require that schools and places of worship be on land adjoining the urban growth boundary as proposed in the Consultation Paper.

While it is Council's preference that schools and places of worship in the first instance are accommodated within the urban areas, we understand that these institutions can struggle to find affordable land within the urban growth boundary, as is acknowledged within the

Consultation Paper. Council believes that the trend of schools and places of worship being forced into the green wedge needs to also be better understood and addressed through complimentary changes within an urban context.

Restricting where these uses are allowed within the green wedge will not facilitate their establishment within urban areas without these complimentary changes within the urban area. Further support should be given to schools and places of worship to establish in urban areas in proximity to their communities where they are needed. Non-government schools and places of worship must also be better considered and better planned for during the PSP planning process to allow them the opportunity to establish within an urban context.

Soil and earth storage

As noted in the Consultation Paper, the schedule to the Green Wedge Zone and Green Wedge A Zone in the Hume Planning Scheme contains a permit requirement for earthworks which involve the receipt, importation, stockpiling or placement of more than 100 cubic metres of fill.

This requirement has proven successful in Council's efforts to combat the dumping of clean fill within Hume's green wedge as it enables Council to engage in compliance action against properties who have received clean fill.

It is understood that in the past a number of green wedge and peri-urban councils have sought to have a similar permit requirement included within their green wedge or rurally zoned land schedules, however these amendments have been unsuccessful.

Council encourages the need for such a condition within rural zones to be re-examined as an outcome of this current process. This is necessary considering the detrimental impact that the dumping of fill can have on agricultural properties and the absence of any options proposed within the Consultation Paper to address this.

Other discretionary uses: halls, exhibition centres, accommodation, food and drink premises, etc.

As has been outlined throughout this submission, Council believes that Hume's green wedge owes its existence to the presence of Melbourne Airport, not simply to protecting and enabling agriculture as is implied in the Consultation Paper. If the land is to not be seen solely for its urban development potential, support for viable non-agricultural land uses need to form part of the plan.

Given that Melbourne's green wedges fulfil many different roles, Council is disappointed that the review of discretionary uses in the green wedge does not consider the need for relaxing the restrictions for some of these uses – particularly for important tourism or recreation uses. As has been discussed throughout this submission, the need to balance the impacts of discretionary uses in agriculturally viable green wedge areas such as Yarra Ranges and Mornington Peninsula, is not appropriate in the context of Hume's green wedge where there are very few such uses and certainly not enough to create a loss of agricultural land. Indeed, the concern is the lack of such uses to help make Hume's green wedge (including agriculture) more viable.

Council is disappointed and frustrated that the Consultation Paper on the one hand seeks to recognise that all green wedges are different and then present options that either ignore or contradict this sentiment in the options proposed for managing discretionary uses. These options only serve to maintain or further restrict the diversification of agricultural or new tourism uses to establish in their own right.

It is necessary that these uses are supported in order for Hume's green wedge to continue to fulfil its function, to allow landowners and businesses to establish and thrive and minimise the loss of land to underutilised land banking opportunities.

Council urges the State Government to give further consideration to the appropriateness of tourism and recreational uses in different green wedge contexts where the scale of these uses and built forms are appropriate for a site. This includes the potential to:

- increase patron caps and capacities for restaurants, accommodation, function centres and other key tourism uses.
- allow councils to specify increased maximum patron caps in the Green Wedge Zone and Green Wedge A Zone schedules reflective of the needs of their green wedge areas.

Implementing Design and Development Guidelines

Council is generally supportive of the premise of creating design and development guidelines for the green wedge as large-scale developments can be difficult to readily assess.

Given the diversity of roles, features and landscape elements across Melbourne's green wedge, Council recognises the challenges of creating and implementing a standard suite of State-wide guidelines for all green wedges.

It is noted that the options proposed will require councils to undertake landscape typology work in order to develop these design guidelines via GWMPs. Council is concerned that given the extensive work that is required for this approach, that undertaking this work is beyond the scale of the problem, particularly for Hume where such permit applications are currently very rare. Council is also concerned at the lack of guidance on how these landscape typologies would translate into considerations of a design response, and that trying to manufacture design standards for such diverse landscapes and development proposals is not an effective way to achieve good design outcomes. Further, Council is concerned at the lack of statutory weight afforded to GWMPs in permit considerations.

If DELWP is minded to pursue this idea further, Council proposes that the State Government lead any landscape typology assessments, in partnership with Council, to assist in managing the resource burden. These assessments should be prepared in conjunction with an assessment of the appropriateness of increasing the capacity for discretionary uses within certain green wedge landscapes and inform tailored design objectives only in areas where it is deemed critical and an efficient use of scarce planning and design resources.

Conclusion

Council supports the intention of the Consultation Paper to provide certainty and a clarity of policy for Melbourne's green wedges and is generally supportive of the State Government's vision.

Council also appreciates that the diversities and complexities inherent in each green wedge make it extremely challenging to develop consistent policies and controls that are suitable for all green wedges. This challenge has eventuated in the Consultation Paper's pervasive and reductive focus on agriculture that is not appropriate or useful for encouraging a viable green wedge in Hume.

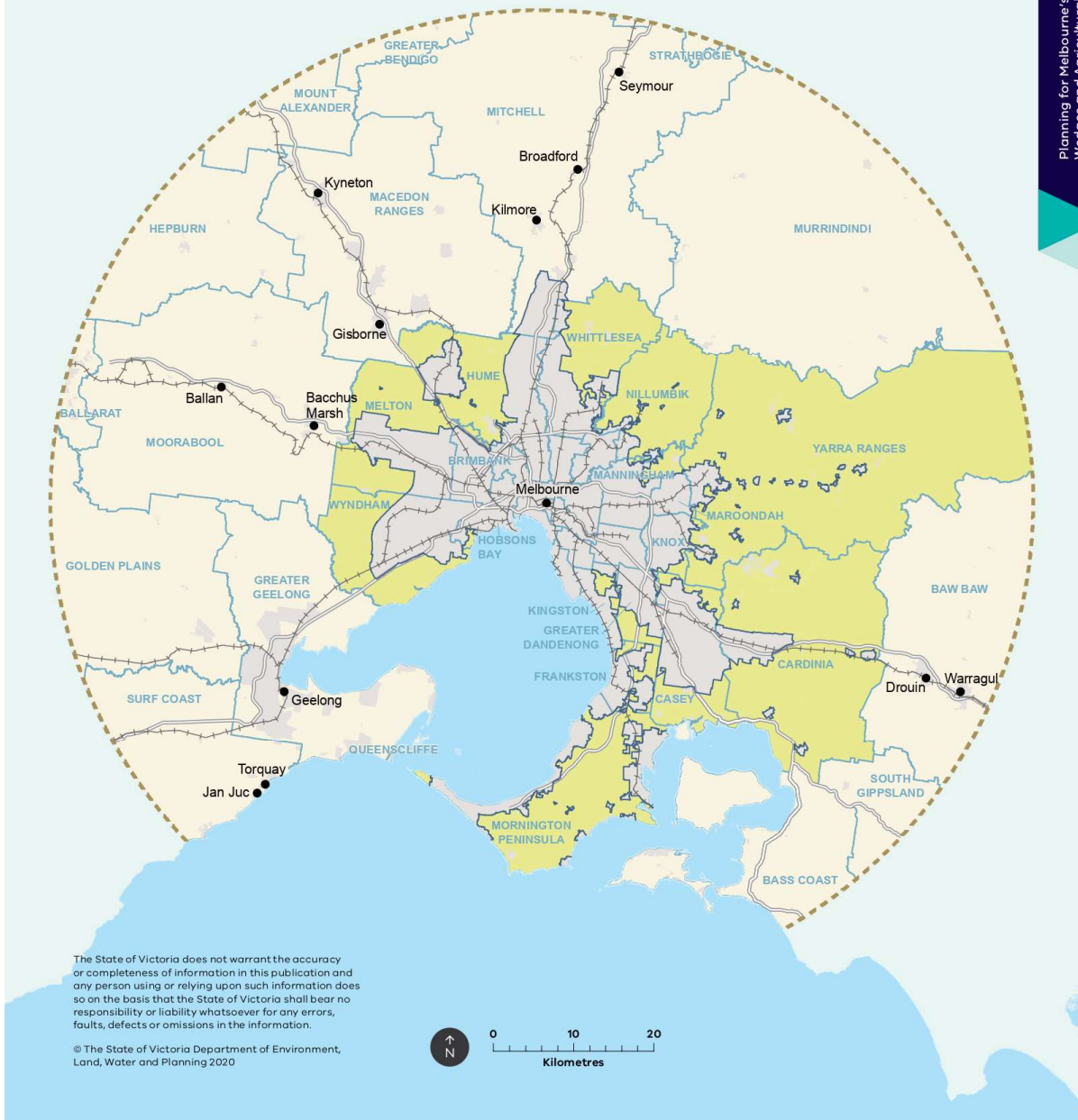
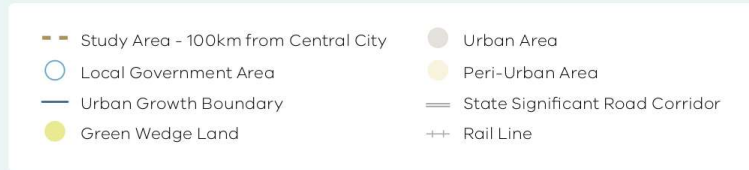
Hume's green wedge does not exist for its limited agricultural viability, it exists primarily to protect the operation of Melbourne Airport with other important roles. In order for the State Government to support Hume's green wedge to fulfil this function, and not simply be seen for its urban potential, the State Government must do more than is outlined in the Consultation Paper. It must allow, facilitate and support development and land uses that are complementary to this role to establish and grow. Doing this will also begin to address the vicious cycle of land speculation, the increased costs of land management and the continued pressure to see green wedge land urbanised.

The State Government must also engage in more practical supportive actions that can provide a meaningful impact on Hume's green wedge and achieve the vision outlined in the Consultation Paper.

Unfortunately, Council considers that many of the options proposed in the Consultation Paper are either inconsequential to Hume's green wedge or do little to prevent its continued degradation to the detriment of the rural community and to State priorities, notably the curfew free operation of Melbourne Airport.

Map 1

The study area — Melbourne's green wedge and peri-urban areas



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REPORT NO:	SU535
REPORT TITLE:	Submission to public consultation on Craigieburn West Precinct Structure Plan (PSP)
SOURCE:	Jillian McQuade, Coordinator Integrated Planning
DIVISION:	Planning and Development
FILE NO:	HCC15/759
POLICY:	-
STRATEGIC OBJECTIVE:	4.1 Facilitate appropriate urban development while protecting and enhancing the City's environment, natural heritage and rural spaces.
ATTACHMENTS:	<ol style="list-style-type: none">1. <i>Draft Craigieburn West PSP November 2020</i>2. <i>Hume City Council Cover letter HCC Submission to Craigieburn West PSP November 2020</i>3. <i>Hume City Council Submission to Craigieburn West PSP November 2020</i>4. <i>Technical Note - Traffic and Transport Peer Review - GTA 18 December 2020</i>5. <i>Proposed Transport Network Plan - Mesh-Planning - 17 December 2020</i>6. <i>Whites Lane proposed Cross Section for Craigieburn West and Craigieburn R2 PSP</i>7. <i>Track changed version of the PSP and UGZ Schedule</i>8. <i>Craigieburn West Green link cross sections</i>9. <i>Minister for Planning letter to HCC dated 18 October 2020</i>

1. SUMMARY OF REPORT:

The Craigieburn West PSP will provide for additional residential development and completes the structure planning for residential precincts in the Hume Growth Corridor. Council supported the Victorian Government's proposal to fast tracking of the *Craigieburn West Precinct Structure Plan* (PSP) process at its meeting on 7 September 2020 contingent on commitments relating to the delivery of Mickleham Road and consultation on the PSP process. Since then the Victorian Planning Authority (VPA) released the draft PSP (Attachment 1) and associated documents for public feedback from 17 November 2020 to 18 December 2020. The PSP incorporates key elements set out in Council's Hume HIGAP Strategy. Officers lodged a draft submission (Attachments 2 to 8) with the VPA in December 2020 highlighting several issues with the draft PSP, particularly regarding the road network, details around the Town Centre and tree protection. The draft submission also reiterates that Council does not support the expedited finalisation of the Craigieburn West PSP without an associated budget commitment to the duplication of Mickleham Road between Somerton and Donnybrook Roads (in accordance with Council resolution of 2 September 2020). The draft submission is attached to this report for Council's formal endorsement.

2. RECOMMENDATION:

That Council:

- 2.1 endorses the officers draft submission made to the Victorian Planning Authority (VPA) for Craigieburn West PSP dated 18 December 2020 (Attachments 2 to 8).**
- 2.2 endorses officers making submissions at a future Standing Advisory Committee Hearing based on the draft submission and in response to submissions to the proposed amendment made by other parties.**

REPORT NO: SU535 (cont.)

2.3 confirms and continues to advocate its position that it does not support the approval of the PSP without an associated budget commitment to the duplication of Mickleham Road between Somerton and Donnybrook Roads that sees completion of works prior to or in tandem with development proposed in the PSP.

2.4 continues to advocate to State Government for the duplication of Mickleham Road between Somerton and Donnybrook roads that sees completion of works prior to or in tandem with development proposed in the PSP.

3. **LEGISLATIVE POWERS:**

Planning and Environment Act 1987.

4. **FINANCIAL IMPLICATIONS:**

4.1 The Planning Scheme Amendment proposes the introduction of an Infrastructure Contribution Overlay requiring developers of land in the PSP area to pay the standard levy amount for transport infrastructure (\$126,713 per ha) and the capped levy amount for community and recreation infrastructure (\$91,050 per ha). The standard levy rates for the Metropolitan Greenfield Growth Areas development settings are indexed on 1 July each year in accordance with the indexation method specified in Ministerial Direction on the Preparation and Content of ICPs.

4.2 Based on indicative costings for the proposed the transport items outlined in the PSP, the standard ICP levy amount for transport is considered sufficient and suitable. However, officers submission acknowledges that any additional transport items or significant increases in item costs that might arise through submissions and changes to the PSP prior to it being finalised would most likely exceed the standard transport levy amount. In this case Council would seek full costings to determine the adequacy of a standard levy or whether a supplementary ICP is necessary.

4.3 An indicative drainage network has been included in the PSP and finalisation is subject to Melbourne Water and Council approval. The finalisation of the drainage strategies must be designed to minimise management obligations and costs for Council.

5. **ENVIRONMENTAL SUSTAINABILITY CONSIDERATIONS:**

The PSP protects a significant portion of the sites biodiversity values and provides important connectivity to landscape and ecological values in the wider area, including the Merrifield West conservation area and conservation area in the Lindum Vale PSP. The open space network connects to the existing open space network along Aitken Creek (and its tributaries and to open space areas around the Greenvale Reservoir. This ensures a continuous open space network connecting the PSP area to Merri Creek, the Melbourne CBD and Port Phillip Bay. The PSP interfaces with green wedge land and provides important fauna connectivity between urban open space and green wedge land. Ensuring safe and viable kangaroo movement is critical in the Craigieburn West PSP to ensure that kangaroos are not locked into urban areas by development staging or an absence of long-term connections once development is complete. In submissions to the PSP officers have sought permit controls to manage kangaroo movement and staging of development.

6. **CLIMATE CHANGE ADAPTATION CONSIDERATIONS:**

The PSP supports sustainable development outcomes including higher density development within a walkable catchment of the Town Centre; off road pedestrian and cycle linkages; a road network oriented on cardinal axis and retention of trees to contribute to urban cooling.

7. **CHARTER OF HUMAN RIGHTS APPLICATION:**

The Charter of Human Rights has been considered and the recommendations of this report give no rise to any matters.

REPORT NO: SU535 (cont.)

8. COMMUNITY CONSULTATION:

- 8.1 Consultation on the PSP is the responsibility of the VPA as the Planning Authority. The VPA placed the PSP and supporting amendment documentation on public exhibition from 17 November 2020 to 18 December 2020.
- 8.2 As part of this consultation process documents were available online and the VPA held a virtual public information session via Zoom videoconferencing software on Monday 30 November 2020. Three Council officers attended the meeting and the community's concerns, primarily with the need to deliver the duplication of Mickleham and Craigieburn roads and protect native vegetation in the precinct, have been taken into consideration in officer's assessment of the PSP.
- 8.3 The consultation process was consistent with Council's resolution of 2 September 2020 which supported the fast track process contingent, in part, on notifying the local community and holding a public information session or webinar in the first two weeks of the engagement period.
- 8.4 The public consultation was limited to four weeks closing on 18 December 2020 making reporting to Council prior to the submission deadline unachievable. The attached submission was lodged with the VPA on 18 December 2020 on the basis that a report would be put to Council seeking its endorsement.

9. DISCUSSION:

Background

- 9.1 The Craigieburn West precinct has an area of approximately 562 hectare and sits between Mickleham Road to the west and developed areas of Craigieburn to the east. The precinct has a narrow east-west dimension with a long, north-south axis extending from Mount Ridley Road to Greenvale Reservoir in the south. Due to significant biodiversity values on the land, the precinct was left out of the Urban Growth Boundary until 2012 and by this time the planning, and some development, of adjoining precincts had been completed. It is the last major, residential Precinct Structure Plan (PSP) to be planned for the Hume corridor.
- 9.2 Council has been working with the VPA for over two years to develop an urban structure that responds to the features of the precinct and ensure priority matters are addressed in the finalisation of a PSP for Craigieburn West (refer Figure 1). The PSP has been developed under a trial of the VPA's 2.0 streamlined process that aims to resolve PSPs from background studies through to a completed planning scheme amendment in reduced timeframes. This process has allowed officers to input into consultant background reports; a codesign workshop which involved collaboration from various stakeholders including landowners, State Government agencies, Hume City Council, and the VPA; and at regular meetings with the VPA.
- 9.3 The VPA released the draft PSP (Attachment 1) and associated documents for public feedback from 17 November 2020 to 18 December 2020. The Place Based Plan (Figure 1) provides a guiding framework for the development of residential, open space, community and open space land uses and the arterial and connector transport network.

Contingent support for fast track process

- 9.4 The VPA advised Council on 3 August 2020 that as part of the Victorian Government's response to the COVID-19 crisis, they will be fast tracking 19 planning and infrastructure projects including the Craigieburn West PSP. The changes to fast track the statutory amendment process generally compare with the statutory planning scheme amendment.

REPORT NO: SU535 (cont.)

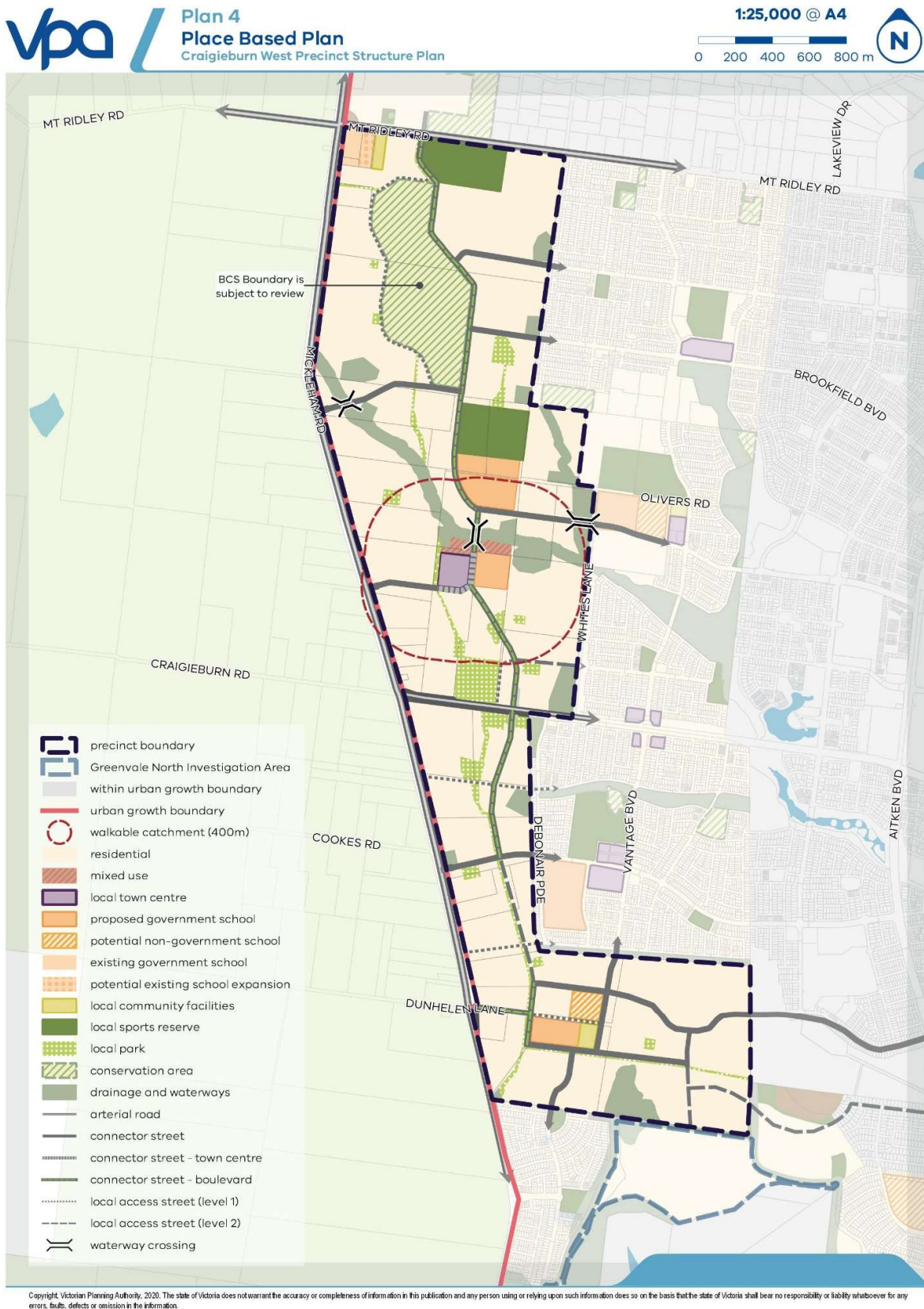


Figure 1 – Craigieburn West PSP draft Place Based Plan

REPORT NO: SU535 (cont.)

- 9.5 Council confirmed support for the Craigieburn West PSP to be facilitated through a fast track process at its meeting on 7 September 2020 subject to Council:
- 1) *Write to the Minister for Planning indicating that it does not support the expedited finalisation of the Craigieburn West PSP without an associated budget commitment to the duplication of Mickleham Road between Somerton and Donnybrook roads that sees completion of works prior to or in tandem with development proposed in the PSP.*
 - 2) *Advise the Minister and the VPA that its support of any outcomes of the engagement process run by the VPA and review of unresolved issues by an Advisory Committee would be contingent upon:*
 - a) *A funding commitment to duplicate Mickleham Road between Somerton Road and Donnybrook Road before approval of the Craigieburn West PSP.*
 - b) *Notifying the local community and holding a public information session or webinars in the first two weeks of the engagement period initiated by the VPA.*
 - c) *Evidence of the times, benchmarks and costings being provided to Council confirming the Infrastructure Contribution Plan (ICP) will be a standard ICP levy.*
- 9.6 The consultation for the draft PSP met the expectations under Item 2b) as discussed under 'Community Consultation' above.
- 9.7 The draft PSP confirmed items to be funded under the ICP and the VPA provided indicative costings to Council that indicate a standard transport levy will be adequate (and satisfies Council's resolution under 9.5 (2c) above). However it is noted that the ICP transport costings are nearing the upper limit for a standard levy and if further transport items are included in the ICP (as has been requested by a number of developer submissions), Council would seek full costings to determine the adequacy of a standard levy. The community and recreation levy is fixed and unable to be supplemented. The costings provided confirm that the community and recreation proposed in the PSP and ICP will be fully funded by contributions collected under the PSP.
- 9.8 In regard to the duplication of Mickleham Road, Council wrote to the Minister for Planning and VPA on 11 September 2020 outlining the issues in items 1 and 2a). The Minister provided a response dated 18 October 2020 (Attachment 9) that failed to make a commitment to the timing and budget for Mickleham Road and noted:
- The Department of Environment, Land, Water and Planning (DELWP), the VPA and the Department of Transport are working together to better inform a prioritisation of state transport improvements and services to support growth in the northern growth corridor. The relevant growth area councils will be updated on the outcomes of these investigations and recommendations in due course. These agencies are aware that Mickleham Road is an important part of the network and its upgrading will be required over time to accommodate growth.*
- 9.9 The State Budget released on 23 November 2020 includes \$9.11 million to upgrade the Mickleham Road – Aitken College intersection and scoping works for the development of Mickleham Road corridor improvements. The budget commitments do not provide for the construction of the duplication of Mickleham Road.
- 9.10 Mickleham Road is a State managed, arterial road and runs along the western boundary of the Craigieburn West PSP. It is the primary north-south arterial for the northern growth region of Hume connecting communities in southern Hume and Melbourne Airport with the suburbs of Greenvale, Mickleham and Craigieburn – two of Australia's fastest growing communities. As an arterial road, any new and upgraded intersections to Mickleham Road must be planned for at PSP stage however other improvements, such as duplication, are reliant on State and Federal government funding and are not compelled through the PSP process.

REPORT NO: SU535 (cont.)

- 9.11 In the six years between 2013 and 2019 traffic volumes on Mickleham Road have more than doubled with an increase of 54% from 15,313 vehicles per day (vpd) to 28,590 vpd in 2019. This level of traffic exceeds the capacity on Mickleham Road as a single lane rural road and the State's trigger of 20,000 vpd for duplication of arterial roads. The community regularly experiences congestion on Mickleham Road that leads to lengthy delays in their commute to work and school and in accessing essential services. Urban traffic also spills onto other rural roads in the area west of Mickleham Road, including Craigieburn Road, Bardwell Drive, Oaklands Road, and Somerton Road, as motorists seek alternative routes to bypass the congestion on Mickleham Road.
- 9.12 Hume's growth corridor has been developing faster than anticipated over the last 10 years. As a result of this very rapid development traffic volumes have increased exponentially in the Hume corridor such that the road network is failing to meet the safe and convenient travel needs of the community. Despite the State Government expanding the UGB and fast-tracking residential development there has been no State or Federal investment in any arterial roads north of Somerton Road. At the same time Council, with local developers, has delivered Aitken Boulevard (a new north-south arterial connection between Mount Ridley and Somerton roads) which has provided great additional benefit, along with a high number of connector roads.
- 9.13 Council has been advocating to the State Government to fund and prioritise the much needed duplication of Mickleham Road for the last decade. Despite Council's frequent efforts to highlight road traffic and safety issues in the Hume corridor, the State Government has still not provided a compelling commitment to duplicate Mickleham Road and provide a road network that reflects the growing resident population in Melbourne's northern growth area.
- 9.14 The Hume HIGAP Infrastructure and Delivery Strategy (IDS) adopted by Council in 2015 recommends Council advocate for Mickleham Road to be duplicated by 2021 and confirms "The duplication of Mickleham Road and Somerton Road by 2021 is fundamental to supporting on-going development in this (Craigieburn West) precinct". To manage this the IDS sets reasonable limits to new development based on corridor wide transport modelling assessments and using 20,000 vpd as the standard threshold for duplication of arterial roads.
- 9.15 The Craigieburn West PSP will allow approximately 8,200 additional dwellings and will further exacerbate the congestion on Mickleham Road resulting in development exceeding the projected limit of development by more than 30%. The draft PSP submission raises the overdue need for Mickleham Road to be upgraded and reiterates that Council does not support approval of the PSP until the State Government makes a budgeted commitment for the design and delivery to duplicate Mickleham Road from Donnybrook Road to Somerton Road.
- 9.16 It is noted that 10 submissions to the draft PSP from residents and affected parties urgently seek the upgrading of Mickleham Road.
- 9.17 In future submissions to a Standing Advisory Committee, Council's position will be insistent on delaying approval of the PSP without an associated budget commitment to the duplication of Mickleham Road between Somerton and Donnybrook roads that sees completion of works prior to or in tandem with development proposed in the PSP. However, in the past, Planning Panels have been unwilling to make recommendations where councils have sought views on State infrastructure delivery and the need to delay development due to issues with capacity of state infrastructure. Therefore, Council needs to continue its advocacy to the Ministers for Planning, Roads and Road Safety, Suburban Development as well as Hon. Ros Spence, Local Member for Yuroke.

Council Submission to Draft Craigieburn West

- 9.18 Council has been involved in the planning for the future of the Craigieburn West PSP since the development of the *Hume Corridor HIGAP* strategy in 2015. Over the last 18 months officers have worked with the VPA to ensure development of the site connects

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with surrounding areas, provides a linear open space network and protects native vegetation, particularly a large patch of trees immediately north of Craigieburn Road. Overall, the draft PSP incorporates these elements and furthers the objectives and outcomes sought within the *Hume Corridor HIGAP* Strategy reflective of the collaboration between Council and the VPA over the last couple of years.

9.19 Given this, the draft submission is on the whole highly supportive of the draft PSP. In particular, the draft submission gives a high level of support for the following elements of the PSP:

- A continuous linear park throughout the precinct supported by a range of other linear spaces and parks which will create desirable opportunities for walking and cycling in and around the precinct with links to the wider open space corridors throughout Craigieburn and Mickleham. During the COVID-19 shutdowns the valuable role and function played by linear open space has been emphatically demonstrated as the community have sought local opportunities to walk and cycle.
- the continuation of a bus capable north-south connector road from the Merrifield West and Lindum Vale PSPs which in combination with the continuous north south linear open spaces provides a range of sustainable and vehicle connectivity to the proposed new schools, community and recreation facilities and town centre.
- Integration with adjoining precincts by extending the roads to the east along a series of east-west connector boulevards (Grand, Brookfield, Marathon, Elevation and Fairways Boulevards) providing convenient car, bus, bike and walking connections to facilities in Craigieburn, including Craigieburn Town Centre and Craigieburn Train Station.
- Protection of scattered trees in local parks, including a 1.58 hectare area to the north of Craigieburn Road.
- Inclusion of a town centre located along connector streets (and not on Mickleham Road) and well-integrated with schools, active open space, linear parks and landscape features.
- Creation of a vision and urban structure that leverages the vegetation and landscape values of the site to create a strong identity and unique recreation opportunities for the Craigieburn West and wider communities.

9.20 The matters of greatest concern identified by officers are the following, and are fully detailed within the attached submission:

- Approval of the PSP prior to the budgeted commitment for the design and delivery to duplicate Mickleham Road from Donnybrook Road to Somerton Road (as discussed in detail above).
- Significant concerns with the Transport Impact Assessment only made available to officers for the first time in November 2020 (despite officers requests over the preceding 18 months). In particular, concerns with the reliance upon fundamentally flawed assumptions, such as the anticipated dwelling yield that is more than 2,000 lower than that proposed by the PSP document itself and that dwellings will be close to public transport – the nearest train station is some 5km to the east. Officers are concerned that this may make the proposed road network inadequate for the dwelling yield and land uses proposed in the PSP. To address this, the draft submission proposes a revised urban structure/transport plan to better address traffic issues through: additional access points onto surrounding arterial roads and changes to the internal road network as well as some changes to the location of key traffic generating land uses (refer Figure 1, page 11 and Table 1, page 13 in Attachment 3). This alternate plan is based on expert advice from Mesh Consultants and GTA Traffic engineers who were commissioned to work with Council officers and the VPA to improve the proposed transport network.

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- The PSP does not incorporate a concept plan for the integrated development of the Town Centre. Council's experience in the planning and development of greenfield town centres underlines the value of an overarching masterplan to guide coordinated development outcomes. Where this important middle step is missed and development is undertaken, site by site opportunities are missed to create both efficient outcomes (i.e. coordinated site access, consolidated car parking and loading areas) and good urban design outcomes (i.e. integrated and shared public realm outcomes; continuous building edge; pedestrian linkages; consolidated signage and landscaping). Officers submission has requested the amendment include planning provisions to require a concept plan or masterplan prior to permits being granted for development or subdivision in the Town Centre.
 - Officers have inspected areas within and surrounding the Biodiversity Conservation Area # 29 (BCS29). The draft submission recommends changes to its boundary to incorporate areas that are currently outside of the proposed conservation area but have some of the highest biodiversity values in lieu or areas (currently outside of the proposed conservation area) that have very low values. These changes have been discussed with the VPA, DELWP and the landowner who are all supportive of taking it further in seeking Federal approval. The reconfiguration of the BCS area also offers opportunities to relocate the sports reserve (SR01) to collocate it with BCS29. In the lead up to the Standing Advisory Committee process, officers will continue to work with affected parties on the final location and layout of SR01 and seeking to secure Federal approval for the proposed BCS29 changes prior to the SAC.
 - Officers consider that the design response in the PSP is based on a flawed expectation in the Kangaroo Management Plan (KMP) prepared to support the PSP that kangaroos can be eradicated from urban areas by displacing them into nearby green wedge areas. Kangaroo habitat will (appropriately for conservation values) be maintained in the Craigieburn West precinct within the BCS land, waterway corridors and other open space areas. The precinct also adjoins and is proximate to permanent sources of kangaroo habitat such as Greenvale reservoir, Mt Ridley conservation area, Aitken Creek and its tributaries and green wedge land running along the precincts 5.4km long western boundary. Given this, the KMP's solution to permanently exclude kangaroos from the precinct is considered unrealistic. Despite regular culling of kangaroo populations in urban areas, kangaroos continue to be observed in highly urbanised landscapes of Hume. Kangaroos are large mobile animals and will move through the open spaces, street networks and fine habitat of the precinct and adjoining areas. To address this, the draft submission supports the retention of waterway corridors in the precinct as a movement option for local kangaroo populations. In addition, the submission recommends design of local bridge culverts which support kangaroo movement and development is staged to ensure kangaroo populations do not become land locked.
 - Large old trees are a feature throughout the Craigieburn West precinct and are highly valued element of nearby developed suburbs. The draft submission is supportive of the large extent of trees that are shown to be retained but recommends the retention of 14 additional trees. This is considered an important, realistic and justified request based on the findings in the tree assessments in the Treetec Report commissioned to inform the PSP, officers ecological expertise and experience of how trees have been practically and successfully retained within a residential subdivision elsewhere, and will provide attractive landscape features that the future community will value.
- 9.21 The draft submission also seeks clarity or improved outcomes in relation to more minor matters, including: the design and process for the upgrading of the existing Whites Lane to urban standards; hydrology requirements; bushfire setback controls; and the drainage network.

REPORT NO: SU535 (cont.)

Submissions on the PSP

- 9.22 Over 40 submissions were received on the PSP including ten submissions raising the need for duplication of Mickleham Road.
- 9.23 Sixteen submissions have been made by landowners and developers who are likely to make lengthy and well-resourced submissions at the Standing Advisory Committee.
- 9.24 The challenge of the fragmented landownership in the PSP is represented strongly in these 16 submissions with many citing concerns about the impact of the proposed community and recreation facilities, open spaces, tree protection and drainage infrastructure constraining their capacity to achieve their desired dwelling yields .
- 9.25 Some of the developer/landowner submitters support the connector road and bridge infrastructure being included in the Craigieburn West ICP. As discussed above, adding further transport items in the ICP is likely to trigger the development of a supplementary ICP.
- 9.26 Given these competing interests there is likely to be pressure to make changes to the urban structures in the next stages of the PSP process. Officers will need to closely consider these in preparing Council's submission to the Standing Advisory Committee – see Nest Steps.

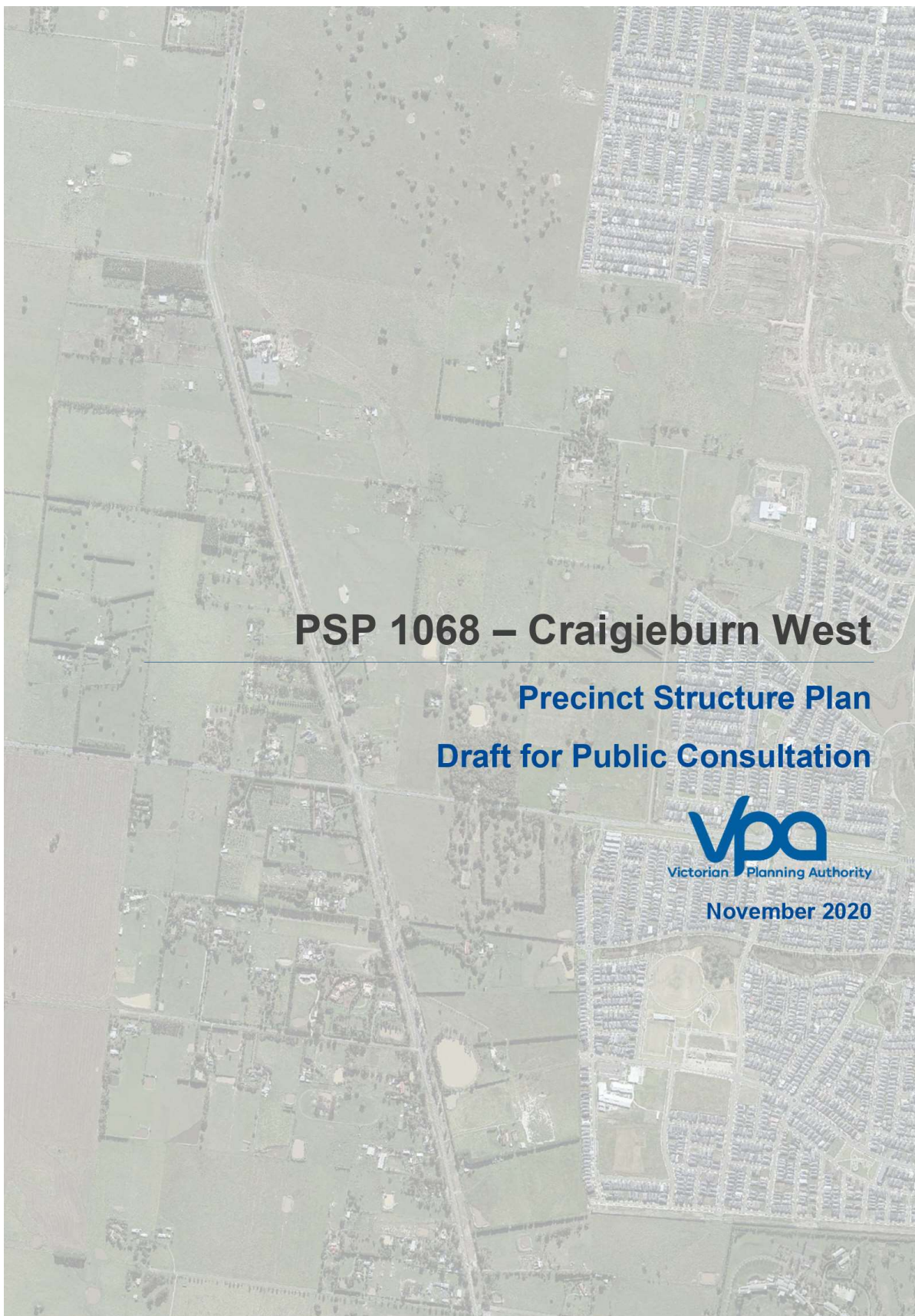
Next steps

- 9.27 The public consultation on the draft PSP effectively constituted the VPA giving notice of a 'draft Amendment'. Under the *VPA Projects Standing Advisory Committee Terms of Reference* the VPA is expected to "consider all submissions and where possible seek to resolve issues with submitters prior to referring matters to the Committee" prior to providing a letter of referral to the Committee Chair.
- 9.28 The VPA has now referred the 'draft Amendment' and submissions to the SAC. A key difference with the fast tracked process for the Craigieburn West PSP is that the VPA will only refer unresolved matters to the SAC who can then choose how it will deliberate - either by convening round table discussions, considering matters on the papers (written submissions) or through the conduct of a public hearing.
- 9.29 Officers have been advised that hearings will commence on 26 April 2021 and further details about this process will be made available through a direction hearing scheduled for 22 March 2021.
- 9.30 Officers will continue to seek resolution on important issues with affected landowners and government agencies, such as changes to the boundaries of the BCS; updated and accurate Transport Impact Assessment and a final drainage design and asset allocation. Other issues, which are unable to be resolved, will be put to the SAC for their consideration.
- 9.31 Whilst Council officers are generally supportive of the urban structure it is likely that individual parties will propose significant changes in making submission to the SAC. Officers will seek to maintain the Place Based Plan exhibited by the VPA along with changes proposed to the Transport Network and associated land use (Attachment 5) through the SAC process.
- 9.32 Once the hearings are completed, the SAC must submit its report for the VPA and Minister for Planning within 20 business days from the last hearing day in order for the formal planning scheme amendment to be prepared and submitted to the Minister for Planning for approval.

REPORT NO: SU535 (cont.)

10. CONCLUSION:

Thorough consideration of the draft *Craigieburn West Precinct Structure Plan* has resulted in an officer submission which is on the whole highly supportive of the PSP but highlights several key issues and offers suggestions for their resolution. Officers consider that these matters must be addressed prior to approval and adoption of the amendment by the VPA. Officers will make submissions to a Standing Advisory Committee on these matters on behalf of Council and will continue to seek their resolution with the VPA prior to an amendment being finalised and approved by the Minister for Planning.



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1 INTRODUCTION

The Craigieburn West Precinct Structure Plan (the PSP) has been prepared by the Victorian Planning Authority (VPA) with the assistance of Hume City Council, Government agencies, service authorities and major stakeholders.

The PSP is a long-term plan for urban development. It describes how the land is expected to be developed, and how and where services are planned to support the development of new communities. The PSP is a set of decisions about how the land is to be developed, and it:

- Sets out plans to guide the delivery of quality urban environments in accordance with Victorian Government guidelines and policies.
- Enables the transition of non-urban land to urban land.
- Sets the vision for how the land should be developed and the outcomes to be achieved.
- Outlines the projects required to ensure that future residents, visitors and workers within the area can be provided with timely access to services and transport necessary to support a quality and affordable lifestyle.
- Sets out objectives, requirements and guidelines for land use, development and subdivision.
- Provides Government agencies, the Council, developers, investors and local communities with certainty about future development.
- Addresses the requirements of the *Environment Protection and Biodiversity Conservation Act 1999* (EPBC Act 1999) through Federal approval of the Biodiversity Conservation Strategy and Sub Regional Species Strategies for Melbourne's Growth Areas (September 2013).

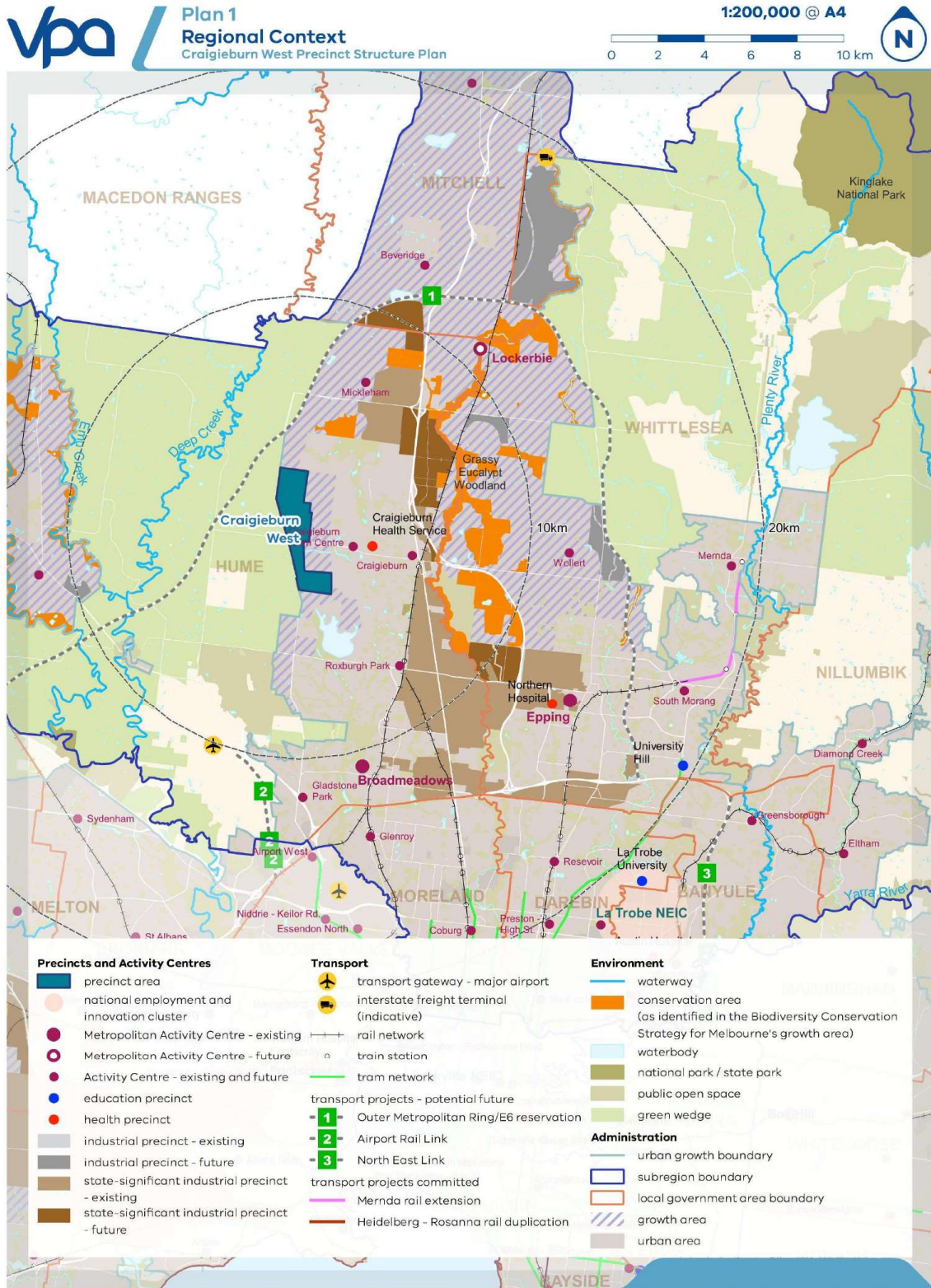
The PSP is informed by:

- Plan Melbourne – Metropolitan Planning Strategy, May 2017
- The Growth Corridor Plans: Managing Melbourne's Growth (Growth Areas Authority, June 2012)
- The Planning Policy Framework as set out in the Hume Planning Scheme
- The Local Planning Policy Framework as set out in the Hume Planning Scheme
- The Biodiversity Conservation Strategy and Sub Regional Species Strategies for Melbourne's Growth Areas (Department of Environment and Primary Industries, June 2013)
- The Precinct Structure Planning Guidelines.

The following documents have been developed in parallel with the PSP to inform and direct the future planning and development of the Precinct:

- The Craigieburn West Infrastructure Contributions Plan (ICP) requires development proponents to contribute toward infrastructure required to support the development of the Precinct.
- The Background Technical Studies undertaken to inform the preparation of the Craigieburn West PSP.

In preparing this PSP, the VPA has worked closely with Hume City Council, Melbourne Water, Department of Transport, DELWP and land owners.



1.1 How to read this document

This PSP guides land use and development where a planning permit is required under Schedule 12 to the Urban Growth Zone (Clause 37.07 of the Hume Planning Scheme), or any other provision of the Hume Planning Scheme that references this PSP.

A planning application and subsequent planning permit must implement the outcomes of the PSP. The outcomes are expressed as the VISION AND OBJECTIVES.

Each element of the PSP contains requirements and guidelines as relevant.

Requirements must be adhered to in developing the land. Where they are not demonstrated in a permit application, requirements will usually be included as a condition on a planning permit whether or not they take the same wording as in the structure plan. A requirement may reference a plan, table or figure in the structure plan.

Guidelines express how discretion will be exercised by the responsible authority in certain matters that require a planning permit. If the responsible authority is satisfied that an application for an alternative to a guideline, implements the outcomes, the responsible authority may consider the alternative. A guideline may include or reference a plan, table or figure in the PSP.

Meeting these Requirements and Guidelines will implement the vision of the PSP.

Conditions that must be included in a planning permit are outlined in Schedule 12 to the Urban Growth Zone (UGZ) in the Hume Planning Scheme.

Development must also comply with other Acts and approvals where relevant e.g. the Environment Protection and Biodiversity Conservation Act 1999 in the case of biodiversity or the Aboriginal Heritage Act 2006 in the case of cultural heritage, amongst others.

Not every aspect of the land's use, development or subdivision is addressed in this structure plan. A responsible authority may manage development and issue permits as relevant under its general discretion.

1.2 Infrastructure contributions plan

Development proponents within Craigieburn West Precinct will be bound by the Craigieburn West Infrastructure Contribution Plan (the ICP).

The ICP sets out requirements for infrastructure funding across the Craigieburn West Precinct.

The ICP will be incorporated in the Hume Planning Scheme.

Development proponents wishing to commence works prior to incorporation of this ICP may enter into agreements with Hume Council under Section 173 of the Planning and Environment Act 1987 to expedite contributions.

1.3 Background information

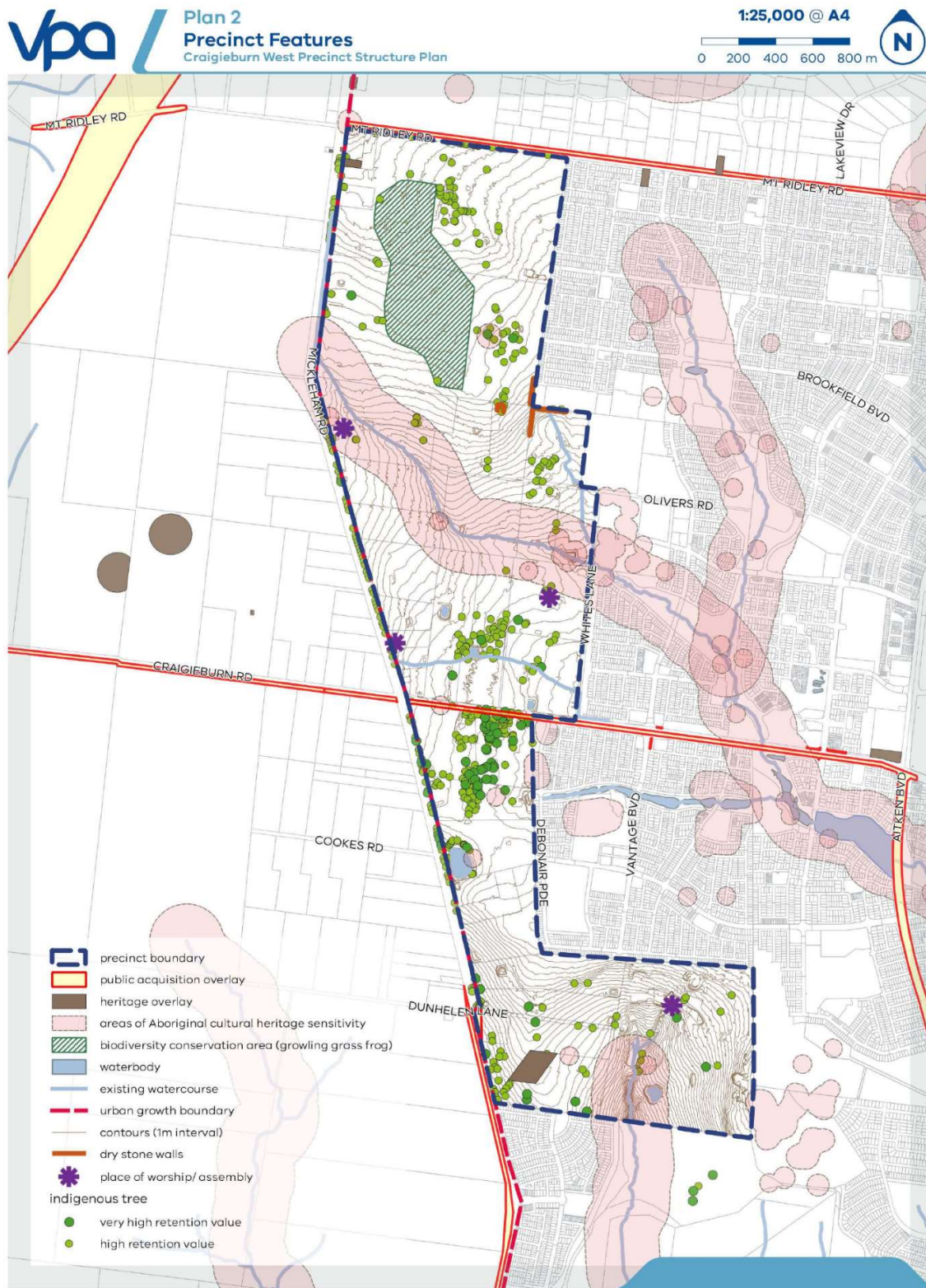
The Craigieburn West PSP Background Report provides detailed background information relating to the precinct, including its local and metropolitan context, history, landform and topography, biodiversity, drainage, open space, transport infrastructure, employment and community facilities. The report also summarises various background technical studies that have informed the preparation of the PSP Land to which this PSP applies.

1.4 Land to which this PSP applies

The land to which this PSP applies is shown on Plan 2 and on the Hume Planning Scheme Maps as Schedule 12 to the Urban Growth Zone.

The PSP applies to approximately 562 hectares of land generally bound by Mt Ridley Road to the north, the Craigieburn R2 PSP area to the east, the Greenvale North R1 PSP area to the south and Mickleham Road to the west. The precinct benefits from the strategic planning work undertaken by surrounding PSP areas which will allow for a seamless integration of the future communities.

The precinct is bisected by Craigieburn Road running east/west and the Aitken Creek running north-west/south-east. Notable features of the precinct in addition to Aitken Creek include established tracts of native vegetation, including Biodiversity Conservation Area 29, and views to external landscape features including remnant volcanic cones and hilltops.



2 OUTCOMES

2.1 Vision

Craigieburn West will develop as a series of predominantly residential neighbourhoods supported by a local Town Centre and adjoining residential areas.

The precinct will leverage its unusual linear form by creating a series of walkable neighbourhoods arranged along a north–south spine comprising open space links and key road connections. The precinct will also seek to embed heritage and landscape features within and around it by capitalising on opportunities to maximise views to nearby volcanic cones and integration with established native vegetation.

The central spine will support the primary place-making focus - creating energy and activation. The PSP features schools, community hubs, and diverse housing typologies linked with a range of open spaces, including conservation reserves, active open space and a network of local parks.

The PSP will complete the structure planning process for the area, completing the delivery of green links within and beyond the PSP boundaries and provision of a sensitive built form interface to rural land west of Mickleham Road/Urban Growth Boundary.

The PSP will complete the catchment to surrounding activity centres external to the PSP, including Craigieburn Central, Aston Village and Highlands Village, while also providing for local facilities, including a centralised activity centre co-located with open space and community facilities, and a series of proposed government and potential non-government schools.

2.2 Purpose

The purpose of the PSP is to embed the vision for the Craigieburn West precinct through the preparation of an orderly and integrated place based spatial plan. To this end, the PSP will:

- Support the timely delivery of integrated transport options, including public and active transport. Active transport is facilitated via a north south green 'spine' linear reserve.
- Support the retail and services catchment of the proposed Local Town Centre, Craigieburn Central and town centres proposed in Aston Village and Highlands Village.
- Facilitate the final drainage outcomes for to the development services scheme for the Aitken Creek, Yuroke Creek and Upper Brodies Creek catchments, including the protection of the Greenvale Reservoir.
- Integrate the significant areas of vegetation and biodiversity into the future urban landscape and open space network and promotes connections to greenspace.
- Integrate with and deliver community facilities and open spaces to complement the adjoining precincts of Lindum Vale PSP, Craigieburn R2 PSP and the Greenvale North PSP.

2.3 Objectives

The following objectives describe the desired outcomes of the precinct's development and guide the implementation of the vision:

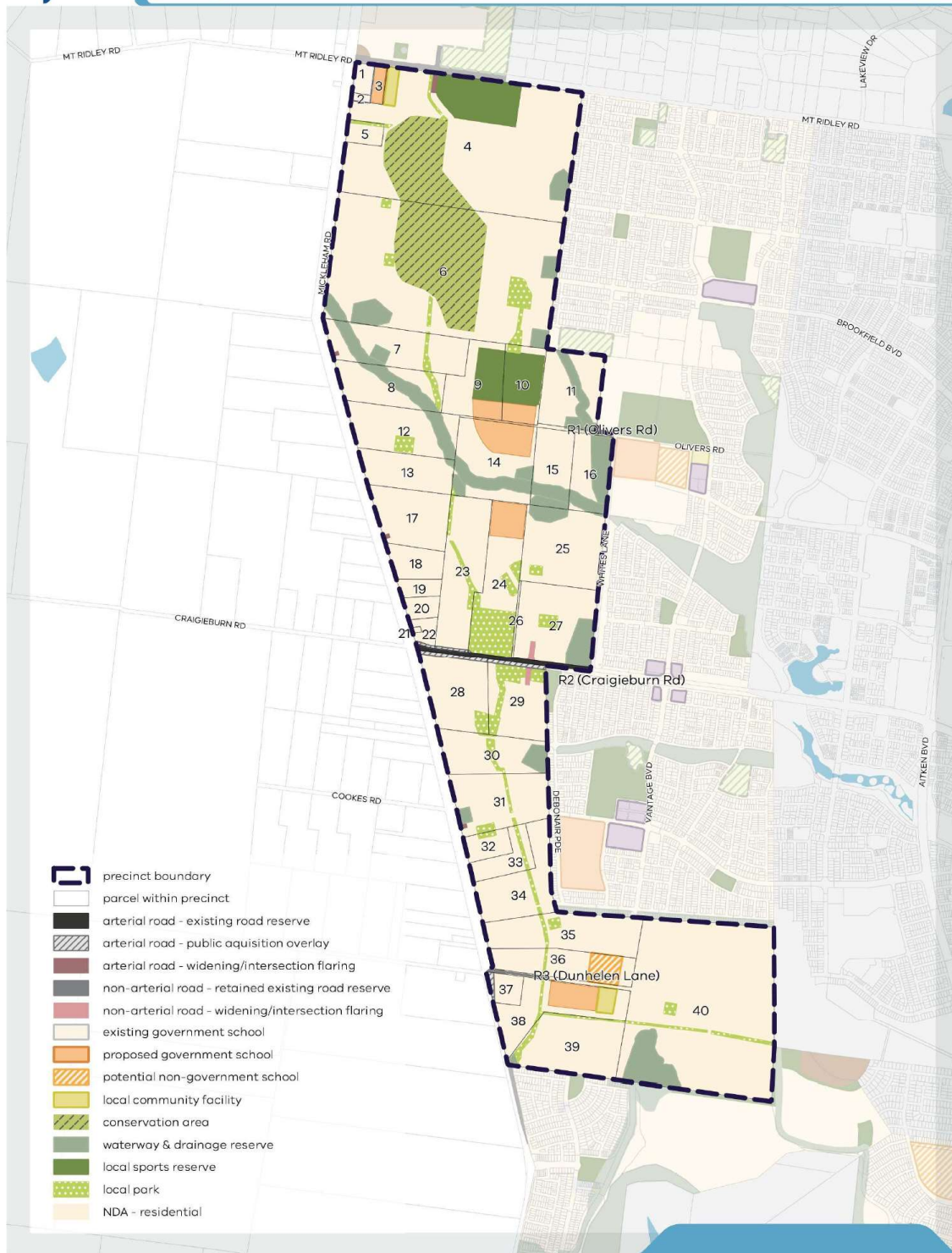
PSP OBJECTIVES	
01	Housing, subdivision & built form To facilitate housing diversity and choice within Craigieburn West, including densities that support access to local services, jobs and sustainable transport options.
02	Transport & movement To facilitate 20-minute neighbourhoods by providing a transport network that integrates with the adjoining established areas and supports active and public transport options, movement of goods and connections to jobs within Craigieburn West and the surrounding areas.
03	Public realm, open space & heritage To provide a framework for a high amenity and integrated urban environment within Craigieburn West that encourages a sense of place and community, as well as responds to the existing natural, cultural and built form features.
04	Water, utilities & safety To facilitate safe, resilient and water sensitive urban environments in Craigieburn West that respond to climate change, bushfire management and final drainage outcomes including the protection of the Greenvale Reservoir Drinking Water Catchment.
05	Biodiversity & ecosystems To facilitate the retention and protection of Conservation Area 29 and landscape features within Craigieburn West including scattered trees and waterways as key community assets that are integrated with the urban landscape.
06	Education & community infrastructure To identify and facilitate the delivery of adaptable and multi-purpose open spaces, community facilities, schools, and other essential community infrastructure to support development.
07	Centres, employment & economic activity To facilitate investment in an innovative and vibrant local and regional economy within a network of highly accessible activity and employment centres that support jobs and business activity for residents in Craigieburn West and surrounding areas.
08	Precinct infrastructure delivery To identify and guide the timely delivery and staging of key essential infrastructure required for Craigieburn West.



Plan 3
Land Use Budget
Craigieburn West Precinct Structure Plan

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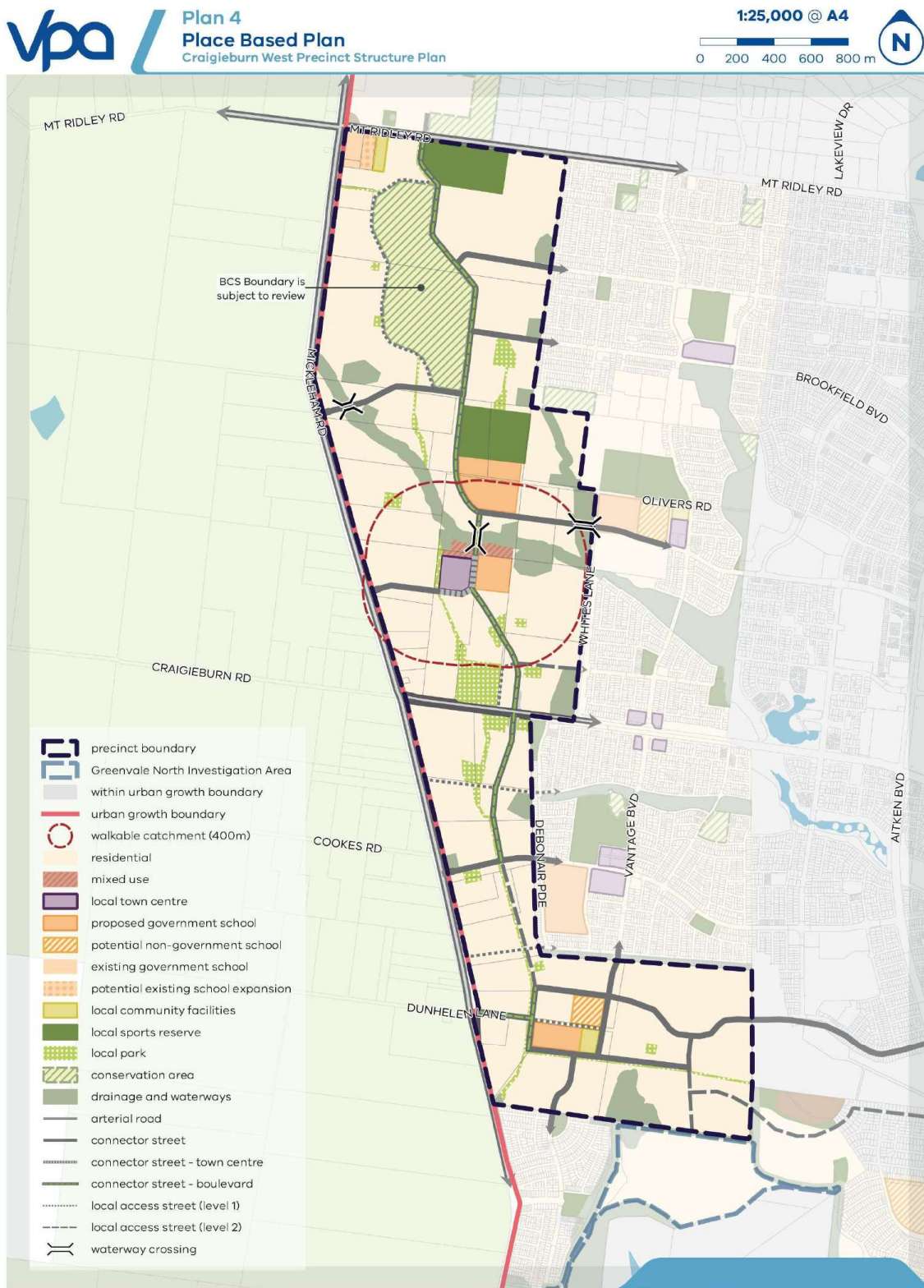


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2.4 Precinct land use budget

Table 1: Precinct land use budget

Description	HECTARES	% OF TOTAL	% OF NDA
TOTAL PRECINCT AREA (ha)	562.34		
Transport			
Arterial road – existing road reserve	1.87	0.33%	0.45%
Arterial road – public acquisition overlay	2.16	0.38%	0.52%
Arterial road – new / widening / intersection flaring (ICP land)	0.73	0.13%	0.18%
Non-arterial road – retained existing road reserve	0.66	0.12%	0.16%
Non-arterial road – new / widening / intersection flaring (ICP land)	0.79	0.14%	0.19%
Sub-total transport	6.20	1.1%	1.50%
Community & education			
Existing government school	2.04	0.36%	0.49%
Government school	16.85	3.00%	4.07%
Potential non-government school	2.50	0.44%	0.60%
Local community facility (ICP land)	2.40	0.43%	0.58%
Sub-total education	23.80	4.2%	5.7%
Open space			
Service open space			
Conservation reserve	37.70	6.70%	9.10%
Waterway and drainage reserve	38.55	6.85%	9.30%
Sub-total service open space	76.24	13.56%	18.40%
Credited open space			
Local sports reserve (ICP land)	19.00	3.4%	4.59%
Local network park (ICP land)	22.78	4.1%	5.50%
Sub-total credited open space	41.78	7.4%	10.08%
Total all open space	118.02	21.0%	28.49%
TOTAL NET DEVELOPABLE AREA – (NDA) Ha	414.31	73.68%	
NET DEVELOPABLE AREA – RESIDENTIAL (NDAR) Ha	414.31	73.68%	
NET DEVELOPABLE AREA – EMPLOYMENT (NDAE) Ha	0.00	0.00%	



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3 IMPLEMENTATION & DELIVERY

3.1 Housing, subdivision & built form

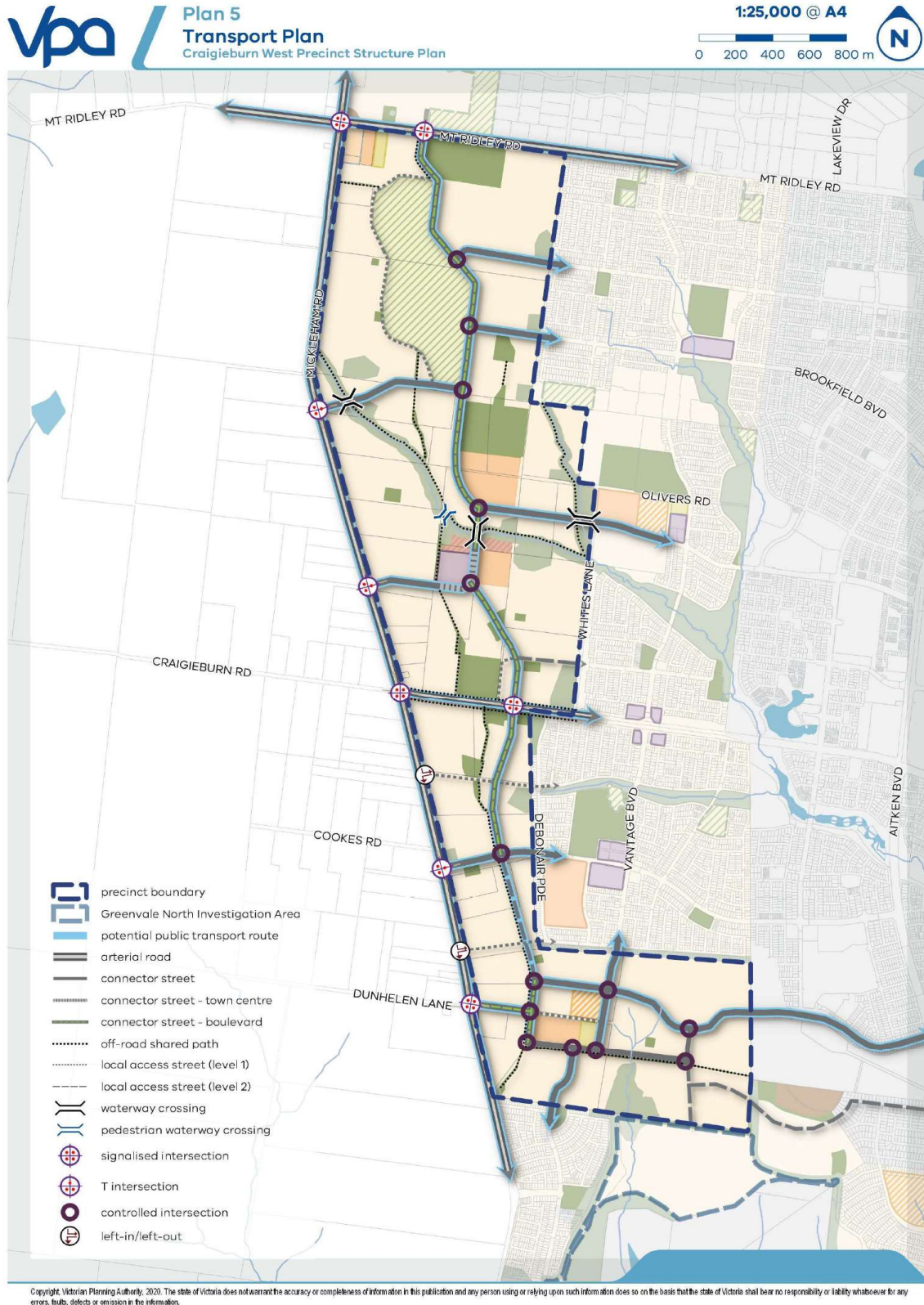
REQUIREMENTS	
R1	Subdivision layouts, lot diversity and housing typologies must respond to the natural and existing built features of the surrounding developed area, including (but not limited to): <ul style="list-style-type: none"> • Topographical features; • Aitken Creek and Yuroke Creek tributaries; and • Rural landscape interface west of old Mickleham Road.
R2	Subdivision must provide a diverse neighbourhood character by providing a range of lot sizes and dwelling types in appropriate locations throughout the Precinct, including achieving minimum average densities and planned neighbourhood character as specified in Plan 4 and Table 2.
R3	In order to reorient school buildings and access away from Mickleham Road and Mt Ridley Road, subdivision adjacent to the existing Mickleham Primary (and the proposed expansion) as indicated on Place Based Plan (Plan 4) must provide an internal subdivision layout which facilitates access to the School from the internal road network. Local streets adjacent to the school must provide a road easement wide enough to allow for school bus movement while accommodating on-street parking and two way traffic movement in accordance with the the Department of Transport's guidance for public transport and land use development.
R4	Development along Mickleham Road and Mt Ridley Road must provide a sensitive rural interface through design treatments, which include a landscaped nature strip between the row of housing and road reservation.
R5	An application for subdivision of land into residential lots or development of land for residential or mixed-use purposes must provide affordable housing as defined by the Planning & Environment Act 1987 to the satisfaction of the responsible authority

GUIDELINES	
G1	Subdivisions that retain lots around existing dwellings should be designed to ensure that the future subdivision of retained lots will appropriately integrate with the surrounding subdivision layout.
G2	Subdivision layouts and development should respond to and address the relevant elements of the Urban Design Guidelines for Victoria.
G3	Lots should front (in order of priority where a lot fronts multiple elements): <ul style="list-style-type: none"> • Public open space. • Local access streets. • Connector roads. • Arterial roads.
G4	Applications for residential subdivision or development should provide an equivalent of up to 10% of the total number of dwellings forecast to be provided (and may be provided as constructed dwellings or land or otherwise). The affordable housing should: <ul style="list-style-type: none"> • be provided within walkable catchments where practicable; • provide for a range of housing typologies to meet demonstrated local need; and provide for very low, low, and moderate income households
G5	Subdivision should deliver a broad mix of lots that are an appropriate size and shape to support the planned neighbourhood character of the precinct, as specified in Table 2, by: <ul style="list-style-type: none"> • Providing a range of lot sizes, widths, depths and densities • Providing higher residential densities and more intensive building typologies in locations where they will: <ul style="list-style-type: none"> ○ Support the viability and vibrancy of activity centres, access to community infrastructure and amenities. ○ Have good access to public transport and support walking and cycling.

	<ul style="list-style-type: none"> ○ Make a positive impact to planned neighbourhood character and identity through the incorporation of remnant vegetation (where indicated in Plan 10).
G6	<p>Subdivision should provide for a street separating development from waterways, sporting reserves and local parks and the linear reserve.</p> <p>Where subdivision does not propose a local street separating development, design and layout options should demonstrate:</p> <ul style="list-style-type: none"> • Lots directly fronting open space and landscape value areas should be set back at least 4.0 metres from the waterway corridor and open space. • Lots directly fronting open space should allow for vehicular access via a rear laneway. • A “paper road” should be provided as the primary point of access from a footpath or shared path with a minimum width of 1.5 metres along the lot frontage. • Subdivision design should avoid side or rear fence treatments fronting open space. • Subdivision design should maximise opportunities for informal passive surveillance. • Subdivision design should not limit the use of adjacent open space. <p>All to the satisfaction of the responsible authority and Melbourne Water, where adjacent to a waterway.</p>
G7	<p>Subdivision applications for super-lots identified for future medium density, high density, or integrated housing should demonstrate:</p> <ul style="list-style-type: none"> • Expected dwelling density in line with Table 2. • Connections and active interfaces with adjacent streets, open space and waterways. • Safe and effective internal vehicle and pedestrian circulation. • Indicative treatments for interfaces with non-residential land uses.
G8	<p>Specialised housing forms, such as retirement living, or aged care should:</p> <ul style="list-style-type: none"> • Respond to and integrate with adjoining development, avoiding inactive interfaces and blank facades to the public street network. • Be located within the walkable catchment shown on Plan 4. • Be accessible by public transport. • Not present a barrier to movement through the surrounding road and pedestrian movement network.
G9	<p>Any retaining structures within public and private spaces (except for those which are part of a building) should be:</p> <ul style="list-style-type: none"> • No more than 1.0 metres in height between a dwelling and a street or public space, or where visible from a street or public space. • Set back at least 1.0 metres from any building envelope. • Staggered, with a minimum 0.75 metre distance between each stagger to allow for the inclusion of landscaping where cutting and filling is deeper than 1.0 metres. • Positioned so that associated drainage infrastructure and structural foundation are fully located within the same lot. <p>Please note: Melbourne Water will not accept ownership or maintenance responsibilities associated with retaining structures in land to be vested to them.</p>

Table 2: Housing density guide and planned neighbourhood character

DEVELOPMENT AREA	PLANNED NEIGHBOURHOOD CHARACTER	MINIMUM AVERAGE DENSITY (DW/NDHA)
Standard residential outside walkable catchment	Development will have a traditional suburban neighbourhood character characterised by buildings up to three storeys in height. Housing will generally comprise detached and semi-detached typologies. However, more intensive forms of development such as terraced homes and townhouses should be provided in proximity to areas of high amenity, or where it can be demonstrated that a positive contribution will be made to neighbourhood character and identity.	18.5
Residential within walkable catchment	Development will have an urban neighbourhood character, characterised by buildings up to four storeys in height. Housing will comprise a variety of typologies, including low-rise apartments buildings, terraced homes and townhouses (including rear-loaded product), and detached dwellings.	26.5



3.2 Transport & movement

3.2.1 Public transport

REQUIREMENTS	
R6	Any road nominated in Plan 5 as a potential public transport route must be constructed (including partial construction where relevant) in accordance with the corresponding cross section in the PSP and the Department of Transport's guidance for public transport and land use development.

GUIDELINES	
G10	Bus stop facilities should be designed as integral parts of the town centre and activity generating land uses such as schools, sports fields and employment areas.
G11	The street network should be designed to ensure all households have direct and convenient walking access to public transport services.

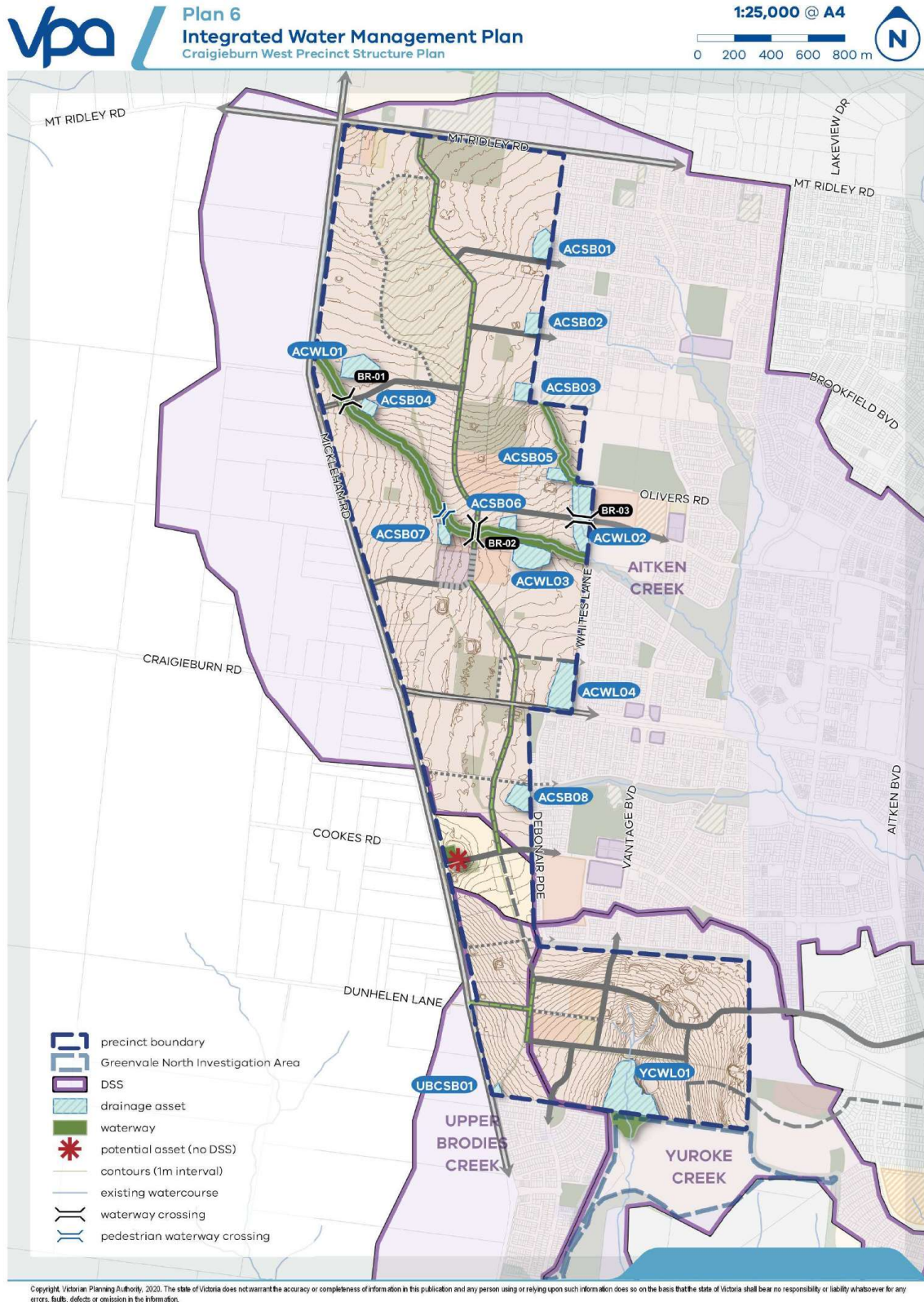
3.2.2 Walking & cycling

REQUIREMENTS	
Development of the linear park as shown on Plan 5 and Plan 8 must:	
<ul style="list-style-type: none"> • Provide for a shared path (pedestrian and cycling) which is interconnected with the surrounding footpath and cycle network. • Be designed and located to provide efficient movement of pedestrians and cyclists. • Provide for pedestrian and cyclist priority over vehicular traffic. • Ensure that where a road crosses the linear park, the road is raised with priority given to the linear park. • Utilise the cross section designs in Appendix 4.5. • Have a standard minimum width of 15m or 10m where adjacent to a connector road, unless otherwise agreed to by responsible authority. 	

GUIDELINES	
G12	Location of walkways or pedestrian and cycle paths in addition to those described through the standard cross sections should consider the need for appropriate lighting and passive surveillance.
G13	The alignment of dedicated off-road bicycle paths should be designed for cyclists travelling up to 30km/hr.
G14	In addition to waterway crossings shown on Plan 5, development proponents should provide waterway crossings at intervals no greater than 400m or corresponding with all perpendicular through roads or pedestrian and cycle paths.
G15	High quality walking and cycling links are encouraged to connect destinations within and adjoining the PSP that are not directly serviced by the linear park.

3.2.3 Street network

REQUIREMENTS									
R7	Design of all subdivisions and streets must provide: <ul style="list-style-type: none"> A permeable, direct and safe street network prioritising walking and cycling. Safe and convenient crossing points of connector roads and local streets at all intersections and on key desire lines as well as crossing waterways. Safe pedestrian crossings of arterial roads at all intersections, at key desire lines, and on regular intervals appropriate to the function of the road and public transport provision. Safe and convenient transition between on- and off-road bicycle networks. Convenient access to regional and local points of interest and destinations for effective integration with neighbouring properties, parkland and sports reserves. Direct and convenient walking access to public transport services. 								
R8	Vehicle access to lots fronting arterial roads must be provided from the local internal loop road or rear lane, to the satisfaction of the Road Authority.								
GUIDELINES									
G16	Slip lanes should be avoided in areas of high pedestrian activity (including schools and the Local Town Centre) and only provided at intersections between connector streets and arterial roads where they are necessitated by high traffic volumes but with pedestrian priority crossings.								
G17	Culs-de-sac should not detract from convenient pedestrian and vehicular connections.								
G18	The frequency and impact of vehicular crossovers on verges of connector roads should be minimised by applying a combination of: <ul style="list-style-type: none"> Rear loaded lots with laneway access. Vehicular access from the side streets. Combined or grouped crossovers. Increased lot widths. 								
G19	All signalised intersections should be designed having regard to the Department of Transport (DOT) working document <i>Guidance for Planning Road Networks in Growth Areas</i> November 2015 (as updated), to the satisfaction of The Head, Transport for Victoria and the responsible authority.								
G20	Street trees should be provided on both sides of all roads and streets (excluding laneways) at regular intervals appropriate to tree size at maturity, unless otherwise agreed by the responsible authority. <table border="1"> <thead> <tr> <th>Average interval</th><th>Tree size</th></tr> </thead> <tbody> <tr> <td>8–10 metres</td><td>Small (less than 10 metre canopy)</td></tr> <tr> <td>10–12 metres</td><td>Medium (10–15 metre canopy)</td></tr> <tr> <td>12–15 metres</td><td>Large (canopy larger than 15 metres)</td></tr> </tbody> </table> <p>The design and siting of street trees should address relevant council policies & guidelines.</p>	Average interval	Tree size	8–10 metres	Small (less than 10 metre canopy)	10–12 metres	Medium (10–15 metre canopy)	12–15 metres	Large (canopy larger than 15 metres)
Average interval	Tree size								
8–10 metres	Small (less than 10 metre canopy)								
10–12 metres	Medium (10–15 metre canopy)								
12–15 metres	Large (canopy larger than 15 metres)								
G21	A variety of road cross sections should be utilised in a subdivision layout to create differentiation and neighbourhood character. Alternative cross sections should ensure that: <ul style="list-style-type: none"> Minimum required carriageway dimensions are maintained to ensure safe and efficient operation of emergency vehicles on all streets as well as buses on connector streets. The performance characteristics of standard cross sections as they relate to pedestrian and cycle use are maintained. Relevant minimum road reserve widths for the type of street are maintained, unless otherwise approved by the responsible authority. 								



3.3 Water, utilities & bushfire safety

3.3.1 Integrated water management

REQUIREMENTS	
R9	Development must give effect to relevant policies and strategies being implemented by the responsible authority, Melbourne Water and Yarra Valley Water, including the Healthy Waterways Strategy and any approved integrated water management plan.
R10	Stormwater conveyance and treatment (including interim solutions) must be designed to avoid or mitigate the risk of erosion from sodic/dispersive soils and in accordance with the relevant Development Services Scheme and Plan 6 to the satisfaction of Melbourne Water and the responsible authority. Note: this may result in variation to the Melbourne Water DSS as shown on Plan 6.
R11	Final designs and boundaries of constructed wetlands, retarding basins, stormwater quality treatment infrastructure, and associated paths, boardwalks, bridges, and planting, must include appropriate treatments to provide protection for dispersive soils where these are present and be designed to the satisfaction of both Melbourne Water and the responsible Authority.
R12	Development staging must provide for the delivery of ultimate waterway and drainage infrastructure, including stormwater quality treatment. Where this is not possible, development proposals must demonstrate how any interim solution adequately manages and treats stormwater generated from the development and how this will enable delivery of an ultimate drainage solution, to the satisfaction of Melbourne Water and the responsible authority.
R13	Stormwater runoff from the development must meet the performance objectives of the <i>CSIRO Best Practice Environmental Management Guidelines</i> for Urban Stormwater prior to discharge to receiving waterways and as outlined on Plan 6, unless otherwise approved by Melbourne Water and the responsible authority. Proposals that exceed the performance objectives are highly encouraged and can be considered, to the satisfaction of the relevant authorities.
R14	The design and layout of connector street network and open spaces (including linear links) must ensure the long-term viability of vegetation (especially existing mature River Red Gums) and optimise water use efficiency through the use of overland flow paths and stormwater harvesting for passive irrigation and Water Sensitive Urban Design initiatives.
R15	Applications must demonstrate, through the preparation of Integrated Water management Plans: <ul style="list-style-type: none"> Waterways and integrated water management design enables land to be used for multiple recreation and environmental purposes. Overland flow paths and piping within road reserves will be connected and integrated across property/parcel boundaries. Melbourne Water and the responsible authority freeboard requirements for overland flow paths will be adequately contained within the road reserves. Relevant Integrated Water Management (IWM) requirements of this PSP will be achieved to the satisfaction of the retail water authority, including the supply of recycled water where required by the relevant water authority.

GUIDELINES	
G22	Where practical, development should include integrated water management initiatives to reduce reliance on potable water and increase the utilisation of storm and wastewater contributing to a sustainable urban environment.
G23	Where practical, integrated water management systems should be designed to: <ul style="list-style-type: none"> Maximise habitat values for local flora and fauna species. Enable future harvesting and/or treatment and re-use of stormwater. Protect and manage MNES values, particularly within conservation areas, in relation to water quality and suitable hydrological regimes (both surface and groundwater). Recognise and respond to Aboriginal cultural heritage significance.
G24	The design and layout of roads, road reserves, and public open space should optimise water use efficiency and long-term viability of vegetation and public uses through the use of overland flow paths,

	Water Sensitive Urban Design initiatives such as street swales, rain gardens and/or locally treated storm water for irrigation to contribute to a sustainable and green urban environment.
G25	Where practical, and where primary waterway or conservation functions are not adversely affected, land required for integrated water management initiatives should be integrated with the precinct open space and recreation system and as depicted on Plan 6 and Table 3.
G26	Site specific Integrated Water Management initiatives are encouraged in medium density, commercial and public use developments.
G27	Drainage infrastructure should be designed to avoid and mitigate the impact of earthworks on the health and viability of retained river red gums.
G28	Subdivision and development in areas identified as being affected by sodic and dispersive soils should be managed to avoid or mitigate the potential risk of erosion, both in the master planned design response to the subdivision, during construction phase, and on an ongoing basis.
G29	<p>Stormwater runoff in areas identified as being affected by sodic and dispersive soils should be designed to manage the potential risk of erosion.</p> <p>Potential management methods may include but not limited to:</p> <ul style="list-style-type: none"> • Widening the buffer distances between the core riparian zone and the outside vegetated buffers that allows sufficient tolerances for channel migration. • Diversion of water away from sodic and dispersive materials. • Minimising potential convergence and/or ponding of surface flows. • Compacting to reduce pore spaces and minimise water movement through material. • Physical and chemical soil ameliorants. • Maintenance of topsoil across undisturbed land, preferably with grasses to provide surface soil stability and root anchorage. • Minimise the amount of time land is exposed (e.g. by staging development). • Ensure that culverts and drains excavated into dispersive subsoils are capped with non-dispersive topsoil, gypsum stabilised and vegetated.

3.3.2 Utilities

REQUIREMENTS	
R16	All existing above ground electricity cables (excluding substations and cables with voltage 66kv or greater) must be placed underground as part of the upgrade of existing roads or subdivision works.
R17	All new electricity supply infrastructure (excluding substations and cables with voltage 66kv or greater) must be provided underground.
R18	Above ground utilities must be identified at the subdivision design stage to ensure effective integration with the surrounding neighbourhood, to minimise amenity impacts and be designed to the satisfaction of the Relevant Authority. Where that infrastructure is intended to be located in public open space, the land required to accommodate that infrastructure will not be counted as contributing to public open space requirements specified and will be additional to the areas designated in Table 4.
R19	Utilities and other infrastructure must avoid traversing Conservation Area 29.

GUIDELINES	
G30	The delivery of underground services should be coordinated, located and bundled (utilising common trenching) to maintain the cross section widths of pedestrian paths and nature strips as shown in the PSP and to facilitate trees and other planting within road reserve.
G31	Utilities should be placed outside of conservation areas, natural waterway corridors or on the outer edges of these corridors in the first instance. Where services cannot avoid crossing or being located within a conservation area or natural waterway corridor, they must be located to avoid disturbance to existing waterway values, native vegetation, significant landform features and heritage sites, to the satisfaction of Melbourne Water and the responsible authority.
G32	All new above-ground utilities, including temporary utilities, should be located outside of key view lines and screened with vegetation, as appropriate.
G33	Trunk services should be placed along general alignments as advised by the relevant servicing authorities.
G34	Design and location of underground services should be guided by Appendix 4.6.
G35	Utility easement to the rear of lots should only be provided where there is no practical alternative.

Table 3: Water infrastructure

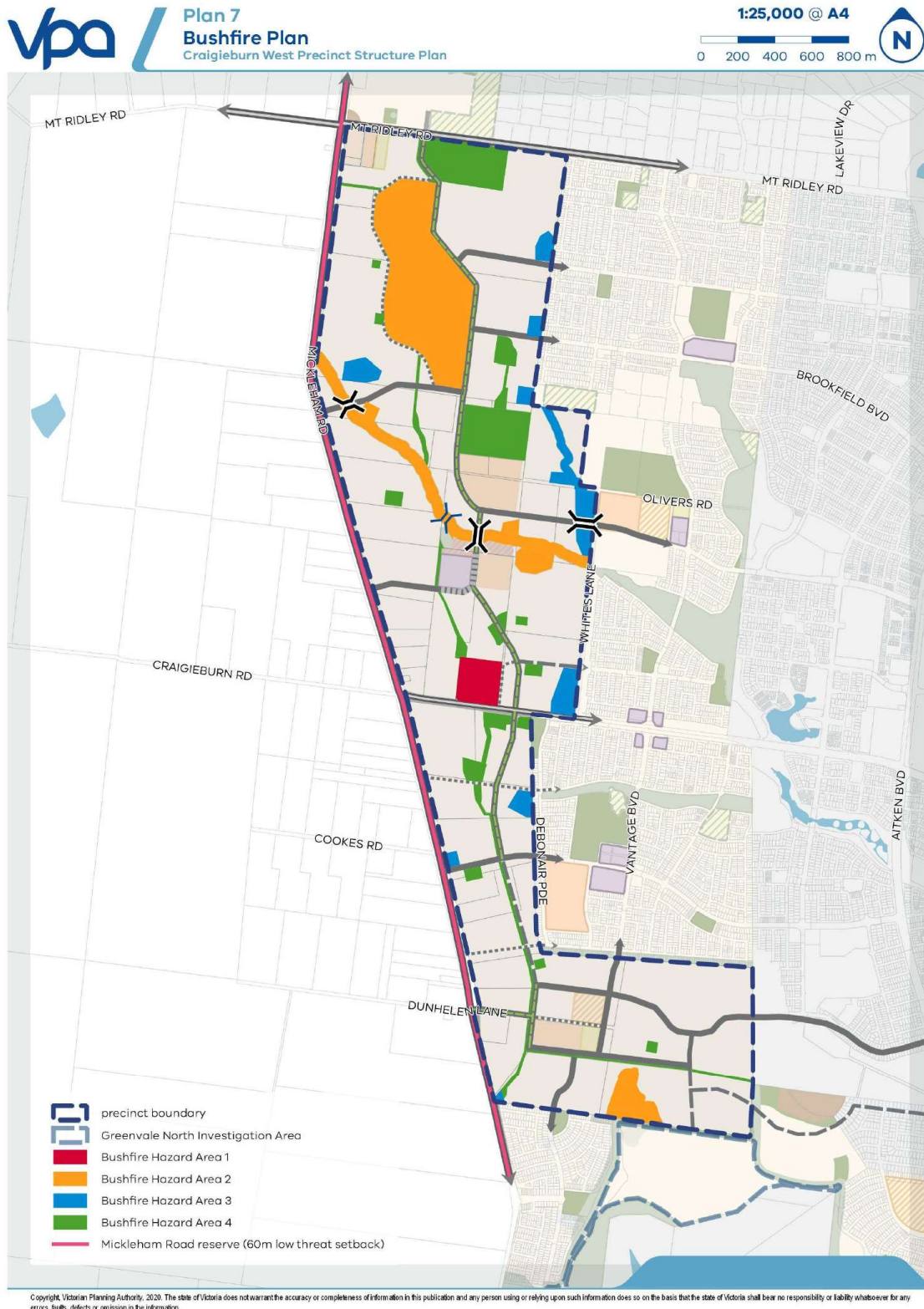
Drainage Scheme	Asset ID	Asset Type	Responsibility	Area (ha)
Aitken Creek	ACSB-01	Sediment Basin	TBC – Council/MWC	1.36
Aitken Creek	ACSB-02	Sediment Basin	TBC – Council/MWC	0.89
Aitken Creek	ACSB-03	Sediment Basin	TBC – Council/MWC	0.82
Aitken Creek	ACSB-04	Sediment Basin	TBC – Council/MWC	0.60
Aitken Creek	ACSB-05	Sediment Basin	TBC – Council/MWC	0.59
Aitken Creek	ACSB-06	Sediment Basin	TBC – Council/MWC	0.56
Aitken Creek	ACSB-07	Sediment Basin	TBC – Council/MWC	0.59
Aitken Creek	ACSB-08	Sediment Basin	TBC – Council/MWC	1.43 [^]
Aitken Creek	ACWL-01	Wetland	TBC – Council/MWC	2.15
Aitken Creek	ACWL-02	Wetland	TBC – Council/MWC	1.46*
Aitken Creek	ACWL-03	Wetland	TBC – Council/MWC	2.21
Aitken Creek	ACWL-04	Wetland	TBC – Council/MWC	2.92
SUB TOTAL				15.59
Upper Brodies Creek	UBCSB-01	Sediment Basin	Council	0.22
SUB TOTAL				0.22
Yuroke Creek	YCWL-01	Wetland/Retarding Basin	Melbourne Water	5.26
SUB TOTAL				5.26
DSS “Gap”	N/A	Retarding & Sediment Basin	TBC	0.52
SUB TOTAL				0.52
TOTAL				21.59

Note: The areas identified in this table are subject to change/confirmation during the functional and detailed design stage to the satisfaction of Melbourne Water and the responsible authority.

[^] Negotiations with landowner are still ongoing regarding the final design and land-take for these assets.

* 3.26 total, 1.8ha included waterway corridor.

MWC= Melbourne Water Corporation



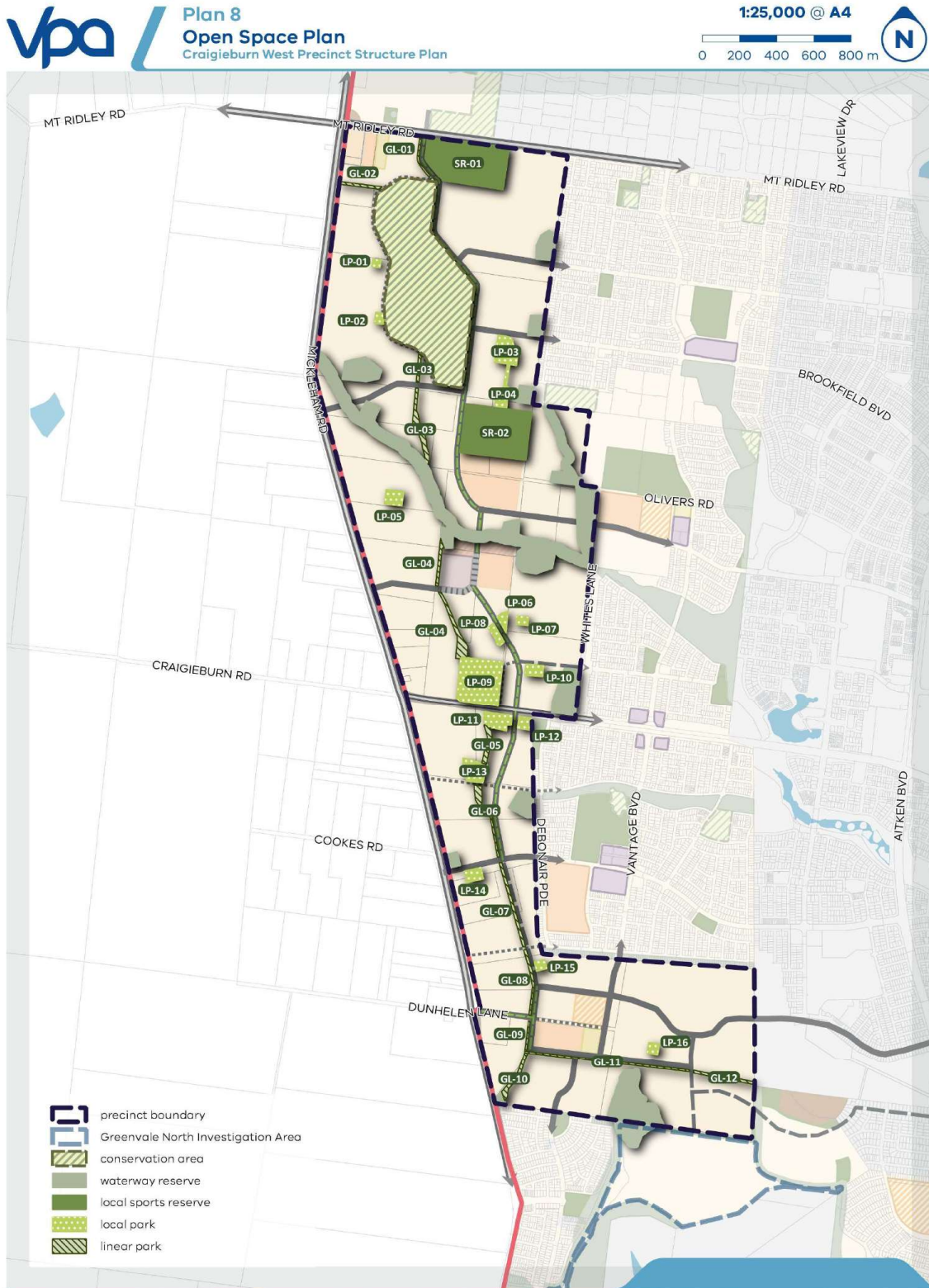
3.3.3 Bushfire management & safety

REQUIREMENTS	
R20	Vegetation within bushfire hazard areas shown on Plan 7 must be managed in accordance with Table 4.
R21	Development adjoining bushfire hazards shown on Plan 7 must be setback in accordance with Table 4
R22	<p>Where a setback from a bushfire hazard area is required by Table 4, unless otherwise agreed by the responsible authority and relevant fire authority, vegetation within the setback must be managed as follows:</p> <ul style="list-style-type: none"> • Grass must be short cropped and maintained during the declared fire danger period. • All leaves and vegetation debris must be removed at regular intervals during the declared fire danger period. • Within 10 metres of a building, flammable objects must not be located close to the vulnerable parts of the building. • Plants greater than 10 centimetres in height must not be placed within 3m of a window or glass feature of the building. • Shrubs must not be located under the canopy of trees. • Individual and clumps of shrubs must not exceed 5 sq. metres in area and must be separated by at least 5 metres. • Trees must not overhang or touch any elements of the building. • The canopy of trees must be separated by at least 2 metres. • There must be a clearance of at least 2 metres between the lowest tree branches and ground level.

GUIDELINES	
G36	All vegetation outside of a bushfire hazard area shown on Plan 7 should be managed to ensure a low risk of bushfire.
G37	Subdivision adjoining a bushfire hazard area should include a publicly accessible perimeter road.
G38	Subdivision should include a network of streets that provide multiple evacuation routes away from bushfire risks and areas of bushfire hazard.
G39	Where a setback is required from a bushfire hazard, the setback should be provided on public land where practical.
G40	All fencing adjoining Bushfire Hazard areas 1, 2 & 3 shown on Plan 7 should be made from non-combustible materials.
G41	Landscape design and plant selection in open spaces, including waterways and drainage corridors, should not increase bushfire risk.

Table 4: Bushfire hazard vegetation management & setback requirements

	BUSHFIRE HAZARD AREA 1	BUSHFIRE HAZARD AREA 2	BUSHFIRE HAZARD AREA 3	BUSHFIRE HAZARD AREA 4
Vegetation management class	Woodland	Grassland	Low threat	Low threat
Setback distance from bushfire hazard area	33m	19m	0m	0m



3.4 Public realm, open space & heritage

3.4.1 Open space and natural system

REQUIREMENTS	
R23	<p>Trees in streets, civic places and the open space network must be provided in accordance with Council's policies and guidelines, and:</p> <ul style="list-style-type: none"> • Complement the existing native indigenous and exotic species. • Be larger species to facilitate continuous canopy cover. • Be planted in modified and improved soil to support tree establishment. • Be appropriately sized to nature strips, nearby utilities and buildings. • Suited to local conditions.
R24	<p>The first development proponent to lodge a permit application for land which contains a section of the linear park as outlined on Plan 8 must undertake a master plan for that section of the entire linear park, unless otherwise agreed by the responsible authority.</p> <p>The masterplan may be prepared in separate stages (i.e. north and south of Craigieburn Road) to the satisfaction of the responsible authority.</p>
R25	<p>Development of the linear park as shown on Plan 8 must:</p> <ul style="list-style-type: none"> • Accommodate the full Tree Protection Zone of all River Red Gums shown as must be retained on Plan 10 within the linear park. • Ensure pedestrian access is provided to all residential lot frontages via a paper road.
GUIDELINES	
G42	<p>Local parks should be generally be provided where shown on Plan 8 and as outlined in Table 4.</p>
G43	<p>Alternative locations and configurations for local parks (other than the linear park) may be considered, subject to:</p> <ul style="list-style-type: none"> • Addressing the required locational attributes as outlined in Table 4. • Not diminishing the quality or usability of the space. • Not adversely impacting on the overall diversity of the precinct open space network. • Being equal to or more than the passive open space provision shown in Table 4 • Still being supported by the preferred path network outlined in Plan 5.
G44	<p>Existing high-quality vegetation should be retained within public space, including road reserves and open spaces, where safe and practicable.</p>
G45	<p>The open space network should:</p> <ul style="list-style-type: none"> • Maximise the amenity and value of service open space through the provision of shared paths, trails and other recreational elements. • Respond to the values of adjoining open space, waterways, and Aboriginal and post-contact heritage. • Provide flexible recreational opportunities that allow for the anticipated range of sporting reserves, and local parks and recreational uses required by the community.
G46	<p>Where a local park illustrated on Plan 8 spans multiple parcels, the first development proponent to lodge a permit application for land containing the park should prepare a master plan for the entire park. Consultation with all relevant landowners should be undertaken as part of the master plan preparation.</p>
G47	<p>Any fencing of open space should be low scale and visually permeable to facilitate public safety and surveillance.</p>
G48	<p>Public recreation and open space areas should be located adjacent to significant landscape value areas and waterways to create and or enhance any buffer area.</p>
G49	<p>Where possible, salvaged rocks should be incorporated into the design of waterways, retaining structures, fences and other landscape features.</p>

Table 5: Credited open space delivery guide

PARK ID	AREA	TYPE	LOCATIONAL ATTRIBUTES	RESPONSIBILITY
ACTIVE OPEN SPACE				
SR-01	9.5	Local Sports Reserve	Located in the north of the precinct to serve the Lindum Vale Catchment and accessible via the connector street network.	Hume City Council
SR-02	9.5	Local Sports Reserve	Located mid-section, north of the town centre and co-located with the proposed government High School, to serve the immediate catchment, and accessible via the connector street network.	Hume City Council
LOCAL PARKS				
LP-01	0.20	Local Park	Located to retain existing vegetation (refer Plan 10) within a passive open space.	Hume City Council
LP-02	0.30	Local Park	Located to retain existing vegetation (refer Plan 10) within a passive open space adjacent to the Conservation Area	Hume City Council
LP-03	1.58	Local Park	Located to retain existing vegetation (refer Plan 10) within a passive open space adjacent to the east/west connector street.	Hume City Council
LP-04	0.79	Local Park	Located to retain existing vegetation (refer Plan 10) within a passive open space adjacent to the east/west connector street.	Hume City Council
LP-05	0.75	Local Park	Located to provide passive open space central the surrounding residential community.	Hume City Council
LP-06	0.36	Local Park	Located to retain existing vegetation (refer Plan 10) within a passive open space adjacent to the north/south boulevard connector street.	Hume City Council
LP-07	0.30	Local Park	Located to retain existing vegetation (refer Plan 10) within a passive open space.	Hume City Council
LP-08	0.44	Local Park	Located to retain existing vegetation (refer Plan 10) within a passive open space adjacent to the north/south boulevard connector street.	Hume City Council
LP-09	5.49	Local Park	Located to retain existing vegetation (refer Plan 10) within a passive open space north of Craigieburn Road.	Hume City Council
LP-10	0.60	Local Park	Located to retain existing vegetation (refer Plan 10) within a passive open space.	Hume City Council
LP-11	1.17	Local Park	Located to retain existing vegetation (refer Plan 10) within a passive open space adjacent to Craigieburn Road and the north south boulevard connector road intersection.	Hume City Council
LP-12	0.50	Local Park	Located to retain existing vegetation (refer Plan 10) within a passive open space adjacent to Craigieburn Road and the north south boulevard connector road intersection.	Hume City Council

LP-13	1.30	Local Park	Located to retain existing vegetation (refer Plan 10) within a passive open space.	Hume City Council
LP-14	0.60	Local Park	Located to retain existing vegetation (refer Plan 10) within a passive open space.	Hume City Council
LP-15	0.30	Local Park	Located to provide passive open space central the surrounding residential community.	Hume City Council
LP-16	0.35	Local Park	Located to provide passive open space central the surrounding residential community.	Hume City Council
GREEN LINK (LINEAR PARK)				
GL-01	0.28	Linear Park	Located to provide a linear park link into Lindum Vale (north of Mt Ridley Road)	Hume City Council
GL-02	0.34	Linear Park	Located to provide a linear park link towards Mickleham Road.	Hume City Council
GL-03	1.21	Linear Park	Located to retain existing vegetation (refer Plan 10) and link the linear park network from the BCS shared path to the Aitken Creek shared path network.	Hume City Council
GL-04	1.40	Linear Park	Located to retain existing vegetation (refer Plan 10) in a passive open space and link the linear park network from the Aitken Creek shared path network to Local Town Centre and LP-09.	Hume City Council
GL-05	0.54	Linear Park	Located to retain existing vegetation (refer Plan 10) and link the linear park network from the LP-11 to LP-13.	Hume City Council
GL-06	0.73	Linear Park	Located to retain existing vegetation (refer Plan 10) and link the linear park network from LP-13 to the east–west connector (Elevation Boulevard extension).	Hume City Council
GL-07	0.70	Linear Park	Located to provide a linear park link between the east–west connector (Elevation Boulevard extension) and east–west local access street.	Hume City Council
GL-08	0.45	Linear Park	Located to provide a linear park link between the east–west local access street and the Boulevard Connector (Dunellen lane extension).	Hume City Council
GL-09	0.16	Linear Park	Located to provide a linear park link between the Boulevard Connector (Dunellen lane extension) and GL-10 & GL-11.	Hume City Council
GL-10	0.55	Linear Park	Located to retain existing vegetation (refer Plan 10) and link the linear park network from GL-09 & GL-11 towards Mickleham Road.	Hume City Council
GL-11	0.89	Linear Park	Located to retain existing vegetation (refer Plan 10) and provide an east–west linear park link adjacent the connector street network between GL-09 & GL-10.	Hume City Council
GL-12	0.50	Linear Park	Located to provide an east–west linear park link between GL-11 and Mount Aitken to the east.	Hume City Council

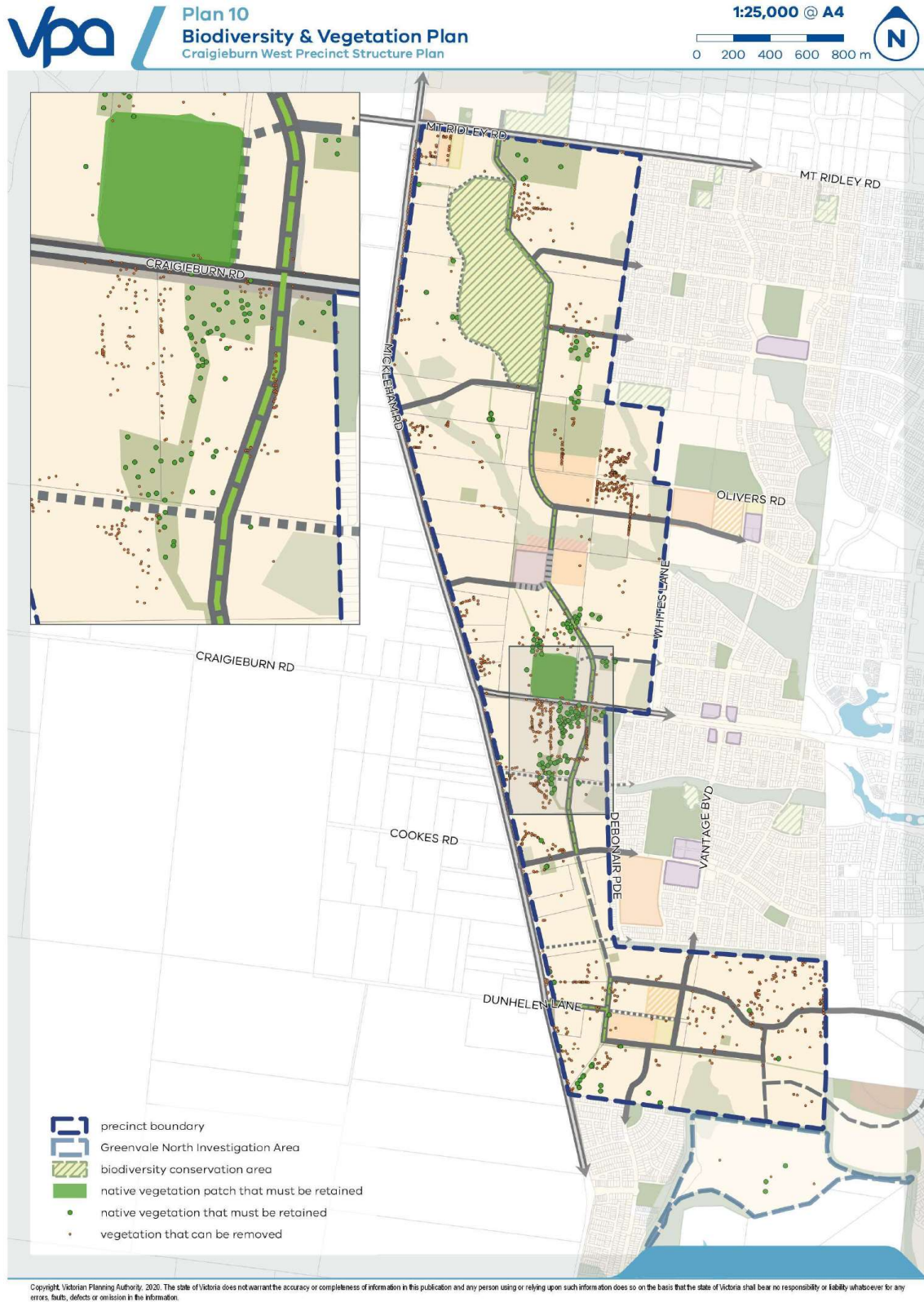


3.4.2 Heritage & public realm

REQUIREMENTS	
R26	<p>A sensitive interface to the Dunhelen House & Barn (HO31) must be provided to enhance the heritage significance of the site as identified in the Heritage Overlay in the Hume Planning Scheme. In particular, the road layout, subdivision design and development must:</p> <ul style="list-style-type: none"> • Ensure the heritage site becomes a feature of the precinct. • Provide a buffer between the heritage buildings and new dwellings. • Ensure the building is contained on a parcel that provides an appropriate curtilage to the building. • Maintain convenient access to the site. • Ensure the parcel containing heritage building has access to the internal subdivision street network. <p>All to the satisfaction of the responsible authority.</p>
R27	<p>Where a Place of Worship/assembly is proposed to be retained, subdivision and development adjacent to existing and future Places of Worship and Places of Assembly as indicated on Plan 9 must:</p> <ul style="list-style-type: none"> • Ensure the site becomes a feature of the precinct. • Provide a buffer between the Places of Worship & Assembly and new dwellings. • Ensure the building is contained on a parcel that provides an appropriate curtilage to the building. • Maintain convenient access to the site. • Ensure the parcel containing the Places of Worship & Assembly has access to the internal subdivision street network. <p>All to the satisfaction of the responsible authority.</p>

GUIDELINES	
G50	Dunhelen House & Barn (HO31) should be considered for adaptive reuse for public or commercial uses.
G51	Significant landscape features, such as high points, vegetation, open space and waterways, should be used as focal points for view lines along streets.
G52	Subdivision design should incorporate natural and constructed design elements which respond to local heritage, neighbouring land uses and topography to assist in place making and the achievement of a "sense of place".
G53	Aboriginal and historic cultural heritage should be recognised through the design of public places, infrastructure and interpretive installations. Opportunity should be explored through cultural heritage interpretation trails along public path networks in areas of known historic cultural history or areas of Aboriginal cultural heritage sensitivity, in consultation with relevant stakeholders.
G54	Signage or interpretive opportunities should be integrated into the public realm to contribute to the knowledge and understanding of the local area's Aboriginal cultural and historic cultural history. Integration opportunities should be provided in consultation with the Wurundjeri Aboriginal Corporation, aligning with the Aboriginal Cultural Values Assessment undertaken for the Craigieburn West PSP.
G55	Subdivision design should respond sensitively to the visual setting and character of heritage places.
G56	<p>Dry stone walls, where assessed to be of high value, should be retained, and:</p> <ul style="list-style-type: none"> • Be situated within public open space or a street reserve, to the satisfaction of the responsible authority • Be incorporated into subdivision design to minimise disturbance to the walls (e.g. utilisation of existing openings for vehicle and pedestrian access) • Have a suitable landscape interface to minimise maintenance requirements (e.g. mulch, garden bed or gravel), which does not encourage public access immediately adjacent to the retained walls • Be checked by a suitably qualified professional for works required to preserve the structural integrity of the wall in a manner suitable for the future context.
G57	Any reinstatement or repair of dry-stone walls should be undertaken by a suitably qualified professional and is to be consistent with the construction style of the original wall, with edges around

	<p>wall openings made secure (cemented) to the satisfaction of the responsible authority. Reinstatement is to use stone from (in order of priority):</p> <ul style="list-style-type: none">• The original wall in that location (including fallen stone adjacent to the wall).• A nearby section of the wall approved to be removed.• Any adjacent land containing wall parts which can be recovered.• Any walls approved for removal in the nearby area (including any stone which has been stockpiled by the responsible authority.• A list of suitably qualified professionals can be obtained from the responsible authority and the Dry Stone Walls Association of Australia.
G58	<p>A consistent suite of lighting and furniture should be used across neighbourhoods, appropriate to the type and role of street or public space.</p>



3.5 Biodiversity, vegetation & landscape character

REQUIREMENTS	
R28	Any proposed development or works within BCS Conservation Area 29 must obtain the approval of the Department of Environment, Land, Water and Planning.
R29	All proposed development adjacent to BCS Conservation Area 29 must provide a minimum 20m edge road along all boundaries to the satisfaction of the Department of Environment, Land, Water and Planning.
R30	Where local parks and recreation areas occur adjacent to BCS Conservation Area 29, they must be designed and managed to complement the outcomes required in Section 5 of the BCS relating to Conservation Area No. 29.
R31	Paths located within the BCS Conservation Area 29 must be designed to avoid and minimise disturbance to native vegetation and habitat for matters of national environmental significance and be located in accordance with the BCS Conservation Area Concept Plan to the satisfaction of the Department of Environment, Land, Water and Planning.
R32	Development abutting the BCS Conservation Area 29 must be in accordance with the Conservation Interface Plan to the satisfaction of the Department of Environment, Land, Water and Planning.
R33	Vegetation shown on Plan 10 as Vegetation for Retention must be retained and incorporated into either the open space network or the public realm.

GUIDELINES	
G59	Where practicable, existing vegetation should be retained, protected and enhanced to provide habitat and movement corridors for local fauna.
G60	Appropriately managed and sensitively designed community access to conservation and landscape values areas should be provided where practicable, and where protection of the primary conservation and landscape values can be maintained
G61	The layout and design of waterways, wetlands and retarding basins (including the design of paths, bridges and boardwalks, and the stormwater drainage system) should integrate with biodiversity and natural systems to the satisfaction of Melbourne Water and other relevant responsible authorities.

3.6 Education & community infrastructure

3.6.1 Community facilities & education

REQUIREMENTS	
R34	Education facilities must have a minimum of two road frontages (three preferred), with one connector road abutting the school with a road easement wide enough to allow for school bus movement while accommodating on-street parking and two way traffic movement.
R35	Any connector road or access street abutting a community or education facility must be designed to achieve slow vehicle speeds and provide designated pedestrian crossing points in the vicinity of the school site.

GUIDELINES	
G62	Education, community facilities and sports reserves should be accessible by active and public transport routes and provide active street frontages.
G63	Subdivision and development should facilitate integration of schools, sports reserves and community facilities where they are co-located and promote: <ul style="list-style-type: none"> Integration with neighbouring facilities to maximise efficiencies through the sharing of car parking and other complementary infrastructure. Out-of-hours use, street activation and permeability. Safe and convenient pedestrian and cyclist shared path access.
G64	Educational, community or civic infrastructure not shown on Plan 11 should be located within or proximate to a town centre, local convenience centre, community hub or council community building, as appropriate.
G65	Emergency services should have access to the arterial road network to maximise coverage and reduce response times.
G66	Public health and justice services should be located within or adjacent to a community hub or town centre and with access to public transport.
G67	Where the responsible authority is satisfied that land shown as a non-government school site is unlikely to be used for a school at ultimate development of the PSP, that land must be used for an alternative purpose that is compatible with the surrounding land uses and the provisions of the applied zone. Justification should be provided in accordance with the VPA's guidance note titled <i>Development of Non-Government School Sites for an Alternative Purpose</i> .
G68	The indicative layout of community facilities and open space as illustrated in Plan 11 may be altered to the satisfaction of the relevant responsible authorities.

3.7 Centre, employment & economic activity

3.7.1 Town centres

Table 6: Craigieburn West town centre hierarchy – internal to precinct

INTERNAL TOWN CENTRE	RETAIL FLOOR SPACE	COMMERCIAL FLOOR SPACE	LOCATION AND USES
Craigieburn West Activity Centre – LTC 1	6,000m ²	1000m ²	Located centrally in the Craigieburn West PSP, accessible from Mickleham Road and located north of Craigieburn Road. Provides a full line supermarket and specialty shops, with the ability to support non-retail local services.

REQUIREMENTS

R36

Development of the Local Town Centre (LTC 1) shown on Plan 4(Place Based Plan) must provide the floor space outlined in Table 5, an appropriate design response to the Performance Requirements and Guidelines of Table 6, and the Local Town Centre Design Principles in Appendix 4.3.

GUIDELINES

G69

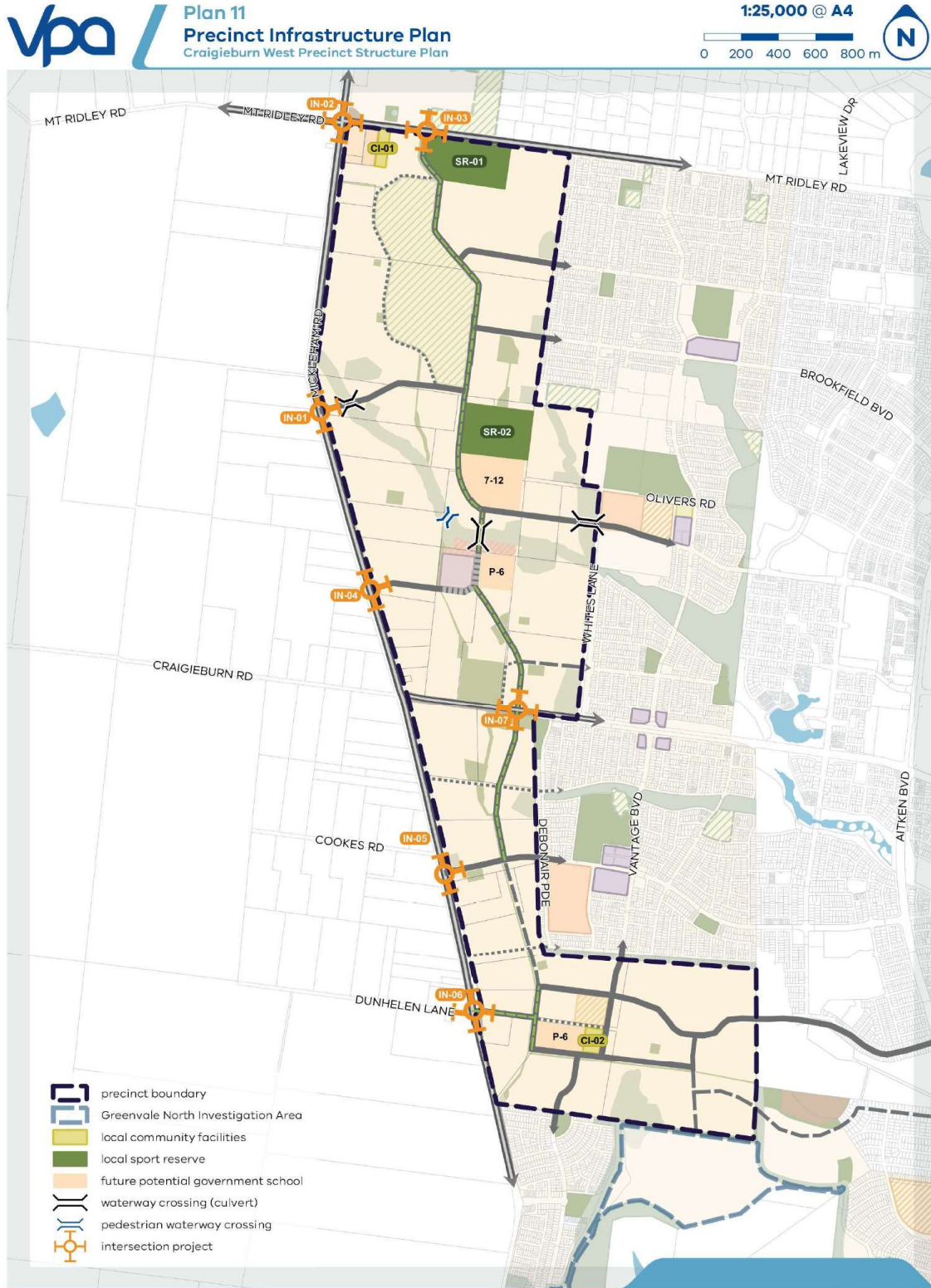
Subdivision layouts should provide for a range of lot configurations to cater for various uses, including small local enterprises.

G70

Additional local convenience centres may be considered subject to demonstrating that they do not compromise the role and function of the nearby Local Town Centres, to the satisfaction of the responsible authority.


Table 7: Craigieburn West Local Town Centre – performance requirements & guidelines

TOWN CENTRE ELEMENT	PERFORMANCE REQUIREMENTS	PERFORMANCE GUIDELINES
Key design elements	<ul style="list-style-type: none"> • Must address all relevant elements of the Urban Design Guidelines of Victoria. • Must integrate the town centre core with the surrounding neighbourhoods, public transport and community facilities. • A centralised town square to be provided that will act as forecourt to the community facility and focal point for surrounding retail uses. • The main streets to be designed to include dense canopy shade tree provision, outdoor dining and pedestrian activity and on-street parking. 	<ul style="list-style-type: none"> • Should respond to the surrounding site features, including the wetlands, waterways, open space areas, surrounding hilltops, and other points of interest to create views and connections and the stories of the place. • Should provide a neighbourhood with a pedestrian and active transport priority, with a focus on creating a resilient and accessible 20-minute neighbourhood. • Mixed-use precincts should provide retail and/or office at ground level, and office, commercial and residential above ground level. • Minimise barriers to pedestrian and bicycle access to the centre, notably across the north south connector road and loading and car parking areas. • Development blocks should be based on a permeable layout to enable flexibility to suit a variety of land uses and allow viable short-term development as well as efficient long-term evolution. • Potential Regional Skate/Play space to be located in or adjacent to the linear reserve.
Retail core (Local Town Centre)	<ul style="list-style-type: none"> • Must provide an area of 3 net developable hectares for the provision of the Local Town Centre. • Must provide active frontages that address connector streets and boulevard connectors as a main street frontage. • Must be accessible via a connector street from Mickleham Road. • Must provide for the strong integration of the centre with the surrounding residential and education facilities, with a high level of surveillance along the primary streets for pedestrian access to the centre. 	<ul style="list-style-type: none"> • Should be located adjacent to and integrate with the linear park and utilise the active transport network. • Pedestrian and cyclist focused shared zone to be delivered on north–south connector street and the linear park, to integrate the town centre core with the surrounding neighbourhoods, public transport and community facilities. • Specialty retail and mixed use to sleeve the supermarket and other anchor retail. • Locate buildings which achieve high levels of articulation along the connector streets, including (as appropriate) clear glazing and regular entrances, an appropriate range of building material/colour palette themes and architectural design treatments (including opportunities for signage integration into building design). • Development on the south side of the east–west connector should provide capacity for ground floor specialist suites (office, medical services, etc) and independent retail and hospitality tenancies. Floor to ceiling heights should allow for adaptive use with a minimum of 3.6m on ground floor.



3.8 Precinct infrastructure delivery

3.8.1 Development staging

REQUIREMENTS	
	Development staging must provide for the timely provision and delivery of:
	<ul style="list-style-type: none"> Boulevard connector streets, connector streets, connector street waterway crossings and connector street intersections with arterial roads. Street links between properties, constructed to the property boundary. On- and off-road pedestrian and bicycle network paths, including the linear park. Essential infrastructure. Land for community infrastructure, sports fields and local open space.
GUIDELINES	
G71	The staging of development should provide for the early delivery of sports fields, community facilities, local parks and playgrounds within each neighbourhood and may be delivered in stages in consultation with the responsible authority.
G72	<p>Staging of development should be determined largely by the development proposals on land within the precinct and the availability of infrastructure services. Development applications should demonstrate how the development will:</p> <ul style="list-style-type: none"> Integrate with adjoining developments, including the timely provision of roads and path connections, to a practical extent. Provide open space and amenity to new residents in the early stages of the development, where relevant. Provide for continuous sealed road access to each new allotment constructed to an urban standard. Deliver any necessary trunk service extensions, including confirmation of the agreed approach and timing by the relevant service provider. Avoid and minimise impacts to BCS conservation areas with regard to the location of essential and other services.
G73	<p>Infrastructure projects identified in the Plan 11 should be delivered as per the timing priority identified in the timing column of Appendix 4.1.</p> <p>Where infrastructure is proposed to be delivered outside or ahead of the sequence identified in Appendix 4.1, the onus is on the developer to fund the infrastructure works as 'Works In Kind'.</p> <p>Note: Project delivery timing outlined in Appendix 4.1 is indicative and subject to periodic review by the relevant responsible authority.</p>
G74	<p>Development staging should have regard to:</p> <ul style="list-style-type: none"> Proximity to existing or proposed development fronts or serviced land. Proximity to significant existing public transport infrastructure or public transport service. Proximity to existing or committed community infrastructure, such as schools. Proximity to new or existing arterial or connector road infrastructure. Its role in facilitating delivery of the above infrastructure. <p>Staging that meets alternative criteria to the above may be considered by the responsible authority where an applicant satisfactorily demonstrates that development will not be isolated from basic and essential infrastructure and services.</p>

3.8.2 Subdivision works

REQUIREMENTS	
R38	<p>Subdivision of land within the PSP must provide and meet the cost for all local infrastructure, other than that provided for within the Craigieburn West ICP. This includes (but is not limited to):</p> <ul style="list-style-type: none"> • Boulevard connector streets, connector streets and local streets. • Local bus stop infrastructure (where locations have been agreed in writing by Head, Department of Transport). • Landscaping, including canopy tree planting, of all existing and future roads and local streets. • Intersection works and traffic management measures along arterial roads, connector streets, and local streets. • Council approved fencing and landscaping (where required) along arterial roads and reserves. • Shared pedestrian and bicycle paths along local arterial roads, connector roads, utilities easements, local streets, waterways and within local parks including bridges, intersections, and barrier crossing points. • Bicycle parking. • Appropriately scaled lighting along all roads, major shared and pedestrian paths, and traversing public open space. • Basic improvements to local parks and open space (refer to open space delivery below). • Local drainage system. • Construction of culverts for waterway crossings of boulevard connector streets, connector streets and local streets. • Local street or pedestrian path crossings of waterways unless outlined as the responsibility of another agency in the Precinct Infrastructure Plan. • Infrastructure as required by utility service providers including water, sewerage, drainage (except where the item is funded through a Development Services Scheme), electricity, gas, and telecommunications. • Construction of shared paths along waterways and open space. • Remediation and / or reconstruction of dry-stone walls, where required.
R39	<p>All public open space including the linear park/green link (where not otherwise provided via the Craigieburn West ICP) must be finished to a standard that satisfies the requirements of the responsible authority prior to the transfer of the public open space, including but not limited to:</p> <ul style="list-style-type: none"> • Removal of all existing and disused structures, foundations, pipelines, stockpiles, and any soil contamination. • Basic levelling including the supply and spread of minimum 75mm topsoil and subsoil if required on the proposed areas of open space to provide a stable free draining surface. • Clearing of rubbish, weeds and rocks, levelled, topsoiled and grassed with warm climate grass (unless conservation reserve requirements dictate otherwise). • Provision of water tapping, potable and recycled water connection points. • Sewer, gas and electricity connection points must also be provided to land identified as sports reserve. • Planting of trees and shrubs (with drought tolerant species). • Adequate protection of existing trees that are to be retained including exclusion zones as appropriate. • Vehicular exclusion devices (fence, bollards, or other suitable method). • Maintenance access points. • Construction of minimum 1.5 metre wide pedestrian paths around the perimeter of the reserve, connecting and linking into any other surrounding paths or points of interest, except where shown as a shared paths on Plan 9. • Installation of park furniture including seating, drinking fountains, shelters, tables, local scale playgrounds and other local scale play elements such as half basketball courts and hit-up walls, soccer goals, rubbish bins and appropriate paving to support these facilities, consistent with the type of public open space listed in the open space delivery guide at Table 4.
R40	<p>Sports reserves must be vested in the relevant authority in the following condition:</p> <ul style="list-style-type: none"> • Free from surface and protruding rocks and structures • Reasonably graded and/or top soiled to create a safe and regular surface, with a maximum 1:6 gradient • Bare, patchy and newly graded areas seeded, top-dressed with drought resistant grass.

	Consistent with the Craigieburn West ICP, where these works are not considered to be temporary, works are eligible for a works-in-kind credit against an ICP obligation. Works associated with adjacent road construction, such as earthworks for a road embankment, are not eligible for works-in-kind credit.
R41	Where an inter-parcel connection is intended or indicated in the PSP, streets must be constructed to property boundaries at the relevant stage of development required or approved by the responsible authority. Provision should be made for temporary vehicle turning until the inter-parcel connection is delivered.

4 APPENDICES

4.1 Precinct infrastructure table

CATEGORY	ICP REF. NO.	TITLE	DESCRIPTION	LEAD AGENCY	COMPONENT INCLUDED IN ICP			TIMING	APPORTIONMENT
					ULTIMATE LAND	INTERIM CONSTRUCTION	ULTIMATE CONSTRUCTION		
INTERSECTION PROJECTS									
Intersection	IN-01	Mickleham Road and east-west Connector Street	Construction of signalised T intersection.	Hume City Council	Yes	Yes	No	S-M	100%
Intersection	IN-02	Mickleham Road and Mount Ridley Road	Construction of signalised 4-way intersection.	Hume City Council	Yes	Yes	No	M-L	50%
Intersection	IN-03	Mount Ridley and north-south Boulevard Connector	Construction of signalised 4-way intersection.	Hume City Council	Yes	Yes	No	S	25%
Intersection	IN-04	Mickleham Road and east-west Connector Street	Construction of a signalised T intersection.	Hume City Council	Yes	Yes	No	S-M	100%
Intersection	IN-05	Mickleham Road and east-west Connector Street (extension of Elevation Blvd).	Construction of a signalised T intersection.	Hume City Council	Yes	Yes	No	S-M	100%
Intersection	IN-06	Mickleham Road and east-west Boulevard Connector Street (extension of Dunhelen Lane)	Construction of signalised 4-way intersection.	Hume City Council	Yes	Yes	No	S-M	100%
Intersection	IN-07	Craigieburn Road and north-south Boulevard Connector.	Construction of signalised 4-way intersection.	Hume City Council	Yes	Yes	No	S-M	100%

BRIDGE PROJECTS									
Bridge	BR-01	Connector Street road waterway crossing Aitken Creek waterway.	Construction of a single carriageway crossing of Aitken Creek	Melbourne Water	No	No	No	S	n/a
Bridge	BR-02	Connector Street road waterway crossing Aitken Creek waterway.	Construction of a single carriageway crossing of Aitken Creek	Melbourne Water	No	No	No	M	n/a
Bridge	BR-03	Connector Street road Bridge/culvert across Aitken Creek waterway.	Construction of a single carriageway crossing of Aitken Creek	Melbourne Water	No	No	No	S-M	n/a
COMMUNITY BUILDING & EDUCATION PROJECTS									
Community Infrastructure	CI-01	Northern Level 2 Community Facility	Purchase of land and construction of a Community Centre collocated with Mickleham Primary School expansion.	Hume City Council	Yes	N/A	Yes	M-L	50%
Community Infrastructure	CI-02	Southern Level 2 Community Facility	Purchase of land and construction of a Community Centre collocated with southern government primary school.	Hume City Council	Yes	N/A	Yes	M	100%
School	N/A	Government year P-6 (Mickleham Primary School expansion)	Purchase of additional land and construction of government year P-6 school (Mickleham Primary School)	Dept. of Education and Training	No	No	No	S-M	n/a
School	N/A	Government year P-6	Purchase of land and construction of government year P-6 school collocated local town centre.	Dept. of Education and Training	No	No	No	M-L	n/a
School	N/A	Government year 7-12 school	Purchase of land and construction of government year 7-12 school collocated with SR-02.	Dept. of Education and Training	No	No	No	M-L	n/a
School	N/A	Government year P-6	Purchase of land and construction of government year P-6 school collocated with community facilities.	Dept. of Education and Training	No	No	No	M-L	n/a
School	N/A	Non-Government year P-6 school	Purchase of land and construction of non-government year P-6 school collocated with government school and community facilities.	Private Education Provider	No	No	No	M-L	n/a

OPEN SPACE & CONSERVATION AREAS									
Reserve	SR-01	Sports fields	Purchase of land and construction of sports fields, hard courts and multipurpose pavilion.	Hume City Council	Yes	N/A	Yes	M-L	50%
Sporting Reserve	SR-02	Sports fields	Purchase of land and construction of sports fields, hard courts and multipurpose pavilion	Hume City Council	Yes	N/A	Yes	M-L	100%
Local Park	LP-01	Passive Open Space	Provision of land for a local park	Hume City Council	Yes	No	No	S-M	100%
Local Park	LP-02	Passive Open Space	Provision of land for a local park	Hume City Council	Yes	No	No	S-M	100%
Local Park	LP-03	Passive Open Space	Provision of land for a local park	Hume City Council	Yes	No	No	S-M	100%
Local Park	LP-04	Passive Open Space	Provision of land for a local park	Hume City Council	Yes	No	No	S-M	100%
Local Park	LP-05	Passive Open Space	Provision of land for a local park	Hume City Council	Yes	No	No	S-M	100%
Local Park	LP-06	Passive Open Space	Provision of land for a local park	Hume City Council	Yes	No	No	S-M	100%
Local Park	LP-07	Passive Open Space	Provision of land for a local park	Hume City Council	Yes	No	No	S-M	100%
Local Park	LP-08	Passive Open Space	Provision of land for a local park	Hume City Council	Yes	No	No	S-M	100%
Local Park	LP-09	Passive Linear Open Space	Provision of land for a local park	Hume City Council	Yes	No	No	S-M	100%
Local Park	LP-10	Passive Open Space	Provision of land for a local park	Hume City Council	Yes	No	No	S-M	100%
Local Park	LP-11	Passive Open Space	Provision of land for a local park	Hume City Council	Yes	No	No	S-M	100%
Local Park	LP-12	Passive Linear Open Space	Provision of land for a local park	Hume City Council	Yes	No	No	S-M	100%
Local Park	LP-13	Passive Open Space	Provision of land for a local park	Hume City Council	Yes	No	No	S-M	100%
Local Park	LP-14	Passive Open Space	Provision of land for a local park	Hume City Council	Yes	No	No	S-M	100%
Local Park	LP-15	Passive Open Space	Provision of land for a local park	Hume City Council	Yes	No	No	S-M	100%

Local Park	LP-16	Passive Open Space	Provision of land for a local park	Hume City Council	Yes	No	No	No	S-M	100%
Local Park (Green Link)	GL-01	Linear Open Space	Provision of land for a linear local park	Hume City Council	Yes	No	No	No	S-M	100%
Local Park (Green Link)	GL-02	Linear Open Space	Provision of land for a linear local park	Hume City Council	Yes	No	No	No	S-M	100%
Local Park (Green Link)	GL-03	Linear Open Space	Provision of land for a linear local park	Hume City Council	Yes	No	No	No	S-M	100%
Local Park (Green Link)	GL-04	Linear Open Space	Provision of land for a linear local park	Hume City Council	Yes	No	No	No	S-M	100%
Local Park (Green Link)	GL-05	Linear Open Space	Provision of land for a linear local park	Hume City Council	Yes	No	No	No	S-M	100%
Local Park (Green Link)	GL-06	Linear Open Space	Provision of land for a linear local park	Hume City Council	Yes	No	No	No	S-M	100%
Local Park (Green Link)	GL-07	Linear Open Space	Provision of land for a linear local park	Hume City Council	Yes	No	No	No	S-M	100%
Local Park (Green Link)	GL-08	Linear Open Space	Provision of land for a linear local park	Hume City Council	Yes	No	No	No	S-M	100%
Local Park (Green Link)	GL-09	Linear Open Space	Provision of land for a linear local park	Hume City Council	Yes	No	No	No	S-M	100%
Local Park (Green Link)	GL-10	Linear Open Space	Provision of land for a linear local park	Hume City Council	Yes	No	No	No	S-M	100%
Local Park (Green Link)	GL-11	Linear Open Space	Provision of land for a linear local park	Hume City Council	Yes	No	No	No	S-M	100%
Local Park (Green Link)	GL-12	Linear Open Space	Provision of land for a linear local park	Hume City Council	Yes	No	No	No	S-M	100%

4.2 Parcel specific land use budget table

PSP property ID	Total area (ha)	Transport		Community & Education				Open Space				Total net developable area (hectares)	Net developable area % of property		
		Arterial road		Other transport					Service open space		Credited open space				
		Arterial road – existing road reserve	Arterial road – public acquisition overlay	Arterial road – new / widening / intersection flaring (ICP land)	Non-arterial road – retained existing road reserve	Non-arterial road – new / widening / intersection flaring (ICP land)	Existing government school	Government school	Potential non-government school	ICP community facilities	Conservation reserve			Waterway & drainage reserve	Local sports reserve (ICP land)
1	1.63	-	-	-	-	1.63	-	-	-	-	-	-	-	0.00	0.00%
2	0.42	-	-	-	-	0.42	-	-	-	-	-	-	-	0.00	0.00%
3	1.36	-	-	-	-	-	1.36	-	-	-	-	-	-	0.00	0.00%
4	76.80	-	-	0.38	-	-	0.10	-	1.20	14.14	1.36	9.50	0.33	49.79	64.83%
5	2.24	-	-	-	-	-	-	-	-	-	-	-	0.29	1.96	87.29%
6	79.44	-	-	-	-	-	-	-	-	23.56	4.76	-	2.83	48.30	60.79%
7	14.80	-	-	0.12	-	-	-	-	-	-	2.39	-	0.44	11.86	80.10%
8	11.34	-	-	-	-	-	-	-	-	-	2.64	-	0.54	8.16	71.93%

9	10.28	-	-	-	-	-	-	-	-	1.55	-	-	-	-	-	4.06	-	4.67	45.45%
10	8.25	-	-	-	-	-	-	-	-	1.81	-	-	-	-	-	5.44	0.27	0.73	8.80%
11	12.30	-	-	-	-	-	-	-	-	-	-	-	-	2.74	-	-	-	9.56	77.71%
12	13.34	-	-	-	-	-	-	-	-	-	-	-	-	1.74	-	-	0.75	10.84	81.28%
13	8.85	-	-	-	-	-	-	-	-	-	-	-	-	0.17	-	-	-	8.67	98.05%
14	16.97	-	-	-	-	-	-	-	-	4.70	-	-	-	3.99	-	-	0.06	8.23	48.49%
15	8.23	-	-	-	-	-	-	-	-	-	-	-	-	2.25	-	-	-	5.98	72.65%
16	8.23	-	-	-	-	-	-	-	-	-	-	-	-	4.55	-	-	-	3.68	44.75%
17	11.25	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	11.13	98.95%
18	4.61	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	4.61	100.00%
19	2.04	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	2.04	100.00%
20	2.02	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	2.02	100.00%
21	0.14	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	0.14	100.00%
22	1.68	-	0.14	-	-	-	-	-	-	-	-	-	-	-	-	-	-	1.55	91.91%
23	16.40	-	0.23	-	-	-	-	-	-	-	-	-	-	0.004	-	-	1.07	15.09	92.05%
24	16.44	-	0.02	-	-	-	-	-	-	3.50	-	-	-	-	-	-	6.56	6.36	38.69%
25	16.73	-	-	-	-	-	-	-	-	-	-	-	-	1.40	-	-	0.30	15.03	89.86%
26	0.41	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	0.41	100.00%
27	16.77	-	-	-	-	-	-	-	-	-	-	-	-	2.92	-	-	0.60	12.86	76.69%
28	12.76	-	0.74	-	-	-	-	-	-	-	-	-	-	-	-	-	0.75	11.28	88.37%
29	12.19	-	0.65	-	-	-	-	-	-	-	-	-	-	-	-	-	2.70	8.51	69.81%
30	11.08	-	-	-	-	-	-	-	-	-	-	-	-	1.43	-	-	0.59	9.06	81.78%
31	13.98	-	-	-	-	-	-	-	-	-	-	-	-	0.52	-	-	0.63	12.72	90.98%
32	3.24	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	0.24	3.01	92.70%
33	4.85	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	0.36	4.49	92.55%
34	11.73	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	0.34	11.39	97.11%
35	14.96	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	0.56	14.40	96.25%

4.3 Craigieburn West Local Town Centre – design principles

LOCAL TOWN CENTRES	
<p>Principle 1</p> <p>Provide every neighbourhood with a viable Local Town Centre as a focus of the community with a fine grain, closely spaced distribution pattern.</p>	<ul style="list-style-type: none"> • Deliver a fine grain distribution pattern of highly accessible Local Town Centres generally on a scale of one Local Town Centre for every neighbourhood of 8,000 to 10,000 people. • Locate Local Town Centres with a distribution pattern of around one Local Town Centre for every square mile (approx. 2.6km²) of residential development. • Deliver a network of economically viable Local Town Centres including a supermarket and supporting competitive local shopping business, medical, leisure, recreation and community needs while allowing opportunities for local specialisation.
<p>Principle 2</p> <p>Locate Local Town Centres on a connector street intersection with access to an arterial road and transit stop.</p>	<ul style="list-style-type: none"> • Locate the Local Town Centre on an arterial/connector intersection and ensure that the Local Town Centre is central to the residential catchment that it services while optimising opportunities for passing trade. • Locate the Local Town Centre with future railway stations or other forms of transit stops to benefit the Local Town Centre, to offer convenience for public transport passengers, and to minimise walking distance between transit stops and the town centre core. • Other Local Town Centre locations may be considered where the location results in the Local Town Centre being central to the residential catchment that it serves and/or the location incorporates natural or cultural landscape features such as rivers and creeks, tree rows, topographic features or other heritage structures which assist in creating a sense of place.
<p>Principle 3</p> <p>Locate Local Town Centres in an attractive setting so that most people live within a walkable catchment of a Local Town Centre and relate to the centre as the focus of the neighbourhood.</p>	<ul style="list-style-type: none"> • Ensure that 80-90% of households are within a 1km walkable catchment of a local or higher order Town Centre. • Locate Local Town Centres in attractive settings and incorporate natural or cultural landscape features such as creeks and waterways, linear open space, pedestrian and cycle links and areas of high aesthetic value. • The design of the Local Town Centre should respect existing views and vistas to and from the Local Town Centre location.
<p>Principle 4</p> <p>Provide a full range of local community and other facilities including a supermarket, shops, medical and recreation uses.</p>	<ul style="list-style-type: none"> • Land uses should be located generally in accordance with the locations and general land use terms identified on the Local Town Centre Concept Plan. • Promote designs which offer a high degree of community interaction and provision of a vibrant and viable mix of retail, recreation and community uses. • Encourage clustering of uses in precincts such as a 'medical precinct' where similar or synergistic uses should be sited together to promote stronger trading patterns.

	<ul style="list-style-type: none"> • Encourage smaller grain individual tenancies and land ownership patterns to attract participation of local business investment and encourage opportunities for greater diversity. • Incorporate flexible floor spaces (including floor to ceiling heights) into building design to enable localised commercial uses to locate amongst the activity of the local town centre. • The local town centre should generally be anchored by one full line supermarket and supported by specialty stores unless otherwise noted on the Local Town Centre Concept Plan. • Supermarkets and other commercial or community anchors or secondary anchors within the local town centre should generally be located diagonally opposite one another across the main street and/or town square to promote pedestrian desire lines that maximise movement within the public realm. • A small access mall that address a supermarket/other 'large box uses' may be considered as part of the overall design. Such access malls may have a limited number of internalised shops. The primary access to the mall should be from the main street and/or the town square. • Active building frontages should address the main- street and town square to maximise exposure to passing trade and promote pedestrian interaction. • Provide retail and/or office at ground level, and office, commercial and residential above ground level in Mixed Use precincts • Locate childcare, medical centres and specialised accommodation (for example, aged care, nursing home, student accommodation, and serviced apartments) within or at the edge of the local town centre to contribute to the centre's activity and the resident's access to services. • Locate car parking areas centrally to development sites and to the rear and or side of street-based retail frontages. • Design car parking areas to accommodate flexible uses and allow for long term development opportunities. • Provide public toilets in safe and accessible locations within the managed area of the property.
<p>Principle 5</p> <p>Focus on a public space as the centre of community life.</p>	<ul style="list-style-type: none"> • Provide a public space which acts as the central meeting place within the local town centre. This space may take the form of a town square, town park, public plaza space, public market place or a similar locally responsive option designed to function as the identifiable 'centre' or 'heart' with a distinctive local character for both the local town centre and the broader residential catchment. • Key uses of the LTC are to be positioned where they front public spaces to ensure it is a dynamic and activated space. • Design flexible and adaptable public spaces so that a range of uses can occur within them at any one time. Such uses may include people accessing daily shopping and business needs as well as social interaction, relaxation, celebrations and temporary uses (such as stalls, exhibitions and markets). • Design the public space so that it is well integrated with pedestrian and cycle links around and through the local town centre so that it acts as a 'gateway' to the activity of the centre.

	<ul style="list-style-type: none"> The main public space or town square should have a minimum area of 500 square metres. Smaller public spaces which are integrated within the built form design, surrounded by active frontages and facilitate high levels of pedestrian movement are also encouraged. Footpath widths within and around the public space as well as along the main street should be sufficient to provide for universal access as well as outdoor dining and smaller gathering spaces.
<p>Principle 6</p> <p>Integrate local employment and service opportunities in a business-friendly environment.</p>	<ul style="list-style-type: none"> Provide a variety of employment and business opportunities through the provision of a broad mix of land uses and commercial activities. Provide a range of options and locations for office-based businesses. Consider appropriate locations for small office/home office housing which maximise the access and exposure to the activity of the local town centre. Provide services and facilities to support home based and smaller businesses within the local town centre. Consider using these uses to sleeve loading areas and car parks where feasible.
<p>Principle 7</p> <p>Include a range of medium and high-density housing and other forms of residential uses within and surrounding the local town centre.</p>	<ul style="list-style-type: none"> Provide medium and high-density housing in and around the local town centre for passive surveillance and contributions to the centre's life and amenity. Provide medium and high-density housing in locations of high amenity in and around the local town centre, connected to the activity of the local town centre through strong pedestrian and cycle links. Provide a range of housing types for a cross section of the community (such as retirement living) in and around the local town centre. Design the local town centre to avoid potential land use conflicts between residential and commercial uses by focusing on retail operations on the main street and around the town square and locating residential uses predominantly at the edge and/or on upper levels. Refer to the Small Lot Housing Code for further information about housing requirements for small lots around local town centres.
<p>Principle 8</p> <p>Design the Local Town Centre to be pedestrian friendly and accessible by all modes including public transport, while enabling private vehicle access.</p>	<ul style="list-style-type: none"> Design the local town centre to provide easy, direct and safe access for pedestrians, cyclists, public transport modes, private vehicles, service and delivery vehicles with priority given to pedestrian movement, amenity, convenience and safety. Provide a permeable street network, walkways and public spaces that provide linkages throughout the centre and designated pedestrian crossing points. Design the main and other streets to comply with the relevant cross sections found within the PSP. A speed environment of 40 kilometres per hour or less should be designed for the length of the main street. Provide public transport infrastructure facilities in convenient locations for commuters. Provide bus stops in accordance with the Public Transport Guidelines for Land Use and Development, to the satisfaction of the Department of Transport. Provide bicycle parking within the street network and public spaces in highly visible locations and close to key destinations.

	<ul style="list-style-type: none"> • Design supermarket and other 'large format' buildings so they do not impede on the movement of people around the local town centre. • Encourage pedestrian movement along the length of the street and through public spaces by locating key buildings at strategic points/sites. • Design buildings so they have a positive relationship with the interface to the public street network and does not impede on the pedestrian movement. • Design car parking areas with adequate positioning and lighting to ensure passive surveillance and public. • Provide dedicated pedestrian routes and areas of landscaping within off street car park areas. • Provide on-street car parking to encourage short stay/convenience uses. • Group and limit the number of car park access crossovers. • Design heavy vehicle access points to limit the pedestrian and vehicle conflict. Loading and deliveries should be located to the rear and or side of street-based retail frontages. • All streets, public spaces and car parks to be lit to Australian standards and with pedestrian (generally white) light. Lighting should be designed to avoid unnecessary spill to the side or above. • Secondary access to the supermarket from car parking areas should only be considered where it facilitates convenient trolley access and does not diminish the role of the primary access from the main street and or town square.
<p>Principle 9</p> <p>Create a sense of place with high quality engaging urban design.</p>	<ul style="list-style-type: none"> • Design developments to complement and enhance the character of the surrounding area by responding to key visual cues associated with the topography and other natural features of the local town centre location and its surrounds. • Minimise amenity and noise impacts resulting from the mix of uses by maintaining appropriate separation and transitional areas between retail and housing activities using open space, road networks and community facilities. • Use materials and design elements that contribute to a cohesive and legible character for the local town centre as a whole. • Designate sites in prominent locations (such as at key intersections, surrounding public spaces and terminating key view lines and vistas) for significant buildings or landmark structures. • Ensure that the design of corner sites, where the main street meets an intersecting and/or arterial road by: <ul style="list-style-type: none"> ○ providing built form that anchors the main street to the intersecting road. This can be achieved through increased building height, scale and articulated frontages ○ incorporating either 2 storey buildings or 2 storey elements (such as awnings and roof lines) ○ providing an active ground floor frontage and active floor space components to the main street frontage ○ providing a consistent covered walkway or veranda for weather protection in the design of building frontages on main pedestrian routes. • Align built form with the property boundary to define the street edge.

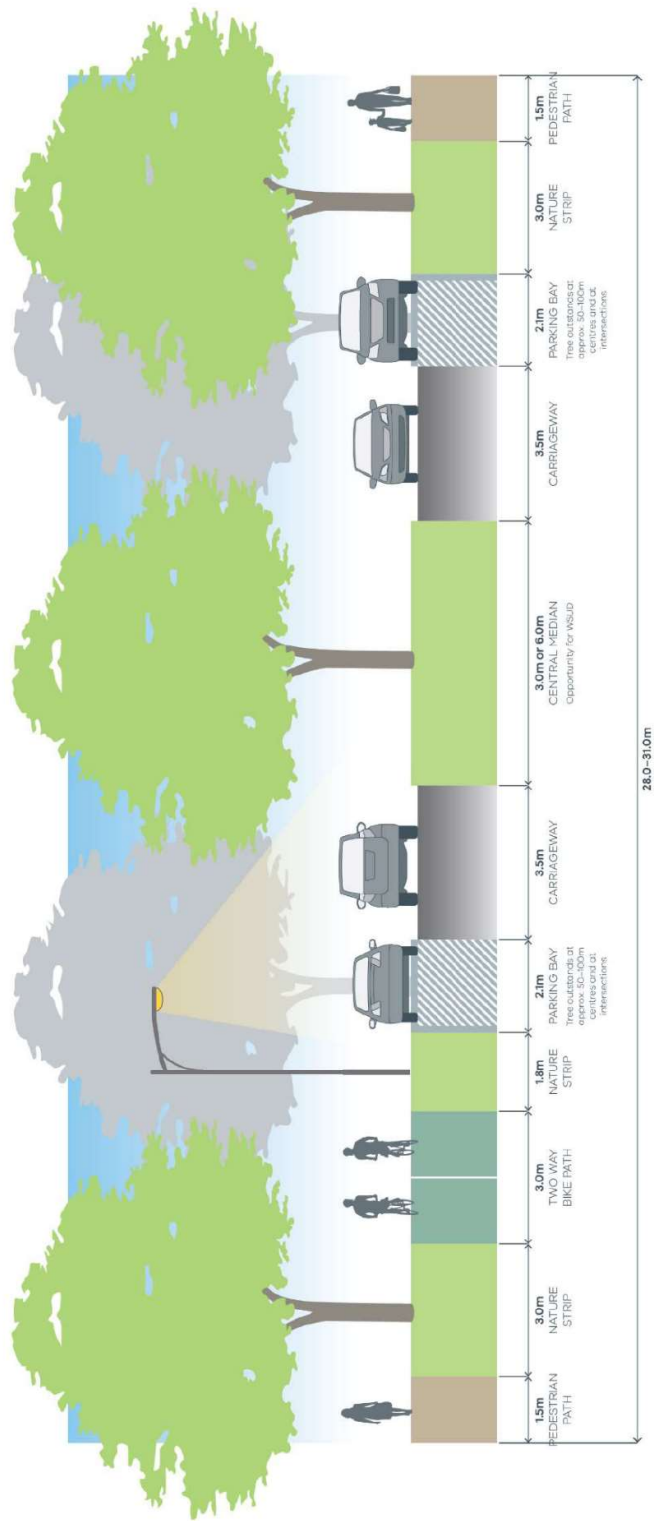
	<ul style="list-style-type: none"> • Provide visually rich, interesting and well-articulated street interfaces and all visible side or rear facades finished in suitable materials and colours that contribute to the character of the local town centre. • The design and siting of supermarkets and other 'large format retail uses should provide an appropriate response to the entire public domain. This includes but is not limited to car parking areas, predominant routes and street level interfaces. • Design supermarket and secondary anchors with frontages that directly address the main street and/or town square so that the use integrates with and promotes activity within the main street and public spaces/ thoroughfares. • Design supermarkets or large format retail uses with a direct frontage to the main street using clear glazing to allow view lines into the store from the street. (Planning permits for buildings and works should condition against the use of "white washed" or frosted glass windows, excessive window advertising and obtrusive internal shelving or 'false walls' offset from the glazing). • Retail uses along street frontages should generally include access points at regular intervals to encourage activity along the length of the street. • Public spaces should be oriented to capture north sun and protect from prevailing winds and weather • Landscaping of all interface areas should be of a high standard as an important element to complement the built form design. • Urban art should be incorporated into the design of the public realm. • Street furniture should be located in areas that are highly visible and close to or adjoining pedestrian desire lines/gathering spaces and designed to add visual interest to the Local Town Centre. • Wrapping or sleeving of car parking edges with built form, to improve street interface, should be maximised. • Car parking areas should provide for appropriate landscaping with planting of canopy trees and dedicated pedestrian thoroughfares. • Screening of centralised waste collection points should minimise amenity impacts on adjoining areas and users of the centre. • Where service areas are accessible from car parks, they should present a well-designed and secure facade to public areas. • Mechanical plant and service structure roofs should be included within roof lines or otherwise hidden from view.
<p>Principle 10</p> <p>Promote localisation, sustainability and adaptability.</p>	<ul style="list-style-type: none"> • The Local Town Centre should promote the localisation of services which will contribute to a reduction of travel distance to access local services and less dependence on private vehicles. • The Local Town Centre should be designed to be sympathetic to its natural surrounds by: <ul style="list-style-type: none"> • investigating the use of energy efficient design and construction methods for all buildings • including Water Sensitive Urban Design principles such as integrated stormwater retention and reuse (e.g. toilet flushing and landscape irrigation) • promoting safe and direct accessibility and mobility within and to and from the Local Town Centre

	<ul style="list-style-type: none">• including options for shade and shelter through a combination of landscape and built form treatments• ensuring buildings are naturally ventilated to reduce the reliance on plant equipment for heating and cooling• promoting passive solar orientation in the configuration and distribution of built form and public spaces• grouping waste collection points to maximise opportunities for recycling and reuse• promoting solar energy for water and space heating, electricity generation and internal and external lighting• investigating other opportunities for the built form to reduce greenhouse gas emissions associated with the occupation and the ongoing use of buildings.• Ensure the local town centre and building design has an inbuilt capacity for growth.
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4.4 Local Convenience Centre performance criteria

LOCAL CONVENIENCE CENTRES	
<p>Principle 1</p> <p>Local Convenience Centres should facilitate access to goods, services, community facilities and opportunities for social interaction.</p>	<ul style="list-style-type: none"> Local Convenience Centres should make a positive contribution to the structure and planned future character of neighbourhoods Local Convenience Centres should reinforce neighbourhood legibility and sense of place Local Convenience Centres should support the delivery of more diverse and higher density forms of housing.
<p>Principle 2</p> <p>Local Convenience Centres should provide for the convenience needs of the local community.</p>	<ul style="list-style-type: none"> Local Convenience Centres should be of a form, scale and design quality that reinforces their role as focal points for the community. Commercial activities within Local Convenience Centres should be limited to a range and scale that meets the convenience needs of local residents and passers-by. Local Convenience Centres should be adaptable to a range of uses and allow activities to change over time. Local Convenience Centres should make a positive contribution to the visual quality and interest of streets and other public open spaces.
<p>Principle 3</p> <p>Local Convenience Centres should be safe, accessible environments that provide for a range of transportation choices.</p>	<ul style="list-style-type: none"> Local Convenience Centres should make a positive contribution to pedestrian amenity, movement, safety and convenience for people of all ages and abilities. At grade parking and vehicle access should be located and designed in such a manner as to avoid or mitigate adverse effects on pedestrian safety and amenity, and the streetscape. Bicycle parking should be provided within the street network and public spaces in highly visible locations and close to pedestrian desire lines and key destinations. Bus stops should be designed in accordance with the Public Transport Guidelines for Land Use and Development, to the satisfaction of the Department of Transport.

4.5 Street cross sections

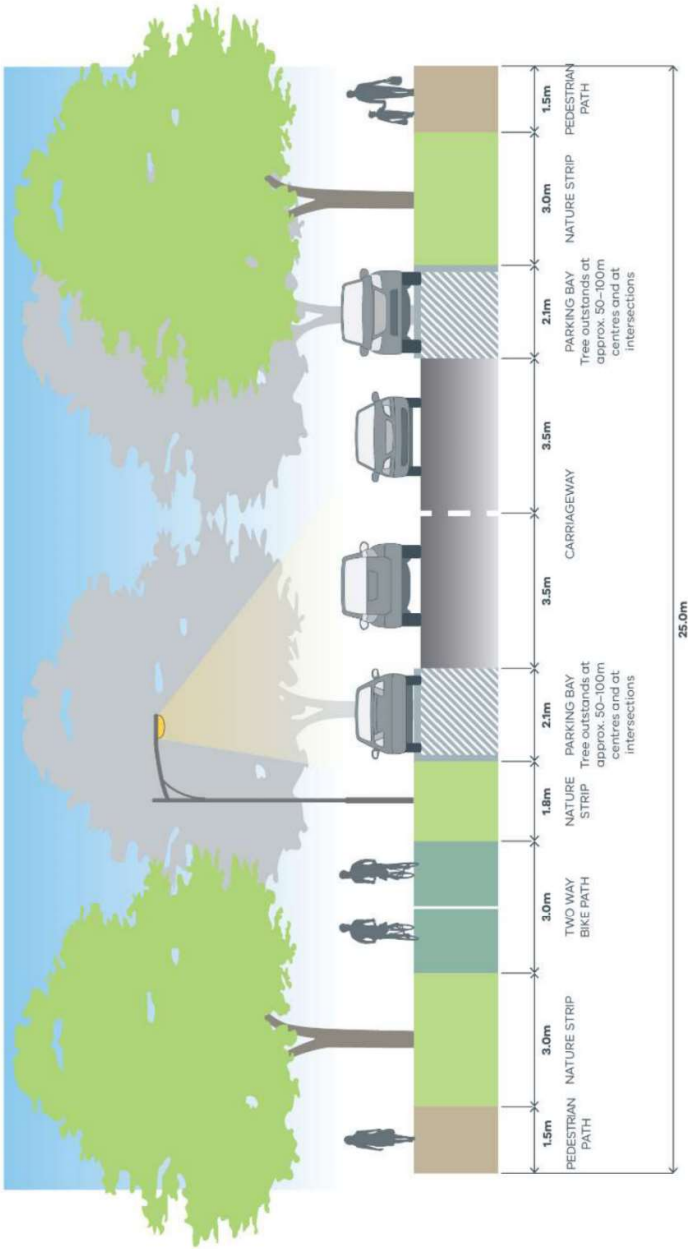


NOTES:

- Include a central median with large canopy trees to create a boulevard effect. Trees are to be centrally planted in median.
- Topsoil used in central medians is to be sandy loam, with a minimum depth of 200mm. The surface of medians is to be free-draining with a minimum cross fall of 2%, and is to be planted with warm season grasses.
- In areas where high pedestrian volumes are expected (eg. around schools and town centres), central medians should be paved with harder wearing surfaces such as granitic sand or other pavements.
- Any garden beds in central medians are to be offset 1.5m from back of kerb.
- Kerb to central median is to be SH2 Semi-mountable kerb.
- Depending on the location of breaks in the median, provide intermediate pedestrian crossing points to accommodate mid-block crossings
- An alternative boulevard treatment can be achieved through a wider verge on one side capable of accommodating a double row of canopy trees.
- Verge widths may be reduced where roads about open space with the consent of the responsible authority.

Connector Street (28.0-31.0m)
Boulevard



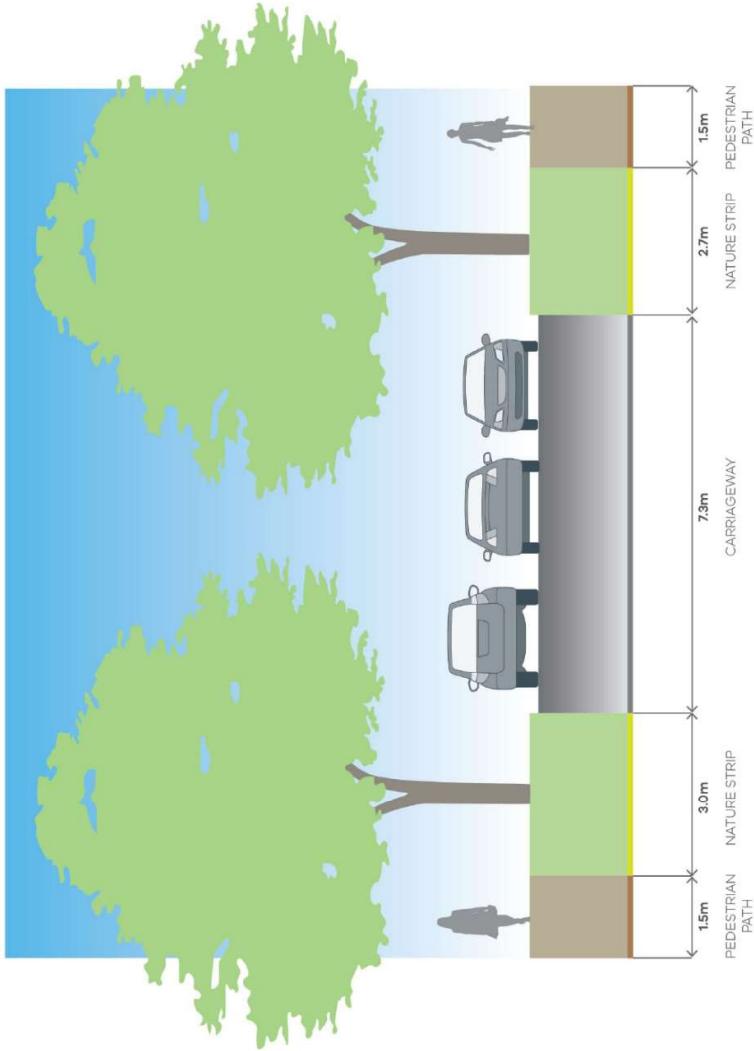


NOTES:

- Minimum street tree mature height 15 metres.
- All kerbs are to be B2 Barrier Kerb.
- Where roads about school drop-off zones and thoroughfares, grassed nature strip should be replaced with pavement. Canopy tree planting must be incorporated into any additional pavement.
- Verges widths may be reduced where roads about open space with the consent of the responsible authority.
- Variations to indicative cross-section may include water sensitive urban design (WSUD) outcome. These could include but are not limited to bioretention tree planter systems and/or median bioretention swales. Such variations must be to the satisfaction of the responsible authority.

Connector Street (25.0m)
Residential

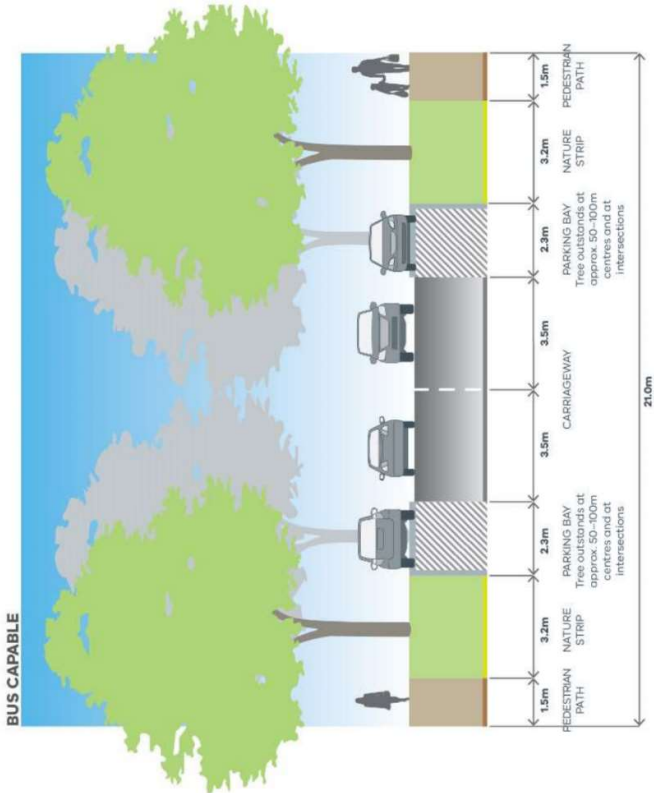




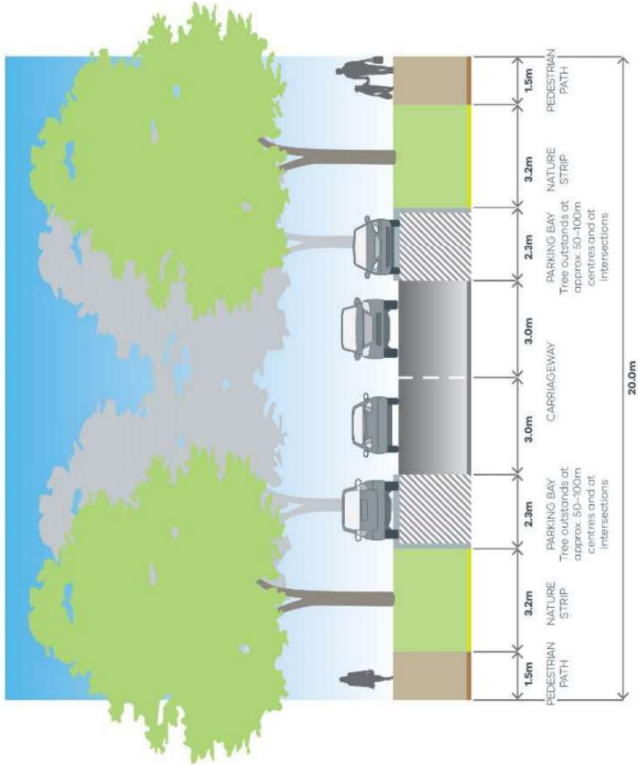
NOTES:

- Minimum street tree mature height 12 metres
- All kerbs are to be B2 Barrier Kerb as per Figure 008 in Engineering Design and Construction Manual for Subdivision in Growth Areas (April 2011)
- Verge widths may be reduced where roads about open space with the consent of the responsible authority.

Local Access Street Level 1 (16.0m)



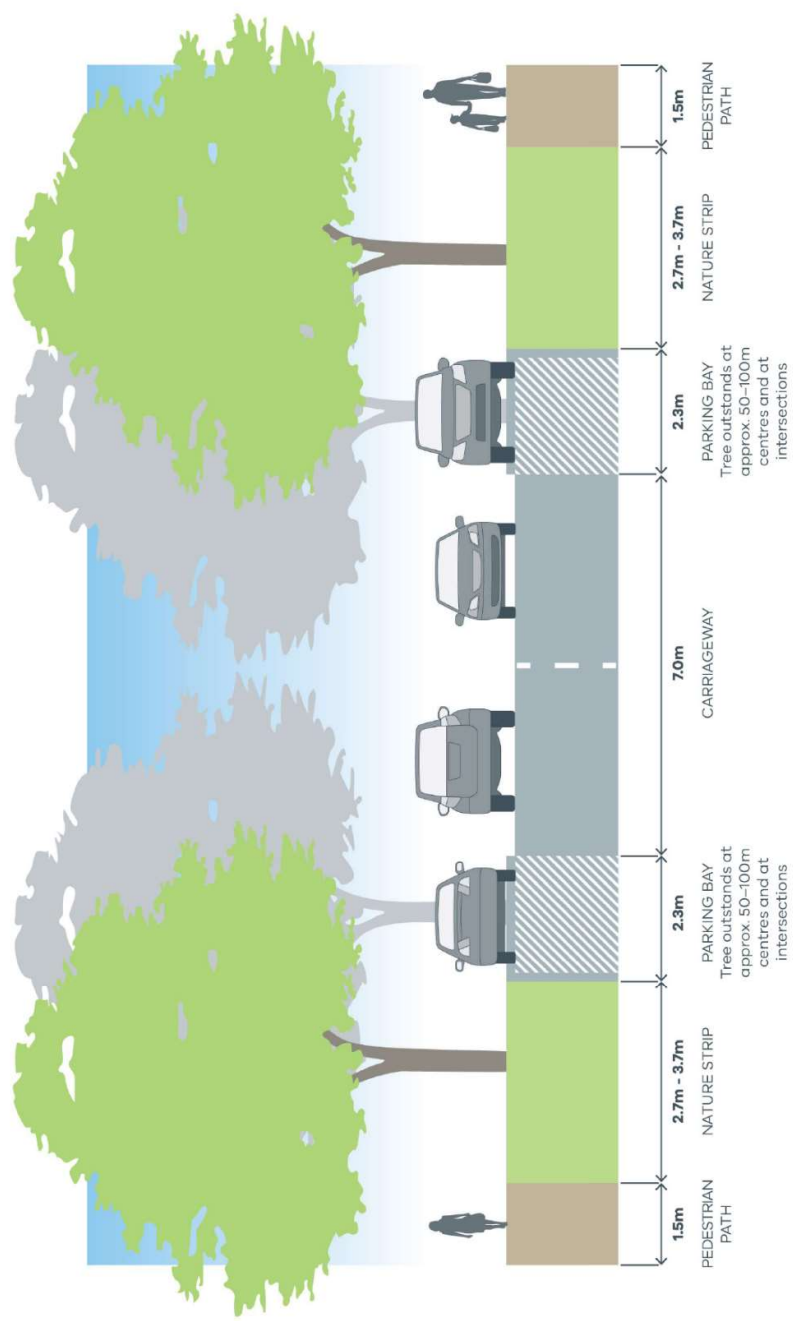
- NOTES:**
- The above cross section applies to Local Access Streets required to accommodate future bus movements.



- NOTES:**
- Minimum street tree mature height 12 metres
 - All kerbs are to be B2 Barrier Kerb as per Figure 008 in Engineering Design and Construction Manual for Subdivision in Growth Areas (April 2011)
 - Verges widths may be reduced where roads about open space with the consent of the responsible authority.

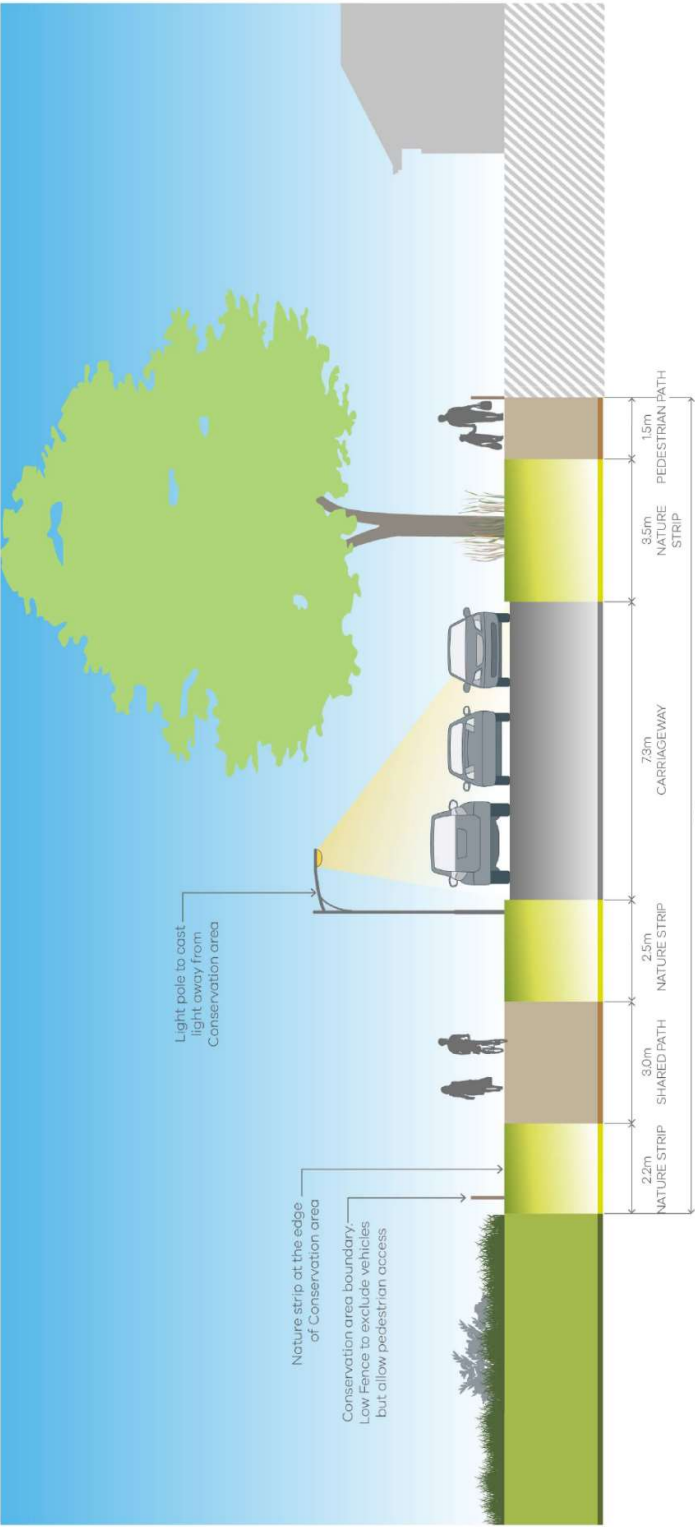
Local Access Street Level 2 (20 - 21m)





NOTES:

- Minimum street tree mature height 12 metres
- All kerbs are to be B2 Barrier Kerb
- Verge widths may be reduced where roads abut open space with the consent of the responsible authority.

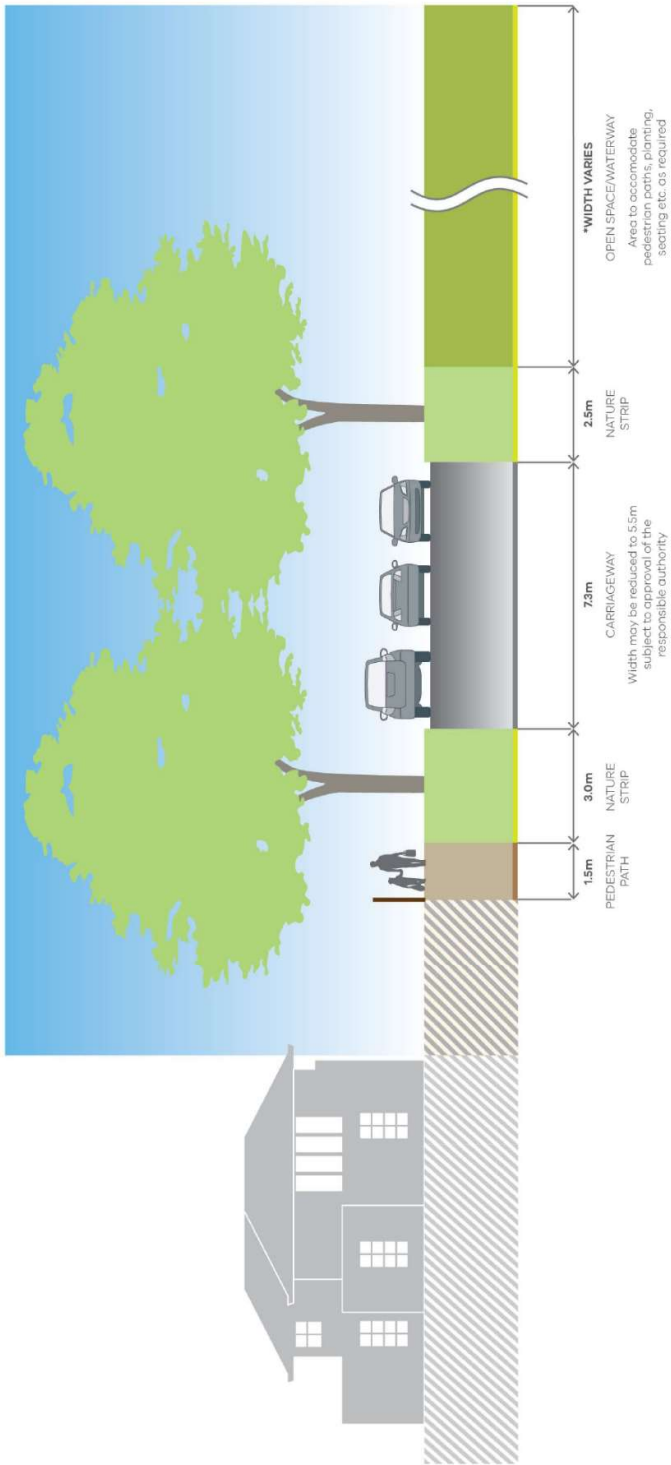


NOTES:

- All trees located within 10m of the conservation area boundary must be of local provenance.
- no trees are to be located within 10m of the conservation area boundary.
- All properties are to be oriented the front the conservation area.
- All necessary fire breaks must be located outside of the BCS Conservation Areas.
- All private property boundaries to be setback by at least 20 metres from the BCS Conservation area boundary.
- Where the road reserve does not directly abut the conservation area, the shared path may be located in the open space or drainage reserve.

Local Access Street (14.5 - 20.0m)
Conservation Interface BCS Area 28

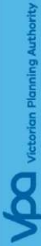




NOTES:

- Where active interfaces to waterways are not provided, waterway corridor widths will be increased in order to ensure maintenance access, to the satisfaction of Melbourne Water
- Where a 2.5m nature strip adjacent to the open space/waterway is not needed for tree planting and/or provision of services, the width may be reduced, to the satisfaction of the Responsible Authority
- 3m wide nature strip may be reduced to the satisfaction of the Responsible Authority and subject to servicing infrastructure.

Waterways & Open Space Interface



4.7 Service placement guidelines

4.7.1 Standard road cross sections

Figures 003 and 004 in the Engineering Design and Construction Manual for Subdivision in Growth Areas (April 2011) outline placement of services for a typical residential street environment. This approach is appropriate for the majority of the 'standard' road cross sections outlined in Appendix C containing grassed nature strips, footpaths and road pavements.

4.7.2 Non-standard road cross sections

To achieve greater diversity of streetscape outcomes in Melbourne's growth areas, which enhances character and amenity of these new urban areas, non-standard road cross sections are required. Non-standard road cross sections will also be necessary to address local needs, such as fully sealed verges for high pedestrian traffic areas in town centres and opposite schools. This PSP contains suggested non-standard 'variation' road cross sections, however other non-standard outcomes are encouraged. For non-standard road cross sections where service placement guidance outlined in Figure 003 and 004 in the Engineering Design and Construction Manual for Subdivision in Growth Areas (April 2011) is not applicable, the following service placement guidelines will apply.

	Under pedestrian pavement	Under nature strips	Directly under trees ¹	Under kerb	Under road pavement	Within allotments	Notes
SEWER	Preferred	Possible	Possible	No	Possible	Possible ³	
POTABLE WATER	Possible ⁴	Preferred	Preferred	No	No	No	Can be placed in combined trench with gas
RECYCLED WATER	Possible ⁴	Preferred	Preferred	No	No	No	
GAS	Possible ⁴	Preferred	Preferred	No	No	No	Can be placed in combined trench with potable water
ELECTRICITY	Preferred ⁴	Possible	Possible	No	No	No	Pits to be placed either fully in footpath or nature strip
FTTH/TELCO	Preferred ⁴	Possible	Possible	No	No	No	Pits to be placed either fully in footpath or nature strip
DRAINAGE	Possible	Possible	Possible	Preferred	Preferred	Possible ³	
TRUNK SERVICES	Possible	Possible	Possible	Possible	Preferred	No	
NOTES	<ol style="list-style-type: none"> 1 Trees are not to be placed directly over property service connections. 2 Placement of services under road pavement is to be considered when service cannot be accommodated elsewhere in road reserve. Placement of services beneath edge of road pavement/parking bays is preferable to within traffic lanes. 3 Where allotment size/frontage width allows adequate room to access and work on a pipe . 4 Where connections to properties are within a pit in the pedestrian pavement/footpath. 						

4.7.3 General principles for service placement

General principles for service placement:

- place gas and water on one side of road, electricity on the opposite side
- place water supply on the high side of road
- place services that need connection to adjacent properties closer to these properties
- place trunk services further away from adjacent properties
- place services that relate to the road carriageway (e.g. drainage, street light electricity supply) closer to the road carriageway Maintain appropriate services clearances and overlap these clearances wherever possible

4.8 Centres External to the Precinct

Table 8: Town Centre hierarchy – external to Craigieburn West Precinct

EXTERNAL TOWN CENTRE	RETAIL FLOOR SPACE	LOCATION AND ANCILLARY USES
Craigieburn Central Major Activity Centre	50,000 m ²	Situated on the north-eastern corner of Craigieburn Road and the Aitken Boulevard, and services the wider Craigieburn area as the Major Activity Centre for the wider region.
Craigieburn R2 Northern 'Neighbourhood' Activity Centre	5,300m ²	Services the Craigieburn R2 PSP area north of Craigieburn Road, existing residential to the east and part of Craigieburn West PSP in the north. Integrated with a Community Centre and provides for one major, one possible minor supermarket and specialty shops and local services including offices, medical, childcare, banking etc.
Craigieburn R2 Southern 'Neighbourhood' Activity Centre	5,700m ²	Services the Craigieburn R2 PSP area south of Craigieburn Road, existing development to the east and part of Craigieburn West PSP in the south. Integrated with a Community Centre, provides a supermarket and specialty shops and local services including childcare, medical, offices, dining.
Craigieburn R2 Northern Local Activity Centre	1,000 m ²	Serves a catchment approximately half the size of a Neighbourhood Activity Centre. Provides a small 'convenience' supermarket and specialty retail shops, along with office, banking and other local services, with opportunity for a residential component.
Craigieburn R2 Southern Local Activity Centre	500m ²	Serves a catchment approximately half the size of a Neighbourhood Activity Centre. Provides small shops, local office uses and other local services, with the opportunity for a residential component.

4.9 Employment generation in precinct

Table 9: Anticipated employment creation within the precinct

LAND USE	MEASURE	TOTAL QUANTITY IN PSP	ESTIMATED JOBS
Community facility CI-01	10 jobs/hectare	1.20	12
Community facility CI-02	10 jobs/hectare	1.20	12
All community facilities	10 jobs/hectare	2.4	24
Local Town Centre LTC1	40 jobs/hectare	3.00	120
All town centres	40 jobs/hectare	3.00	120
Mixed-use	40 jobs/hectare	2.16	86
All mixed-use	40 jobs/hectare	2.16	86
Government primary school (including existing Mickleham Primary)	40 jobs/campus	3	120
Government secondary school	90 jobs/campus	1	90
Non- government primary school	30 jobs/campus	1	60
All education facilities	40 jobs/hectare	5	270
Home based business	0.05 jobs/dwelling	8,308	415
TOTAL ESTIMATED			915



**HUME CITY COUNCIL
SUBMISSION
TO THE DRAFT
CRAIGIEBURN
WEST PSP -
DECEMBER 2020**

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Hume City Council Submission to
Draft for Public Consultation on **Craigieburn West Precinct Structure Plan**, December 2020

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Hume City Council Submission toDraft for Public Consultation on **Craigieburn West Precinct Structure Plan**, December 2020

Council has worked closely with the VPA on developing an urban structure that responds to the unique context of the Craigieburn West precinct with significant remnant trees, important biodiversity and developed urban areas on three sides. The PSP provides an urban structure that builds on this context with a commitment to a network of open spaces, green links for pedestrians and cyclists and the services necessary for over 8,000 new homes.

Council has been involved in the PSP since 2018 and a close working relationship with the VPA has confirmed a PSP that is welcomed by Council. The documentation reflects much of the work that has been undertaken in the planning of the site and responds to several of the objectives and outcomes sought in Hume Corridor HIGAP (adopted by Council in December 2015).

Consistent with HIGAP's vision for Craigieburn West, the following elements of the PSP have a high level of support:

- The urban structure - notably the provision of an open space network that achieves a high level of retention of native vegetation across the site and provides connectivity to the ecological and landscape values of the wider area.
- Pedestrian and cycle connectivity throughout the precinct and connectively to the wider open space network through Mickleham and Craigieburn.
- The provision of a north-south boulevard connector which connects Merrifield West PSP to the future Craigieburn West PSP area.
- The provision of a network of school and community facilities for the future Craigieburn West and Lindum Vale communities.

Given the collaborative working arrangement with the VPA, Council's response is now focussed on new information that has been released for public engagement as well as matters that have been discussed and are nearing resolution. Council's submission clearly articulates changes sought to resolve issues in the PSP and other documents.

We are committed to seek workable solutions with parties where possible and would encourage the VPA to ensure adequate time is made available to resolve outstanding matters prior to referring the PSP to an Advisory Committee.

Town Centre

The town centre is well located proximate to open space links, schools and at the junction of connector roads. The absence of a Town Centre Concept Plan is a notable departure from previous PSPs that included a Concept Plan as a standard to give structure to a town centre's land use and movement network. Council appreciates that the role and form of town centres have been evolving such that revisions to a concept plan closer to development stage is often negotiated by developers and the Concept Plans within the PSP then become redundant.

Council's experience in the planning and development of greenfield town centres underlines the value of a concept plan or masterplan to guide coordinated development outcomes. Where this important middle step is skipped and development undertaken site by site opportunities are missed to create both efficient outcomes (i.e. coordinated site access, consolidated car parking and loading

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areas) and good urban design outcomes (i.e. integrated and shared public realm outcomes: continuous building edge; pedestrian linkages; consolidated signage and landscaping).

Instead of including a Concept Plan within the PSP Council supports this step being undertaken at a later stage closer to development of the town centre (which is often 10+ years after the PSP is approved). It is requested that the UGZ require a Concept Plan before any permits can be granted in either the local town centre or mixed use areas. As the Concept Plan is essential to coordinate land use; a multi modal movement network for pedestrians, cyclists, cars and delivery vehicles and an integrated public realm this requirement is not appropriate in the PSP and must instead be a mandatory requirement in the UGZ.

This approach is consistent with changes made to the Beveridge North West PSP where after a Concept Plan was included in the exhibited PSP it was later removed prior to Planning Panel in August 2020 and replaced by changes. In that case the Panel supported the deletion of the Concept Plan subject to additional PSP and UGZ requirements.

Changes requested to the PSP and planning provisions

Include the following provision requiring a Concept Plan before subdivision or development in UGZ Schedule.

A permit must not be granted to use or subdivide land, or to construct a building or construct and carry out works until a Concept Plan for the whole of the land subject shown as Town Centre and Mixed Use on Plan 4- Place Based Plan of the Craigieburn West PSP has been prepared to the satisfaction of the responsible authority.

A permit must be generally in accordance with the approved Concept Plan.

The Concept Plan must not be prepared in stages.

The Concept Plan may be amended with the approval of the responsible authority.

The responsible authority may grant a permit to use, subdivide land, or to construct a building or construct or carry out works prior to the approval of an Concept Plan if it is satisfied that the proposal will not affect the outcomes for the land sought by the Craigieburn West PSP.

The Concept Plan must provide for the following information, as appropriate:

- *an active, built edge treatment with passive surveillance to sites adjoining north-south and east-west connector street frontages.*
- *the prioritisation of pedestrian movement on key desire lines, and a continuous, separated path of travel within the centre to key destinations, including the location and form of pedestrian crossing of streets and paths across car parks that reflect desire lines.*
- *separated access routes from adjoining linear park and waterway reserves to the town square and north-south and east-west connector street.*
- *mixed use development on the south side of the east-west connector with capacity for ground floor specialist suites (office, medical services, etc) and independent retail and hospitality tenancies.*
- *a town square adjacent to the north-south connector activated by active frontages of retail tenancies.*

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- *A layout that demonstrates visual and physical relationships and connections to adjacent schools, open space so that entrances, driveways etc. align and connect across the street and opportunities for shared or out of core hours usage of car parking is maximized.*
- *car parking areas concealed from public connector street by built form.*
- *Locations and arrangements for the provision of service areas for delivery and waste disposal, including access for larger vehicles and measures to minimise the impact on adjoining neighbourhoods and safe use of car park areas by vehicles and pedestrians.*
- *an overall landscape concept which includes provision for mitigation of the urban heat island effect through use of tall, dense canopy shade trees.*
- *a public space which acts as the central meeting place within the Town Centre. This public space may take the form of a civic square, town park, foreshore park, public plaza space, public marketplace or a similar locally responsive option and*
 - *be located in a position where the key uses of the Village Centre are directly focused on this public space to ensure that it is a dynamic and activated space.*
 - *be designed to function as the identifiable 'centre' or 'heart' with a distinctive local character for both the Village Centre and the broader residential catchment.*
 - *be designed as a flexible and adaptable space so that a range of uses can occur within this space at any one time. Such uses may include people accessing their daily shopping and business needs as well as providing a space where social interaction, relaxation, celebrations and temporary uses (such as stalls, exhibitions and markets) can occur.*
 - *be well integrated with pedestrian and cycle links around and through the Village Centre.*
 - *Maximise solar passive orientation and provide suitable protection from high winds through suitable siting and design techniques.*
 - *Ensure that this public space remains publicly accessible outside regular business hours.*
 - *Establish viewlines into the centre from the north-south connector street, the parklands and the school.*
- *for views to Crows Hill (volcanic cone on the west side of Mickleham Road) and ensure landmark buildings and public spaces present well to key view lines within the centre.*

Make the following changes in the PSP:

1. New requirement: *Applications involving the development of all Local Town Centres and Local Convenience Centres must demonstrate how the proposed design has appropriately considered and responded to the Design Guidelines in Appendix 4.3 and 4.4, having regard to local context and the functional requirements of the proposed activity.*
2. Changes to Table 7 and Place Based Plan (PBP) to refer to Mixed use areas and include mixed use frontage on south side of east west connector adjacent to town centre.

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Kangaroo Management

Council applauds the VPA on the preparation of a precinct wide Kangaroo Management Plan and appreciates that this strategic approach allows a more strategic response to kangaroo management rather than the site by site preparation of KMPs that have been required in the past. A precinct wide approach is critical for the Craigieburn West precinct that creates a boundary between urban and green wedge land and is the final prospect to provide opportunities for kangaroos to safely exit from the existing and planned urban open space areas into adjoining rural land.

A Kangaroo Management Plan (KMP) was released with the PSP background documents and outlines that over 250 kangaroos currently live in the area. In planning for urban development, the KMP proposes that no kangaroo population will be retained across the precinct (although the KMP is unclear on how this population will be removed from the PSP). It anticipates kangaroos have no desire or ability and could not move back in through the street network via Green Wedge linkages or creeks system – recolonising the retained landscapes.

Kangaroo habitat will be maintained in the Craigieburn West precinct, within the BCS land, waterway corridors and other open space areas. The precinct also adjoins and is proximate to permanent sources of kangaroo habitat such as Greenvale reservoir, Mt Ridley conservation area, Aitken Creek and its tributaries and green wedge land running along the precincts 5.4km long western boundary. Given this, the KMP's solution to permanently exclude kangaroos from the precinct is unrealistic. Despite regular culling of kangaroo populations in urban areas, kangaroos continue to be observed in highly urbanised landscapes of Hume. Kangaroos are large mobile animals and will move through the open spaces, street networks and fine habitat of the precinct and adjoining areas.

The KMP proposes removing kangaroos by displacement from the precinct into green wedge areas. Displacement is not proven to be effective as kangaroos will attempt to return to their territory. The only way to eradicate the kangaroo population will be culling across the entire precinct and wider area. This may work temporarily but given the surrounding habitat, it will not permanently eradicate populations from moving into the precinct. Instead, kangaroo management must include a response in the land use framework that enables habitat connectivity from large parcels of conservation and creek reserves as well as maintaining kangaroo numbers through periodic culling.

The delayed delivery of the KMP has meant that it has not been an influence on the Place Based Plan being consulted on. The need for a precinct wide solution for a Kangaroo Management Plan (KMP) was discussed at the CW PSP inception meeting over two years ago. It is disappointing that the first time that Hume City Council has had the chance to review the KMP was via the public consultation process. In the absence of a KMP Council has offered suggestions in the design of the PBP to enable kangaroo movement. As the KMP has not offered a realistic option for kangaroo management the final PBP for Craigieburn West must allow for movement and maintenance of kangaroos through the landscape including:

- Providing continuous open space linkages connecting waterways from the urban areas to the east to green wedge areas immediately to the west of the precinct (including through changes to the configuration of the BCS area – see further discussion below).
- Managing development sequencing that recognises kangaroo habitat and connectivity.

Council supports the following recommendations of the KMP:

- the recommendation to control development direction at a precinct wide scale to avoid landlocking of Kangaroo populations.

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- speed limits reduction and virtual fencing recommendations. As the majority of the surrounding roads are managed by the Department of Transport (VicRoads), DOT should be co-listed as the responsible authority for this action. Written commitment is required by the Department of Transport to ensure this recommendation will be implemented once development commences in the Precinct.
- the use of other Fauna Sensitive Urban design measures including, culverts, overpasses, road signage, virtual fencing at waterway road crossings in the precinct and in strategic locations on Mickleham Road. These measures should also be clearly mandated in the design of creek crossings and locations clearly identified in the PSP including where these should link across Mickleham Road. The VPA should gain commitment from the Department of Transport that these crossings will be installed in the future design of Mickleham Road.

Changes requested to the PSP and Kangaroo Management Plan

1. Include provisions in Planning controls to:
 - Manage development direction, staging and sequencing to prevent land locking kangaroo populations and provide continued kangaroo habitat and movement within the precinct and wider area.
 - Require design and construction of culverts and bridges in properties 9 and 10 to support fauna (including kangaroo) movement along waterway corridors.
 - Require planning approval for the removal of all dams, reservoirs and bodies of water that provides for:
 - Assessment of the impact of removal of water points on kangaroo populations to ensure kangaroo populations are not at immediate risk of becoming land locked. This could cause significant animal welfare issues. In addition, dam removal must not occur in summer, when this could cause animals to become at risk of dehydration and cause them to move erratically through the landscape
 - fill and compaction in accordance with relevant Australian Standards Dam filling and under level 1 supervision.
 - The land must be filled in a manner that does not:
 - Cause a nuisance on nearby land through the emission of dust.
 - Adversely affect the drainage of adjacent land including through sediment and altered run off.
 - alter overland flow paths.
 - Following completion of fill and compaction, compaction test results and a report prepared by a suitably qualified geotechnical engineer, must be prepared to the satisfaction of the responsible authority, certifying that the filling has been properly carried out.
1. Amend the KMP to respond to Attachment 1: Hume City Council comments on Kangaroo Management Plan, December 2020

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Improvements to the configuration of the Biodiversity Conservation Area

A realignment of the boundaries of the Biodiversity Conservation Area 29 has been under consideration in recent months involving discussions with Stockland, DELWP and VPA. The revised boundaries better respond to current on site biodiversity values as assessed by Council and DELWP officers in April and June 2020.

The realignment will require approval from the Federal Department of Agriculture, Water and the Environment (DAWE). DAWE has provided in principle support for the changes and a formal request is due to be submitted imminently. The revised BCS area is generally supported by all parties on the basis that the process to gain Federal approval for the changes does not stall the progress of the PSP amendment.

The reconfiguration of the BCS area also offers opportunities to relocate the sports reserve SR-01 to collocate it with the BCS. Council supports the location of SR-01 adjacent to Mount Ridley Road as shown in the November 2020 PSP.

Alternative locations to the north of the BCS can also be explored prior to Advisory Committee hearing subject to:

- suitable siting and layout to allow for sports fields, district level playgrounds, pavilions, as well as appropriate access points and carparking.
- co-location with community facility CI-01, while providing CI-01 with frontage to the north-south connector street.
- minimising removal of trees.
- liaison with other affected parties.

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Transport

Council has worked extensively with the VPA over the past eighteen months to create a road network for Craigieburn West that:

- provides safe and efficient movement to key destinations within the PSP.
- provides key connections from surrounding precincts to Mickleham Road.
- promotes active transport, working and cycling movement; and
- future proofs potential public transport routes.

Given the linear nature of the Craigieburn West PSP, its relatively large size and the fragmented land ownership across much of the PSP, designing a responsive road network faces considerable challenges.

Council's general support for the proposed road network has been contingent on the traffic modelling and testing of the network that was being conducted by onemilegrid in their Transport Impact Assessment. Council has consistently requested details on the findings of the Transport Impact Assessment over the past year to inform a position on key elements of the urban structure, however Council has not been able to review the report until the November 2020 during the community consultation process.

Council's review of the onemilegrid report has identified significant issues with the assessment that was conducted, which in turn has given rise to significant concerns regarding the adequacy of the proposed road network.

Concerns with the Transport Impact Assessment

Council's review of the Transport Impact Assessment identified a number of significant concerns in the data and assumptions used by onemilegrid in their modelling, or in many cases a lack of detail to allow for a comprehensive assessment and provide confidence in the proposed road network.

In preparing a response to the PSP released in November 2020, Council has sought advice from GTA Consultants regarding onemilegrid's Transport Impact Assessment. GTA's advice is attached as part of Council's submission (refer Attachment 2). Several key areas of concern are summarised are discussed below followed by a set of principles that has informed a preferred road network.

1. Traffic modelling

There is a lack of detail regarding the traffic model that was used by onemilegrid. GTA's advice to Council indicates that a bespoke transport model was likely used, which would have been generated as an excel spreadsheet.

While this type of traffic modelling might be appropriate for site specific assessments, Council has significant concerns regarding its appropriateness for assessing the road network for a PSP. These concerns stem from a spreadsheet based model being:

- unable to assess the impacts on the road network if key sections of the network are not delivered due to the challenges of staging across areas of fragmented land ownership.
- unable to test the impact of upgrading or downgrading the road hierarchy in different sections of the network.
- more prone to errors when inputting and updating data.

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Council therefore does not have confidence that the traffic modelling undertaken has been able to adequately test the proposed road network and ensure it has the capacity to support the future Craigieburn West community and residents in the surrounding precincts. Council's concerns regarding the traffic model are further compounded by the errors in the assumptions used in onemilegrid's assessment outlined below.

2. Assumed lot yield

In their calculations of the number of trips generated across the PSP, onemilegrid assumed an average lot density of 600 square metres for dwelling across the PSP. This estimates the number of dwellings in Craigieburn West will be approximately 6,150. This dwelling yield contradicts the density targets specified in the Table 2 (p 17) of the PSP.

Using an average lot density of 20 dwellings per hectare to accommodate the PSP's targets of 18.5 dwellings per hectare and 26.5 dwellings per hectare within the walkable catchment of the town centre, Council's anticipates a more realistic dwelling yield to be approximately 8,200 dwelling across the PSP.

Given that the Transport Impact Assessment already identifies Vantage Boulevard as exceeding its capacity and that several roads approach their upper volume limits, Council has significant concerns regarding the adequacy of the road network to accommodate an approximately 33% increase of dwellings across the entirety of the PSP.

Seeing as Vantage Boulevard is already exceeding its capacity utilising a low estimate of dwellings in the PSP, GTA's advice to Council recommended that an additional north-south connector running parallel to Vantage Boulevard should be considered to accommodate these traffic volumes. Council therefore recommends that the north-south boulevard connector street should continue south of Elevation Boulevard to Dunhelen Lane, rather than reverting to the access street level 2 as proposed.

3. Land use assumptions and trip generation rates

Onemilegrid's traffic generation assessments also do not appear to take into consideration the impacts of non-residential land uses within the PSP, particularly the presence of five schools, two community facilities, two sports reserves and a local town centre with 6000m² of retail floorspace.

The report justifies not specifically considering the potential trips generated for these non-urban uses by adopting a relatively high trip generation rate for the residential components of their modelling. To this end, onemilegrid assumes a residential trip generation rate of nine trips per dwelling per day.

Council's concern is that while onemilegrid adopted a higher traffic generation rate of nine trips per dwelling that might justify the exclusion of non-residential trip generation modelling, the report states that a trip generation rate of 10 trips per dwelling per day is generally accepted for single dwellings in outer suburban areas. The report notes that lower traffic rates are often recorded in areas with good public transport and higher densities as justification for their use of the reduced rate of nine trips per dwelling.

GTA's advice to Council states that the nine trips per dwelling rate adopted by onemilegrid may be acceptable, however they note that this rate is also on the basis of the PSP having lot sizes of 600 square metres and dwellings having good access to the bus network. While most dwellings within the PSP might fall within a 400 metre walkable catchment radius of a

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potential public transport route, Council questions the extent to which dwellings in several areas of the PSP will have meaningful access to the public transport network.

Additionally, given the assumptions influencing the traffic volumes modelled, Council has concerns regarding the safety and accessibility of the intersections within the PSP for pedestrians and cyclists. This is of particular concern around areas with schools and the activity centre, which was not specifically modelled and the potential need for signalised intersections in these locations does not appear to have been considered.

Council's concerns are that these assumptions have a potentially compounding influence which culminates in a lack of confidence that the adequacy of the road network has been appropriately tested or that the network will be safe and efficient.

The issues summarised above and outlined further in the advice received from GTA have resulted in Council's view that:

- **The proposed road network will not support the dwelling yield anticipated and land uses proposed within the PSP.**
- **The impacts of land fragmentation and sequencing cannot be appropriately understood through the traffic modelling that was conducted and consequently are not appropriately addressed or managed through the PSP.**
- **The need for potential signalised intersections on connector streets were not appropriately considered, particularly around schools, community centres and the town centre.**

In order to begin to address these issues, Council has worked with mesh planning to explore solutions based on the following principles:

1. Creating movement choices and connecting neighbourhoods
2. Managing sequencing
3. Creating a safe and equitable movement network for all
4. Responding to features in the urban structure

Figure 1 (also included in Attachment 3) and Table 1 of this submission provide changes to the PSP that incorporate these principles throughout the PSP.

Creating movement choices and connecting neighbourhoods

As it is anticipated that the connector road network will exceed capacity Council has developed a local road network that allows additional access to arterial roads and movement between neighbourhoods. Council acknowledges that south of Craigieburn Road the PSP has identified a number of local access streets connecting onto Mickleham Road via left-in/left-out intersections (refer Plan 5, p 18 of PSP).

Figure 1 of this submission includes a well distributed network of local access streets which incorporate left-in/left-out intersections onto the arterial road network north of Craigieburn Road. These local streets will provide some local movement opportunities for local residents away from the connector road network and encourage movement along the arterial road network. A resolved network at PSP stage also allows these outcomes to be resolved early enabling efficient permit approval processes and minimising speculative proposals for arterial road access.

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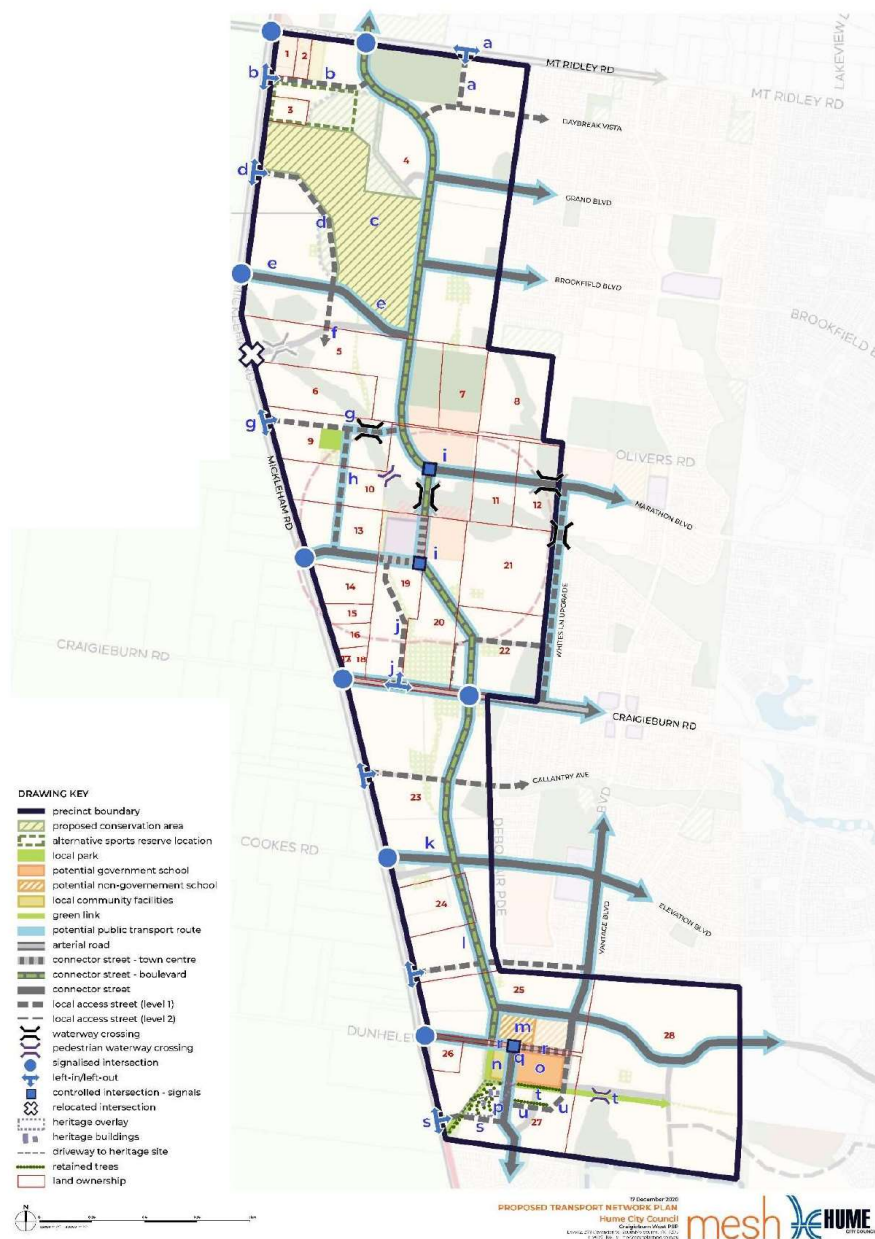


Figure 1: Transport Network Plan revisions

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The structure of at least part of the local street network is also important to ensure development is coordinated across parcels in separate ownership and create efficient and convenient access between local neighbourhoods. The ownership pattern in the walkable catchment has narrow lots fronting onto Mickleham Road and by indicating a local road network an efficient and convenient access between local neighbourhoods can be ensured regardless of the timing of individual development parcels.

Managing sequencing

Council welcomes the recommendation of the draft Guidelines for Precinct Structure Planning in Melbourne's Greenfields which supports a high-level strategic plan that can be used to guide staging of development. It is noted that the Craigieburn West PSP has not included a sequencing plan to manage coordination of road and community infrastructure.

This is a matter that has been raised by growth area Councils for a decade and we believe many of the issues of isolation, high cost of living, lack of access to jobs and services (and the social issues that arise from this) experienced by new communities stems from the ad hoc sequencing of development requiring Councils, road and public transport authorities and other servicing agencies to respond to multiple development fronts without any means to forecast development rollout and consequent servicing needs of population growth.

This is especially critical for a large, linear precinct like Craigieburn West where ad-hoc sequencing of development has a high chance of isolating communities from one another, as well as from community and retail services for many years. With multiple landowners responsible for development within the precinct delivery of key land uses or public transport routes could be held up for years pending a central section of the north-south connector road (for example the section in property 14).

The proposed local road network goes some way to manage connectivity once the precinct has been fully developed, a sequencing plan is considered essential to maximise the opportunities for new residents to be able to efficiently move around in private vehicles, by foot or bicycle or on a public transport system as early as possible.

Creating a safe and equitable movement network for all

Safe and equitable movement opportunities for residents and visitors of the precinct is fundamental to 20 minute neighbourhood outcomes. At a minimum, access to the five schools within the precinct should prioritise safe movement of pedestrians and cyclists. The retention and extension of the Mickleham Primary School will allow the ongoing protection of heritage buildings and serve a school population that has grown with enrolments from new urban areas to the north and west. However, the siting of the existing school on two arterial roads requires access to the school to be reoriented to ensure safe pedestrian and cyclist access and vehicle drop-off/pick up. Council recommends the PSP require a loop road system to manage safe vehicle movements. Figure 1 designates a local access street with a left-in/left-out intersection onto Mickleham Road to provide a range movement options and access points for school road traffic.

The PSP shows controlled intersections where two connector roads connect and generally these would be developed with roundabouts to manage traffic movements unless a signalised intersection is explicitly nominated. (In Council's experience unless a signalised is explicitly required there is an implied perception that a roundabout is a suitable intersection and a strong reluctance to provide signals at planning application stage). However, where controlled intersections adjoin a school priority for the safe crossing for pedestrian and cyclists is a reasonable priority and best provided by

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traffic signals. This is particularly important adjacent to the Year 7 to 12 Secondary School where students are more likely to walk and cycle independently. Signals will also be needed at the connector road intersection to serve the Prep to Year 6 Primary School and the pedestrian movement to the adjacent town centre. A signalised intersection is also important in the southern hub to provide safe crossing to the two schools and community facilities. In this case, if this intersection is at the junction of lower order roads or the road network is designed to slow traffic either full signals or pedestrian signals could be appropriate at this junction.

A strength of the PSP is the inclusion of off-road shared paths including a linear park running north-south through the precinct and east-west linear parks to key destinations such as Mickleham Primary School and Mount Aitken hilltop park. Guideline 15 is a valuable inclusion that supports further pedestrian and cyclist connections at planning permit stage. A variety of local transport modes supports a safe and equitable movement network for all.

Responding to features in the urban structure

Council appreciates the resolution of an effective transport network in Craigieburn West has been heavily influenced by existing development to the east, particularly the continuation of east-west roads, the established arterial road network and growing traffic congestion in the wider area. The PSP itself also presents opportunities for a movement network that embraces PSP features, links to key land uses and recognises opportunities for distinctive views, vistas and gateways. A number of revisions in the reworked transport network of Figure 1 have been designed with these outcomes in mind. Table 1 identifies where changes are proposed to respond to features in the urban structure.

Table 1: Connectivity and access changes to Transport Plan

Key	Proposed change	Basis for change
A	Left in/left out at Mount Ridley Rd	Provides access to arterial road network for the immediate neighbourhood and reduce pressure on north-south connector
B	Access street and left-in in/left-out at Mickleham Road	Provides options for school traffic to access precinct from arterial road network and allow for traffic flow
C	Preferred boundaries for Biodiversity Conservation Area 29	refer discussion at page 7
D	Local road along BCS boundary and left-in in/left-out at Mickleham Road	Provides neighbourhood access to arterial road network to reduce reliance on east-west connector
E	Connector road moved north and adjacent to southern boundary of BCS	Provide arterial road access without creating an awkward land parcel, provide access for the neighbourhood north of waterway and designed to provide viewline and act as buffer to bcs
F	Access from east west connector to the south	Enables efficient access to arterial road network for neighbourhood as well as efficient connections to neighbourhood to the north
G	left-in in/left-out at Mickleham Road with link across waterway to north-south connector	Distributes movement network for large neighbourhood north of east-west connection to activity centre; convenient connection Marathon Blvd and secondary school and centrally located waterway crossing
H	North-south local road	Creates north-south connectivity across fragmented land ownership and provides movement options for immediate neighbourhood onto arterial and connector road network bypassing the town centre
I	Signalised intersection	Refer to discussion on page 12

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Key	Proposed change	Basis for change
J	North south local road to left-in in/left-out at Craigieburn Road	Provides neighbourhood access to arterial road network and reduce pressure on connector road network
K	Realign extension of Elevation Boulevard to align with Cookes Road to the east	Create a four way intersection to Cookes Road, avoid road construction on site of existing dam and better road alignment for passive solar lot design
L	Continuation of connector street – boulevard south of Elevation Blvd	Distributes north-south traffic in response to high traffic volumes on Vantage Boulevard
M	Relocate non-government school to the east	The rearrangement of land use within the southern area provides a stronger gateway element from the west and south and creates a stagger in connector road network to encourage driver choices in favour of arterial road network
N	Relocate CI-02 to the west	Refer to 12, and provides opportunity for integration with uses of heritage buildings
O	Relocate P-6 school	Refer to 12, and incorporates road of trees
P	Recognise heritage precinct on PBP	Allows consideration of heritage precinct in the location of road and linear park links and complementing community service land uses
Q	Signalised intersection	Refer to discussion on page 12
R	Tailored local access street	cross section design to be tailored to respond to land use and manage low speeds and include landscaping and generous pedestrian and cyclist shared paths
S	Access street and left-in in/left-out at Mickleham Road	provides arterial road access for local community and designed to frames heritage precinct
T	East-west pedestrian-cycle link or option for local access street	Replaced road link with pedestrian link (and potential road subject to bridging creek); creates safe active transport options to schools and captures viewlines to Mt Aitken
U	Continue Vantage Boulevard as key local road	Encourage Vantage Boulevard traffic to connect to the west towards Mickleham Road and retain options to the south along lower order road, avoiding the schools/community facility precinct
V	Include a bus capable road cross section for Whites Lane	Refer to discussion on page 12

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Priority delivery of Mickleham Road duplication

The draft PSP Guidelines have articulated the opportunity and challenge around infrastructure coordination and acknowledge that “a PSP can prevent development taking pace until infrastructure is guaranteed to be provided” (p 71). The upgrading of Mickleham Road has reached capacity and it is appropriate to enact a precautionary approach that prevents development in the Craigieburn West precinct until funding is committed for the design and delivery of the for the duplication of Mickleham Road from Donnybrook Road to Somerton Road.

Mickleham Road is a state managed, arterial road and runs along the western boundary of the Craigieburn West PSP. It is the primary north-south arterial for the northern growth region of Hume connecting communities in southern Hume and Melbourne Airport with the suburbs of Greenvale, Mickleham and Craigieburn – two of Australia’s fastest growing communities. As an arterial road, any new and upgraded intersections to Mickleham Road must be planned for at PSP stage however other improvements, such as duplication, are reliant on State and Federal government funding and are not compelled through the PSP process.

In the six years between 2013 and 2019 traffic volumes on Mickleham Road have more than doubled with an increase of 54% from 15,313 vehicles per day (vpd) to 28,590 vpd in 2019. This level of traffic exceeds the capacity on Mickleham Road as a single lane rural road. The trigger for duplication of arterial roads of 20,000 vpd has been exceeded by almost 50%. Liveability in the Hume corridor is significantly compromised with the community regularly experiencing congestion on Mickleham Road and lengthy delays in their commute to work, school and in accessing essential services. Urban traffic also spills onto other rural roads in the area west of Mickleham Road, including Craigieburn Road, Bardwell Drive, Oaklands Road, and Somerton Road, as motorists seek alternative routes to bypass the congestion on Mickleham Road.

Residential development in the Hume corridor has progressed much more rapidly than State Government had anticipated. In less than 8 years the Merrifield West PSP area is over two thirds developed despite having an estimated residential capacity in excess of 15 years. The PSP areas of Craigieburn and Greenvale were anticipated to provide residential development out to 2030 yet very little undeveloped capacity remains. Overall, the State Government has approved six PSPs (refer Figure 2) in the last ten years allowing for 19,000 dwellings. As a result of this very rapid development traffic volumes have increased exponentially in the Hume corridor and the road network is failing to meet the safe and convenient travel needs of the community who struggle with long commutes.

As shown in Figure 2, the majority of these depend upon and/or are highly proximate (within 2km) to Mickleham Road.

Despite the State Government expanding the UGB and fast tracking residential development in the Hume corridor over the last 10 years there has been no State or Federal investment in any arterial roads north of Somerton Road. At the same time Council along with local developers have delivered Aitken Boulevard (a new north-south arterial connection between Donnybrook and Somerton Roads) and connector roads. This infrastructure has provided great additional benefit.

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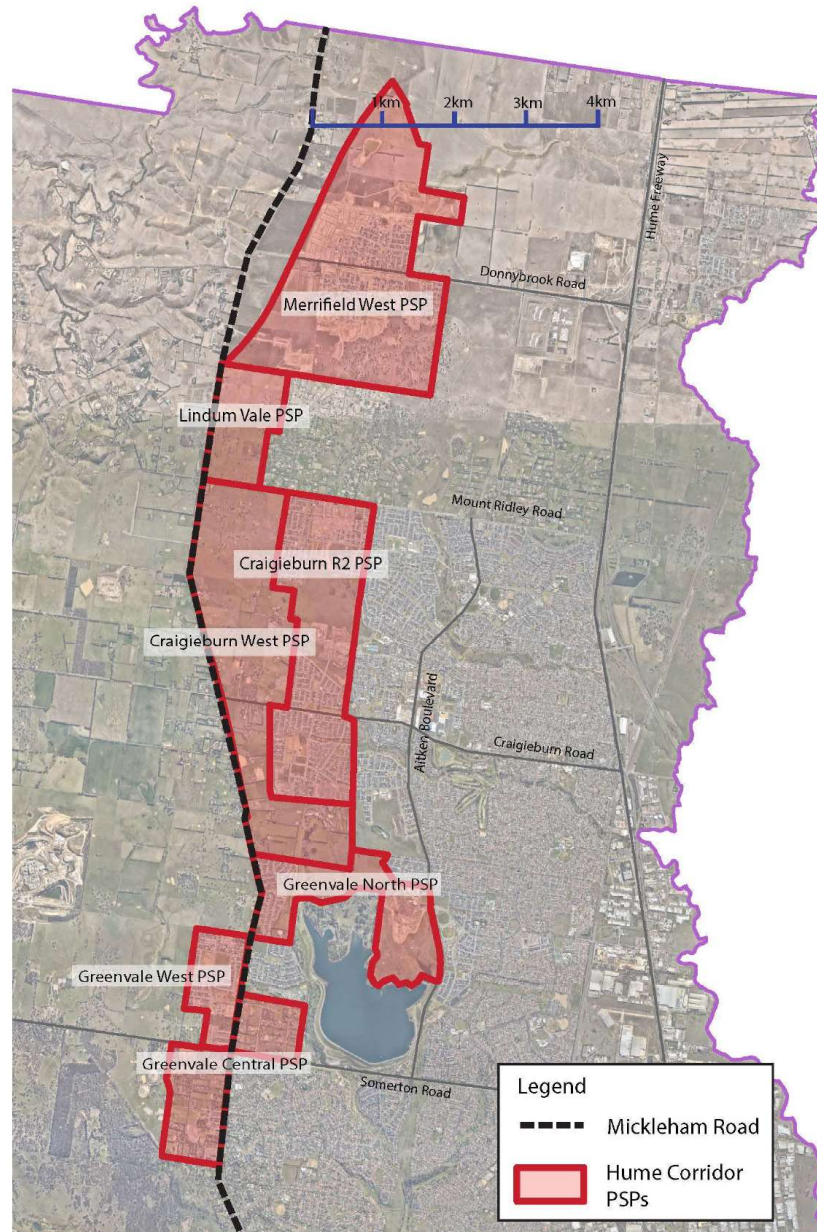


Figure 2: PSP Precincts North of Somerton Road

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Council has been advocating to the State Government to fund and prioritise the much needed duplication of Mickleham Road for more than ten years. Despite Council's frequent efforts to highlight road traffic and safety issues in the Hume corridor, the State Government has still not provided a compelling commitment to duplicate Mickleham Road and provide a road network that reflects the resident population that has moved into Melbourne's northern growth area.

The Craigieburn West PSP will allow more than 8,000 additional dwellings and will further exacerbate the congestion on Mickleham Road resulting in development exceeding the projected limit of development by more than 30%. Craigieburn West precinct will also require the construction of six signalised intersections to access Mickleham. There are a number of benefits to undertake these works at the same time as the future road duplication.

Commitment sought by Council prior to approval of PSP

Given the unsustainable impacts on traffic movement in the wider corridor, Council does not support approval of the Craigieburn West PSP until the State Government makes a budgeted commitment for the design and delivery for the duplication of Mickleham Road from Donnybrook Road to Somerton Road.

Whites Lane

Whites Lane forms the boundary between the Craigieburn R2 precinct and the Craigieburn West precinct with the boundary effectively running down the centre of the existing road reserve. The Craigieburn R2 was silent on the role and cross section design of Whites Lane and a recent planning permit application has allowed Council and Stockland to work on the design of Whites Lane.

At the same time Council has worked with the VPA to resolve the status of Whites Lane that straddles the Craigieburn West and Craigieburn R2 PSPs. Council welcomes the inclusion of a cross section for Whites Lane in the Craigieburn West PSP and the amended Craigieburn R2 PSP. The inclusion of these cross sections will provide consistent guidance to enable the appropriate development of Whites Lane, which will be a key road connection for the Craigieburn West and Craigieburn R2 communities.

However, given Council's concerns regarding the capacity of the road network as outlined above, it is imperative that opportunities to relieve the network pressure across the PSP are maximised including by utilising Whites Lane that contributes a valuable north-south link to the network. This can begin to be done by ensuring the potential for public transport and active transport across the road network of the entire PSP.

Specifically, Whites Lane can help to relieve the network pressure by modifying the cross section and transport plan in both PSPs, see attached updated cross section (Attachment 4), summarised below changes:

- Update the western pedestrian path to be a 2.5 m shared path
- Rather than allowing the verge widths to vary where road abuts open space, set nature strip width at 3.2m and allow for variance to instead be the removal of indented parking where road abuts open space.
- Update Transport Plan to show Whites Lane as an access street level 2 that is bus capable and include a creek crossing where road reserve crosses Aitken Creek.

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Geotech and hydrology

Since May 2018 Council has discussed with the VPA the importance for robust geotech and hydrological assessments to be undertaken across the PSP. Council's concerns have arisen from the challenges that officers have experienced implementing PSPs when soil and hydrological issues are not comprehensively investigated and understood which can lead to challenges to development viability and poor drainage management outcomes.

Following the drafting of the *Hydrogeological, Salinity, Acid Sulphate Soil and Geotechnical Assessment February 2019* prepared by Beveridge Williams, Council has consistently raised concerns relating to the potential presence of sodic and dispersive soils within the Craigieburn West PSP area.

Council has also raised concerns regarding the recommendation from the Beveridge William's report for further assessments to be undertaken for the central portion of the PSP that is impacted by shallow groundwater, refer figure 3. The report states:

Prior to detailed design (drainage, subdivision or underground infrastructure alignments) a precinct wide OR development specific physical groundwater investigation through the areas identified with shallow (<5 m) and potential saline waters (TDS concentrations indicated between 7,000 - 13,000 mg/L), as shown on Figures 4 and 5 respectively, including a gauging/sampling round to confirm the groundwater flow direction and hydrogeological conditions to confirm risk of groundwater intrusion during excavation or bulk earthworks within the precinct area and the management measures required for any water intrusion which may be encountered.



Figure 3: Extract from the Beveridge Williams Report areas in red

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Council has maintained since the Co-Design Workshop in August 2019 that these matters should be resolved prior to the exhibition of the draft PSP. This position was reiterated through our submission during Agency Consultation in June 2020.

To address the potential presence of sodic and dispersive soils, Council supports the proposed UGZ schedule 12 requiring the preparation of a sodic and dispersive soils management plan for any subdivision or bulk earthworks application, in keeping with the recommendations of the Beveridge North West PSP Panel Report.

In regard to the requirement for further groundwater investigations, Council maintains that these additional investigations should not be left to the permit stage where they will be undertaken on a site-by-site basis. Due to the fragmented landownership across the affected areas, a site-by-site approach will not result in the coordinated or cohesive response that is necessary. Such an approach would confine the assessments and any subsequent recommendations to a single property. The recommended site-specific actions, however, would likely have implications for the other affected properties. Additionally, the results of these investigations could necessitate alterations to the urban structure in this area and should therefore be considered before the PSP is finalised.

Undertaking these investigations prior to the finalisation of the PSP will allow for a holistic assessment and recommendations that can be incorporated into the PSP and UGZ schedule to ensure this area can be safely developed. This further investigation should include an assessment of the dam to the north of LP-09 and the impact of its backfilling and development on the shallow groundwater in this area and the potential impact on the river red gums within LP-09.

Changes requested to the PSP

The VPA conduct additional investigations into the central area of the PSP affected by shallow groundwater and incorporate any recommendations into the PSP and UGZ schedule.

Bushfire setbacks**LP-09 buffer setbacks**

Council acknowledges the requirements of State Policy at Clause 13.02-1S of the Hume Planning Scheme and the necessity to prioritise the protection of human life in all planning decisions.

Council acknowledges that to reduce the risks posed by bushfire, that setback buffers will be required from identified hazard areas such as densely vegetated open space. Council has maintained for the past year that there is the potential to reduce the 33 metre setback buffer required for LP-09 and that this warrants further investigation by the VPA. This position was also formalised in our submission during Agency Consultation.

The Australian Standards for construction of buildings in bushfire prone areas (AS 3959-2018) consider appropriate setbacks from woodlands to be between 19 metres to 33 metres. Factors that can affect the required setback include the size of the vegetated area, the extent of the understory fuel load and their anticipated effect on fire behaviour.

The 33 metre setback originates from the Bushfire Development Report February 2020 prepared by Terramatrix. There are a number of assumptions in the Terramatrix report that should be re-examined. Key among these assumptions are the assessment of LP-09 as an unmanaged patch of

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vegetation, rather than a Council managed park as identified in the concept plan in the PSP and they also assessed LP-09 at a size double that which is shown in the Place Based Plan.

Council believes that the Craigieburn West PBP has now been sufficiently resolved to warrant the buffer to LP-09 to be re-examined by Terramatrix, with the potential to reduce the buffer distance to 19 metres based on the intended use and management of this reserve.

Council anticipates that a re-examination of LP-09 will result in the capacity to reduce the setback buffer required for LP-09. This will ensure that the development potential of a high amenity area of the PSP is not unduly limited, while also maintaining the necessary requirements to protect human life.

The VPA has stated that following the consultation period of the PSP, that they will likely update the bushfire report or upon receipt of comments from the CFA. It is Council's expectation that the setback requirements for LP-09 are re-examined post-consultation and the PSP and bushfire report are updated accordingly.

Alternatively, if the VPA is unable to undertake this assessment, the PSP should allow for this additional work to be undertaken at the permit application stage by landowners with the potential to amend setback buffer distances subject to the approval of the responsibility authority.

Changes requested to the PSP and the Terramatrix Report

The assumptions that informed the recommendation of a 33 metre setback for LP-09 should be re-examined in the Terramatrix Report to include an assessment of the following:

- The size of LP-04 as per the proposed PBP.
- The future state of LP-04 as a local park, rather than in its current state as an unmanaged patch of vegetation. This would include an understanding of Council's future use and management regime for the park and its understory fuel load, as per the concept plan in the PSP.
- The implications of the above on LP-04's classification as a Woodland and/or the potential setback buffers required.

Alternatively, the PSP should be updated to allow for the buffer setbacks to be varied subject to approval by Council, using the following wording or similar:

Buffers identified on Table 4 may be reduced where it can be demonstrated that the fire threat has been reduced or mitigated and may be amended to the satisfaction of the responsible authority and the CFA.

BCS buffer setbacks

Council also raised concern during Agency Consultation regarding the reliance of a 19-meter buffer to the large BCS Area 29 buffer if it the Area is not secured through the PSP. Currently the vesting of the BCS Area to a public authority is not a fate accomplished in the design of the planning tools for urban BCS Areas as the MSA allows conservation reserves to remain in private ownership subject to a Section 69 agreement for ongoing management under the *Conservation Forests and Land Act 1987*.

Council is concerned about DELWP's ability to suitably enforce Section 69 agreements to ensure that the bushfire risk level within the reserve remains within the designated bushfire category into the future. Additional measures need to be deployed to ensure that the conservation area will be

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managed, and this management is able to be enforced to keep the bushfire risk level at the rate shown in the Terramatrix report.

Council's experience implementing the Merrifield West PSP has shown that when a landowner is not managing the vegetation and fuel loads within a BCS area the bushfire risk level of the reserve can significantly increase. Similar to the Craigieburn West BCS, the Mount Ridley Grasslands Nature Conservation Area in the BCS was designated as a grassland requiring a setback distance of 19 metres under the Merrifield West PSP. However, this Area has remained in private ownership yet has not been managed to comply with a Section 69 Agreement. Despite Council making DELWP aware of these breaches no enforcement action has been undertaken. The result has been a proliferation of understory fuel load to such a degree that it would likely be classified as Woodland rather than grassland and would therefore require a setback greater than the 19 metres designated in the Merrifield West PSP.

Council notes that requirement R20 of the Craigieburn West PSP does seek to maintain reserves in accordance with their vegetation classification as identified in Table 4 of the PSP, however it is unclear how this requirement could be fulfilled. Council has noted queries around implementation of bushfire requirements and guidelines in Attachment 4- HCC Track changes version to November 2020 exhibited PSP and UGZ Schedule. Ultimately, there appear to be gaps in the effective mitigation of bushfire risk management in the design the PSP and associated controls.

In Council's view, the option to retain BCS Area 29 in private ownership should be removed from the PSP, and instead the compensation measures outlined within the Melbourne Strategic Assessment Levy for this land be deployed to ensure that the developers vest this land and are appropriately compensated prior to or at the same time adjoining development occurs.

Changes requested to the PSP and planning provisions

The ongoing management of the BCS must be secured through the PSP or UGZ schedule to ensure the current vegetation classification, and subsequent setback buffers requirement, are not compromised.

The final PSP amendment must address the matters raised in Attachment 4 - HCC Track changes version to November 2020 exhibited PSP and UGZ Schedule.

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Open space network and tree retention

Open space network

Council has collaborated extensively with the VPA in the development of the open space network for Craigieburn West and welcomes the integrated and diverse network proposed in the draft PSP.

Council strongly supports the green link running along the spine of the PSP that links key destinations, such as the proposed schools, town centre and sports reserves to the wider walking and cycling networks outside the PSP. Council also welcomes the opportunities presented by LP-09 to provide the future Craigieburn West community with a unique local wooded park within an urban context unlike any found in suburbs across Hume.

Council requests the following changes to the Plan 8: Open Space Plan (page 28) in response to the draft November 2020 urban structure:

<u>SR-02</u>	SR-02 should be shifted to the northern boundary of Properties 9 and 10. The current location of SR-02 creates an awkward sliver of development at the north of these properties, shifting SR-02 will allow the 7-12 Government School and the north-south connector road to be adjusted to allow for a more practical sized shape and extent of developable land to be created. ¹ Importantly, shifting SR-02 north will allow for an open space connection from the existing Aitken Creek reserve in Craigieburn R2 through the creek corridors and drainage reserves in Craigieburn West to facilitate the movement of kangaroos into and out of the BCS and throughout habitat in the precinct and wider area.
<u>LP-14</u>	LP-04 should be shifted to adjoin the green link. Currently LP-04 unnecessarily straddles two properties in separate ownership. It could be more centrally located within the PSP along the green link to maximise its catchment.
<u>LP-15</u>	The agency consultation PSP showed the green link running along the eastern boundary of the local access street level 2 running north-south through Properties 33, 34, 35 and 36, with LP-15 integrated into GL-08. As the green link in this section of the PSP has been shifted to the west of the north-south road, LP-15 should be shifted to the west of the road also, to once more be integrated with the green link.
<u>LP-16</u>	LP-16 was previously co-located with proposed drainage in the agency consultation version of the PSP. With the consolidation of the drainage assets into YCWL01, LP-16 should be shifted south of the east-west connector street to be once more integrated with this drainage asset.

1 - Council understands that due to the land equalisation methodology in the ICP that land that is encumbered with open space without any NDA is costed significantly less than land that is similarly encumbered however still feature a limited amount of NDA. Council questions the equity of this and highlights that the ICP system should be designed to ensure planning and development outcomes for new communities are prioritised instead of arbitrary quirks of the ICP land equalisation methodology necessitating poor development outcomes.

Changes requested to the PSP

Council request the open space network is updated and the land equalisation methodology is remedied in keeping with the comments above.

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Tree retention

Council welcomes the extent of trees that are shown to be retained in Plan 10 across the PSP.

Council officers have conducted a review of all the trees within the PSP and are seeking the inclusion of 14 additional trees that are currently shown as *vegetation that can be removed* on Plan 10, to be reclassified as *native vegetation that must be retained* (see further detail in Table 2 below).

Council's review utilised the tree assessments in the Tretec's *Craigieburn West Arboriculture Assessment Report* February 2020, Council's in-house ecological expertise and experience of how trees can be practically retained within a residential subdivision. Where possible the trees were physically inspected in a site visit. When this was not possible, officers reviewed the photos taken by Tretec.

The table below outlines Council's justification for retaining each of these 14 additional trees, alongside Tretec's assessment.

Table 2: Additional Trees to be Protected under Craigieburn West PSP

Tree No.	HCC Rationale	Tretec Comments	Arborist	Report
614	These Trees are in the ground of the established Orthodox Church. They are mature specimens that have been incorporated into the gardens and outbuildings of the facility. There is no reason that the PSP should authorise their removal. The removal should be decided via planning permit if one eventually lodged.	These trees are shown as a high retention value trees with a Useful life expectancy of over 50 years		
616	These Trees are in the ground of the established Orthodox Church. They are mature specimens that have been incorporated into the gardens and outbuildings of the facility. There is no reason that the PSP should authorise their removal. The removal should be decided via planning permit if one eventually lodged.	This tree is shown as a high retention value tree with a Useful life expectancy of over 50 years		
617	These Trees are in the ground of the established Orthodox Church. They are mature specimens that have been incorporated into the gardens and outbuildings of the facility. There is no reason that the PSP should authorise their removal. The removal should be decided via planning permit if one eventually lodged.	This tree is shown as a high retention value tree with a Useful life expectancy of over 50 years.		
763	This tree is located directly adjacent to the large public opens space (LP09) and it would be able to be retained via the buffer/road reserve along the edge.	The tree is a very large specimen with an interesting and stately form that could provide neighbourhood character.		

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989	This tree is located directly adjacent to Debonair parade local park, and currently existing within the parkland inside a mulched bed. The tree would be able to be retained via the buffer/road reserve along the edge. This tree	The tree is currently actively managed by Council as it appears to be in a road reserve. This is a high value tree and should be shown as retained.
1205	This is a rarer tree species within this PSP and therefore its value is inherently higher. The tree is also located directly adjacent to the Mickleham Road reserve and therefore it can be incorporated into the design of the subdivision with very little effort or impact on Net Developable Area	The arborist has provided a high retention value for this tree and a relatively long Useful Life Expectancy. We believe that with suitable arboricultural interventions the tree will have a long and useful life in the future development.
1370	This is a very large mature specimen which would be hundreds of years old. Its location adjacent to Mickleham Road and Dunhelen land means that it can be easily incorporated into the subdivision design providing high value amenity without impact in Net Development Area	The arborist has classified this tree as have a high value with a 50+ Useful Life Expectancy - we see no reason why this tree should be shown as removed.
1472	This is a beautifully formed healthy tree which provide high amenity and character in this area and will do so for many years to come.	The arborist has classified this tree as high with a 50+ Useful Life expectancy
1473	Mature specimen of this species rarely found of this size in this area. This tree is located near other trees of high value and together these create a high amenity location near the heritage site and the green links.	The arborist has classified this tree has high with a long ULE.
1474	This is a very large tree which is located near other trees of high value and together these create a high amenity location near the heritage site and the green links.	The arborist has classified this tree as having high value with a long useful life expectancy
1476	This is a slightly round tree within this larger patch of trees. The retention of this tree will help ensure the patch can survive into the future	The arborist has classified this tree as having high value with a long useful life expectancy
1479	This tree is an extremely large mature tree. This tree contributes highly to the local character and amenity of the areas and contribute toward the landscape containing heritage site its interface to Mickleham Road.	The arborist has classified this tree as having high value with a long useful life expectancy

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1480	This is a large tree with excellent form. It is adjacent to Mickleham Road and the green link and visual appeal to Mickleham Road	The arborist has classified this tree as having high value with a long useful life expectancy
1481	This tree is an extremely large mature tree. This tree contributes highly to the local character and amenity of the areas and contribute toward the landscape containing heritage site and its interface to Mickleham Road.	The arborist has classified this tree as having high value with a long useful life expectancy

Changes requested to the PSP

Council requests that Plan 10 be updated to show the trees identified above as *vegetation that must be retained*.

Community Facilities

Community facilities - role and functions

Council has worked with the VPA to ensure an appropriate provision of community infrastructure is provided across the PSP. Council's community needs assessments supports the provisions of two, level two community facilities in the PSP to meet the needs of the future Craigieburn West (and Lindum Vale) community.

Council's community needs assessments are conducted by understanding the unique role and function that each individual community facility plays within a network of facilities across the northern growth corridor. Key to this is forecasting what the needs of the future community will be beyond the immediate boundaries of a PSP that need to be provided for, as well as reassessing what needs should be catered for in a specific facility at the time of its delivery.

The principles guiding Council's delivery of community facilities are to ensure each facility is a human focused, highly flexible, accessible and resilient facility that fosters community pride by making them a destination and focal point of a precinct.

In order to assist Council achieving these principles, any descriptions in the PSP and ICP of community facilities should read "*a functional community centre that responds to community need and expectations*".

The PSP and ICP should not specify a role or function of any of the community facilities. As outlined above, Council will assess the specific community needs and service that a community facility will be designed to meet at the time of its delivery considering short and longer term needs of the resident community. Specifying services in the PSP and ICP at this time is not needed and can undermine Council's full and proper assessment of the service needs.

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Changes requested to the PSP and ICP

Council requests that the ICP describe CI-01 or CI-02 as *“a functional community centre that responds to community need and expectations”* and not include descriptions of the services that they might provide.

Community facilities - Locations

Council supports the general location of community facility CI-02 and the potential synergies offered through its colocation with the P-6 primary school and non-government school, subject to the changes proposed above in Figure 1 and Table 1.

However, Council has strong concerns regarding the location of CI-01 on Property 4 as proposed in the Place Based Plan.

Since the Co-Design Workshop in August 2019, Council has consistently maintained that locating a community facility along either Mickleham Road or Mt Ridley Road would not be supported. A new community facility should not be located on a future arterial roads as planned for Mickleham and Mt Ridley Roads as this would poses an avoidable safety risk.

The proposed location of CI-01 also reduces the accessibility and walkability of the facility as it would not have direct access to the connector road network. This is of particularly concern for residents in Lindum Vale accessing the facility (given the Lindum Vale ICP contributes to half of the cost of land and construction of the facility).

In order to maximise the accessibility and walkability catchment of CI-01, it should be located more centrally within Property 4 and have a frontage with the north-south connector road. This would ensure CI-01 is safely accessible to all residents, including those in Lindum Vale.

Additionally, Council considers that greater synergies can be achieved by co-locating CI-01 with the active sports reserve SR-01. Co-location with SR-01 would allow for more integrated design opportunities with the sports fields, pavilion, car parking and potentially allow for protection of significant trees.

Council acknowledges the intention to co-locate CI-01 with Mt Ridley Primary School in order to allow for the potential sharing of facilities and services, particularly the provision of kindergarten services. Council does not consider that the co-location of CI-01 with Mickleham Primary School solely for the provision of kindergarten services necessitates the school and the community facility be directly adjoining. Should Council provide kindergarten services in this facility and it is located more centrally within Property 4, these services will still be provided within proximity of the school.

Council does not consider the benefits of co-locating CI-01 with Mickleham Primary School to be sufficient when compared with the greater synergies that can be achieved by co-locating it with SR-01 in a safer and more accessible location within the PSP.

Changes requested to the PSP

Council requests CI-01 be moved based on the following criteria:

- It must be co-located with sports reserve SR-01.
- It must have access to a connector road.
- It must not be located on an arterial road.

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Drainage

Council notes that Melbourne Water has undertaken a review of the three Drainage Services Schemes (DSS) affecting the Craigieburn West PSP, and the drainage infrastructure identified in Plan 6 and Table 3 of the PSP is based on this review.

Council will engage with Melbourne Water during their consultation process for the proposed changes to the DSS to determine ownership and management responsibilities of the proposed assets.

Council also acknowledges the area of land that is not within the catchment of a Melbourne Water DSS. The drainage requirements of this land are still to be resolved to the satisfaction of Melbourne Water and Council.

Any proposed drainage review for this land should be done in consultation with all affected landowners and agencies, particularly where seeking alterations to DSS boundaries.

Infrastructure Contributions Plan

The VPA has provided Council indicative costings for the ICP items identified in the draft Craigieburn West PSP. These costings indicate that the Craigieburn West ICP will be a standard levy and not require the need for a supplementary levy.

It is noted that the items proposed to be funded under the Craigieburn West ICP are approaching the upper limit for a standard levy. Council is supportive of using a standard ICP and benchmark costings based on these ICP items however may vary this position if further items are proposed to be included in the ICP. In this case Council would seek full costings to determine the adequacy of a standard levy or whether a supplementary ICP is necessary.

Hume City Council Submission toDraft for Public Consultation on **Craigieburn West Precinct Structure Plan**, December 2020

Attachments:

1. Hume City Council comments on Kangaroo Management Plan, December 2020
2. Technical Note – Traffic and Transport Peer Review, GTA, 18 December 2020
3. Proposed Transport Network Plan, mesh planning, 17 December 2020
4. Whites Lane proposed Cross Section for Craigieburn West and Craigieburn R2 PSP
5. HCC Track changes version to November 2020 exhibited PSP and UGZ Schedule
6. Craigieburn West Green link cross sections

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Our File: HCC18/463
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18 December 2020

Tony Marks
Acting Director – Outer Melbourne
Victoria Planning Authority
c/- Craigieburn West PSP
Level 25, 35 Collins Street
Melbourne Vic 3000

Email to amendments@vpa.vic.gov.au

Dear Tony,

RE: SUBMISSION TO CRAIGIEBURN WEST PSP AND ASSOCIATED DOCUMENTS, NOVEMBER 2020

Please find enclosed Council's submission to the Craigieburn West PSP and associated documents released for public comment in November 2020. The submission has been prepared by officers and will be presented to Council for endorsement in February 2021.

Much of the content in the PSP and associated documentation is welcomed and strongly supported by Council officers. It reflects the work that has been undertaken in the planning of the site and furthers the objectives and outcomes sought within the Council adopted *Hume Corridor HIGAP*.

Our submission is focussed on new information not provided as part of agency consultation as well as outstanding matters from our agency submission.

Most of these concerns have been discussed with your officers and I understand that a some are close to being resolved.

I am hopeful that the highly collaborative relationship between the VPA and Council in the preparation of the PSP to date can continue and enable many of the concerns outlined in the submission to be resolved prior to the Advisory Committee process.

The submission also outlines that Council does not support the approval of the PSP until there is a funding commitment for the full duplication of Mickleham Road from Somerton Road to Donnybrook Road.

We look forward to continuing discussions with the VPA and others on how this funding commitment could be achieved prior to the approval of the PSP.

Yours faithfully,

A handwritten signature in dark ink, appearing to read 'Andrew Johnson', with a horizontal line extending from the end of the signature.

**ANDREW JOHNSON
MANAGER STRATEGIC PLANNING**

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TECHNICAL NOTE

Traffic and Transport Peer Review

DRAFT

Project Code: V204460 Project Name: Craigieburn West PSP

Dept: Transport Engineering

Date: 18 December 2020 Version No. 1

Author: Reece Humphreys & Hui-Lin Tan

Reviewer: Reece Humphreys & Hui-Lin Tan

SUBJECT: Peer Review of Transport Impact Assessment

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Introduction

GTA have been engaged by Hume City Council to peer review the submitted technical material as part of the proposed Craigieburn West PSP to ascertain whether the work completed is suitably 'fit-for-purpose'.

At a high level, a 'fit-for-purpose' review seeks to determine whether or not the analysis undertaken as part of a submission can, at a minimum, provide outputs to enable an assessment of the proposal. In undertaking this review, documents were provided to GTA as summarised in Table 1, which includes the Craigieburn West Precinct Structure Plan Transport Impact Assessment prepared by onemilegrid.

Table 1: Reviewed Material

Material	File Name	File Description	Received by GTA
Transport Impact Assessment Report	Craigieburn-West-PSP-Transport-Impact-Assessment-One-Mile-Grid-November-2020.pdf	Transport Impact Assessment (09/11/2020)	3/12/2020

It is noted that the scope of our review would be limited to the reporting provided, rather than an audit of any electronic audit files. This memorandum summarises the findings of our review, with the major items discussed in the body whilst the moderate and minor items provided in a Table as an attachment.

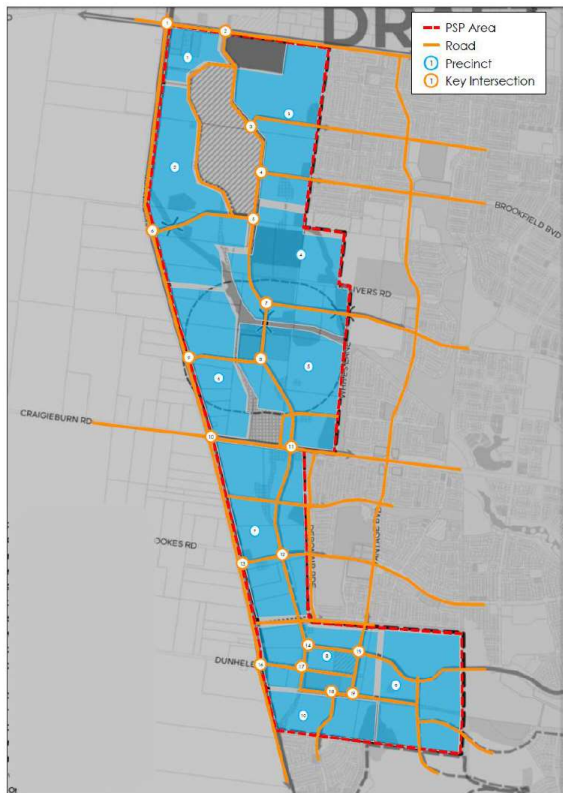
In reviewing the documentation, it is clear that further information from the proponent is required to inform a position. Specific requests have been provided within the discussion within this document.

Traffic Model

The traffic analysis within the report relies upon a bespoke traffic model. There are no details on the specific software used, however it is thought to be a spreadsheet-based model likely developed in excel. The extents of the model are presented in Figure 1.

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Figure 1: Model Extents (reproduced from report)



A limitation of a spreadsheet model is that it does not have the ability to redistribute traffic through the network as a result of capacity limitations or availability. This may result in an underestimation (or even an overestimation) of some volumes on the network within Craigieburn West such as the congested network south of Craigieburn Road.

Recommendation: Information and clarification on the type of modelling platform/software used for the works

Modelled Scenarios

The traffic model covers an interim scenario (2031) and ultimate scenario (2046) representing the 10-year and 25-year horizons respectively. However, the rationale and outcome of the interim scenario modelling is not clear.

Typically, 2046 or 2051 are used as ultimate years for greenfield traffic modelling and PSPs in the north growth corridor. There may be differences with traffic volumes where growth factors are applied to 2051 rather than 2046 however this difference is negligible. Notwithstanding, these represent the 'full build out' rather than a specific year and should be considered in that way.

Recommendation: VPA to provide rationale and purpose of the interim modelling

Background Growth

The report states the following for background growth:

For the purposes of a conservative analysis, a growth rate of 1% per year (compound) has been applied to several non-arterial roads within the vicinity of the site over a 10-year period (interim) and 25-year period (ultimate), equivalent to a 10.5% and 28.2% increase in traffic volumes respectively along the non-arterial roads. The traffic growth has been applied to the east-west aligned connector roads in the northern portion of the site. Whilst no growth was applied to Vantage Boulevard and Elevation Boulevard in the southern portion of the site where the Cardno (Aston Estate) traffic volumes were used.

In addition, a growth rate of 3% per year (compound) has been applied to Craigieburn Road traffic volumes over a 10-year period (interim) and 25-year period (ultimate), equivalent to a 34.4% and 109.4% increase in traffic volumes respectively. Whilst the Mickleham Road and Mt Ridley Road projected traffic volumes were based on the Lindum Vale PSP.

It is acknowledged that a growth rate of 4% was adopted for earlier modelling forecasts, however since those previous assessments updated traffic surveys were undertaken to capture traffic volume growth generated by development along the Craigieburn Road corridor.

The source of the growth rates was not provided. Applying a compound growth to roads in growth areas can also provide inconsistent outcomes, particularly if applying this type of growth from a low base. This may have impacted the previous modelling forecasts.

Recommendation: Provide further justification and sources for the adopted background growth rates

Traffic generation of surrounding PSPs

The model considers and uses some forecast traffic volumes from previous transport reports for the surrounding PSPs. The report includes a description of each report, ultimate traffic volumes at key intersections and any justifications where deviations from the reports were used.

Assumptions from the PSPs adopted for the ultimate scenario include:

- A 20% reduction in the proportion of traffic accessing the Aston Estate via the intersection of Elevation Boulevard / Mickleham Road and Dunhelen Lane / Mickleham Road has been applied compared to the interim scenario. The reduction in traffic volumes is to account for the additional employment opportunities provided to the north and east of the Craigieburn R2 PSP which are anticipated to reduce the number of vehicles from the Aston Estate travelling south along Mickleham Road. Furthermore, it is expected that the construction of the Outer Metropolitan Ring Road would reduce the distribution of traffic from the Aston Estate to Mickleham Road, with vehicles instead using Craigieburn Road to access the Outer Metropolitan Ring Road when required;
- A reduction of 4,400 vehicles per day was applied to the daily volumes (and proportionately peak hour volumes) between Craigieburn Road (west of Vantage Boulevard) and Mickleham Road (north of Dunhelen Lane) to account for traffic utilise Vantage Boulevard for travel between Craigieburn Road and Mickleham Road;
- The traffic movements modelled under ultimate conditions at the intersections of Mickleham Road / Mt Ridley Road and Mt Ridley Road / Marathon Boulevard as part of the Lindum Vale and Merrifield West PSP have been adopted with no modifications to distributions;
- The through volumes along the northern portion of Mickleham Road (north of Craigieburn Road) have been calculated using the turning movements under ultimate conditions at Mickleham Road / Mt Ridley Road which were provided as part of the Lindum Vale PSP; and

- The Mickleham Road through movements at the intersection of Mickleham Road / Craigieburn Road have been calculated by removing the turning movements (the surveyed volumes plus 109.4% growth) from the through movements. [due to OMR]
- Traffic distribution of 90% traffic travelling south along Mickleham Road and 10% travelling north at intersection with Elevation Boulevard (onemilegrid assumption based on the road network and surrounding land uses).

Of these assumptions, is not entirely clear how the 90:10 traffic distribution split and the 4,400 vehicles per day reduction were derived.

Recommendation: Document how the distribution split and reduction in 4,400 vehicles per day on Craigieburn Road (?) was derived.

Land Use

The land use adopted for the model and corresponding to the 10 precincts are presented in Table 2. The land uses have been determined from the Place Based Plan prepared by the VPA.

Table 2: Proposed land use inputs adopted for the model (reproduced from report)

Precinct	Use	Area
1	Residential	9.2 ha (153 dwellings)
	Government School Expansion	1.3 ha
2	Residential	53.3 ha (889 dwellings)
3	Residential	48.5 ha (808 dwellings)
4	Residential	34.1 ha (568 dwellings)
	Government School	8.3 ha
5	Residential	36.3 ha (604 dwellings)
	Local Town Centre	2.2 ha
	Mixed Use	2.2 ha
	Government School	3.3 ha
6	Residential	40.6 ha (677 dwellings)
7	Residential	51.5 ha (858 dwellings)
8	Residential	32 ha (534 dwellings)
	Community Facilities	1.2 ha
	Non-Government School	2.2 ha
	Government School	3.6 ha
9	Residential	46.5 ha (776 dwellings)
10	Residential	17.1 ha (286 dwellings)
Total Dwellings		~6,153 lots

It is based on a yield per developable hectare where residential uses were assumed to have an average lot size of 600sqm. The source of the average lot size has not been provided. The PSP documentation indicates that the net developable residential area is 414.3 ha and that the minimum average density should be 18.5 or 26.5 lots/ha dependent on its location within walkable catchments. Plan Melbourne suggests a target rate of 20 dwellings per hectare which would equate to 7382 lots when applied the reported residential area (369.1 ha) or 8286 using the area reported in the PSP – significantly more than the adopted 6153 lots.

Five schools are proposed within the PSP area, comprising four government schools (expected to be three primary schools and one secondary school) and a non-government primary school (expected to both be primary schools) in the southern portion of the site.

An existing government primary school is located at the south-eastern corner of the intersection between Mt Ridley Road / Mickleham Road and it is planned for its expansion. This (and the community facilities) was not included in the development yield as “they will be shifted with a school expansion.”

It should also be noted that the local town centre was not specifically modelled.

Recommendation: Update traffic assessment with new development yield assumptions or following updated land use information from VPA

Traffic Generation Rates

The report utilises 9 vehicle trips per day per dwelling, with a peak to daily ration of 10% meaning that the trip generation for the AM and PM Peak would be 0.9 trips per dwelling.

It states that:

It is generally accepted that single dwellings on a lot in outer suburban areas may generate traffic at up to 10 vehicle trips per day, whilst in areas with good public transport, and for higher density dwellings, lower traffic generation rates are often recorded.

However, there is no source for the claim that 10 trips per day is “generally accepted”.

Furthermore, the adoption of 9 vehicles per day is on the basis that the site has a lot size of 600sqm (which is also not sourced, as mentioned earlier) and proximity of the site to the existing bus network which is expected to be extended into the site, implying the PSP has “higher density dwellings” and “good public transport” despite Craigieburn’s high level of car dependency and distance from the train station.

Recommendation: Provide further justification and sources for the adopted trip rate (density and PT access)

Despite the lack of justification, the adopted trip rate of 9 vehicle trips per day per dwelling is considered appropriate, when considering trip rates used for other PSPs in growth areas.

The report references trip rates adopted for surrounding PSPs:

The SMEC traffic report for the Merrifield West PSP and Cardno traffic report for the Lindum Vale PSP adopted 8.5 vehicles per lot. Whilst Ashton Traffic Services adopted a rate of 9 vehicles per lot when modelling the Craigieburn R2 PSP

Further benchmarking with PSPs in the north growth corridor which used VITM modelling includes a rate of 8.3 daily trips per dwelling for Beveridge North West PSP which has similar proportions of land use (residential / employment / enrolments) to Craigieburn West. Modelling for Shenstone Park PSP resulted in 14.7 trips per dwelling, however that the PSP included a much higher proportion of employment uses.

Directional splits

The report states the following directional distributions were adopted for the residential traffic generation.

- AM peak hour: 70% outbound, 30% inbound; and
- PM peak hour: 40% outbound, 60% inbound.

No source has been provided for these assumptions. The reported survey data (Figure 8 and 9 from report) for traffic from Highland Drive / Vantage Boulevard and Waterview Boulevard onto Craigieburn Road supports the adopted split for the AM peak hour, however it shows closer to a 50:50 split of traffic in the PM peak hour, which was 3:30-4:30pm and may not capture the full tidal nature of work trips.

A review of SCATS data at the Highland Drive / Vantage Boulevard and Craigieburn Road intersection on weekdays in October 2019 was undertaken. The reported peak hours are consistent with the model (8-9am and 3:30-4:30pm) and the directional distribution for the AM peak hour. However, for the PM peak, the directional distribution for outbound to inbound was around 45:55 to 50:50.

It is also not stated what the AM and PM peak hour turning movement diagrams were used for in the modelling, particularly as the modelling was for the daily period. AM and PM network plots have not been provided in the report.

Recommendation: Provide further justification and sources for the adopted directional splits

Other uses

No traffic generation was considered for the other uses. As stated in the report, there are “a variety of other uses including five schools, a Local Town Centre, a mixed-use zone, community facilities, local parks and local sports reserves.”

The town centre and schools would generate a level of traffic that have the potential to influence the daily demands on the network as well as the distribution and catchment.

The report states:

onemilegrid and other traffic consultants generally consider the use of a higher traffic generation rate for the residential uses to be the more conservative and the appropriate approach in regard to traffic generation for new PSPs especially where no large employment precincts are provided (as is the case with the Craigieburn West PSP). The adoption of a higher rate for the residential component acknowledges that a high proportion of movements associated with uses such as the town centre and community facilities are internal trips, and in terms of modelling, the movement is allocated to the residential use rather than the destination to save double counting.

The report does note the limitation of this approach when considering school uses “which are likely to draw a larger proportion of external trips than the other non-residential uses”. Enrolment numbers were also not available when the report was prepared, however schools in growth areas can be subject to rules of thumb¹ to determine enrolment numbers.

Recommendation: Provide further justification and sources for the adopted trip rate (exclusion of other uses) or update traffic assessment with updated land use from VPA

Traffic Distribution

The report states that VISTA was used in the model to determine trip purpose distributions for typical households. However, it is not stated what is considered a “typical household” (e.g. inner, middle, outer Melbourne) and whether it is appropriate for a primarily residential PSP such as Craigieburn West.

The directional distributions were determined based on the notable uses to the north, south, east and west of the site. It also considered that 9% of trips were internal. Some description was provided on how the external trips would use the internal network.

[1] Source: Department of Education and Training, as advice provided to VPA.

- 1 Government Primary School, per 3,000 dwellings in growth areas
- 1 Government Secondary School, per 10,000 dwellings in growth areas
- Government Primary School: 450-475 Enrolments (students)
- Government Secondary School: 1,100 Enrolments (students)
- This would be referenced to the assumption of around 2.8 to 3.1 people per household in the growth areas. Any private or catholic school would be on top of these (where a site is identified). The assumption would be for similar student numbers per site, unless otherwise known.

Table 3: Proposed traffic distribution (reproduced from report)

Origin/Destination	Percentage ¹	Notable Uses
North	14.5%	Merrifield Employment Park / Folkestone Employment Park / Mickleham Town Centre / Regional Victoria
South	32.7%	Tullamarine Employment Area / Inner Melbourne / Western Ring Road Access
East	39.4%	Craigieburn Town Centre / Craigieburn Employment PSP / Craigieburn Train Station / Craigieburn R2 PSP / Campbellfield Employment Area / Roxburgh Train Station / Alternative Western Ring Road Access
West	4.4%	Sunbury
Internal	9%	

¹Daily Percentage. AM peak and PM peak vary slightly

The source of these distributions are not entirely known. These require further interrogation to form an opinion.

Recommendation: Provide further justification and sources for the adopted directional distribution and use of VISTA

Transport Network

Based on our review of the PSP network and resultant modelled road volumes, the following comments are provided:

1. The majority of internal intersection have been nominated as roundabouts. Consideration should be given to nominating some of the intersections in the vicinity of the school and local town centre as signalised intersections. These would better facilitate the modelled volumes and better accommodate for pedestrians and cyclists.
2. Within the vicinity of Elevation Boulevard, the East-West Connector Road 1 and 3 are over capacity, to provide better certainty, consideration to a new access link connecting to Craigieburn Road could be provided in the form of left-in left out intersections, especially in the northern portion of the site. It is noted that these could be provided as part of the subdivision, however the fragmented land ownership may be challenging for certainty for this to be delivered.
3. To accommodate high volumes on Vantage Boulevard, the southern portion of the north-south PSP link could be considered to be upgraded from "Access Street" to "Boulevard Connector Road" to assist in any potential redistribution of traffic volumes from the congested Vantage Boulevard.
4. The southern portion of the PSP also contains several roundabouts surrounding the proposed schools, typically roundabouts do not provide good levels of service for pedestrians and cyclists as well as resulting inefficient journey for through traffic when a more direct link could be provided. It may be more appropriate to remove the intersection control from the report.
5. There is also a limited level of active travel discussion within the report, with no mention of implications on pedestrians or cyclists (such as impact of high traffic volumes or roundabouts). It is noted that the pedestrian and bicycle network within the PSP street network plans do indicate the links for actives uses as part of their cross sections.

Figure 2: PSP Road Network and Intersections (reproduced from report)



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ATTACHMENT 1

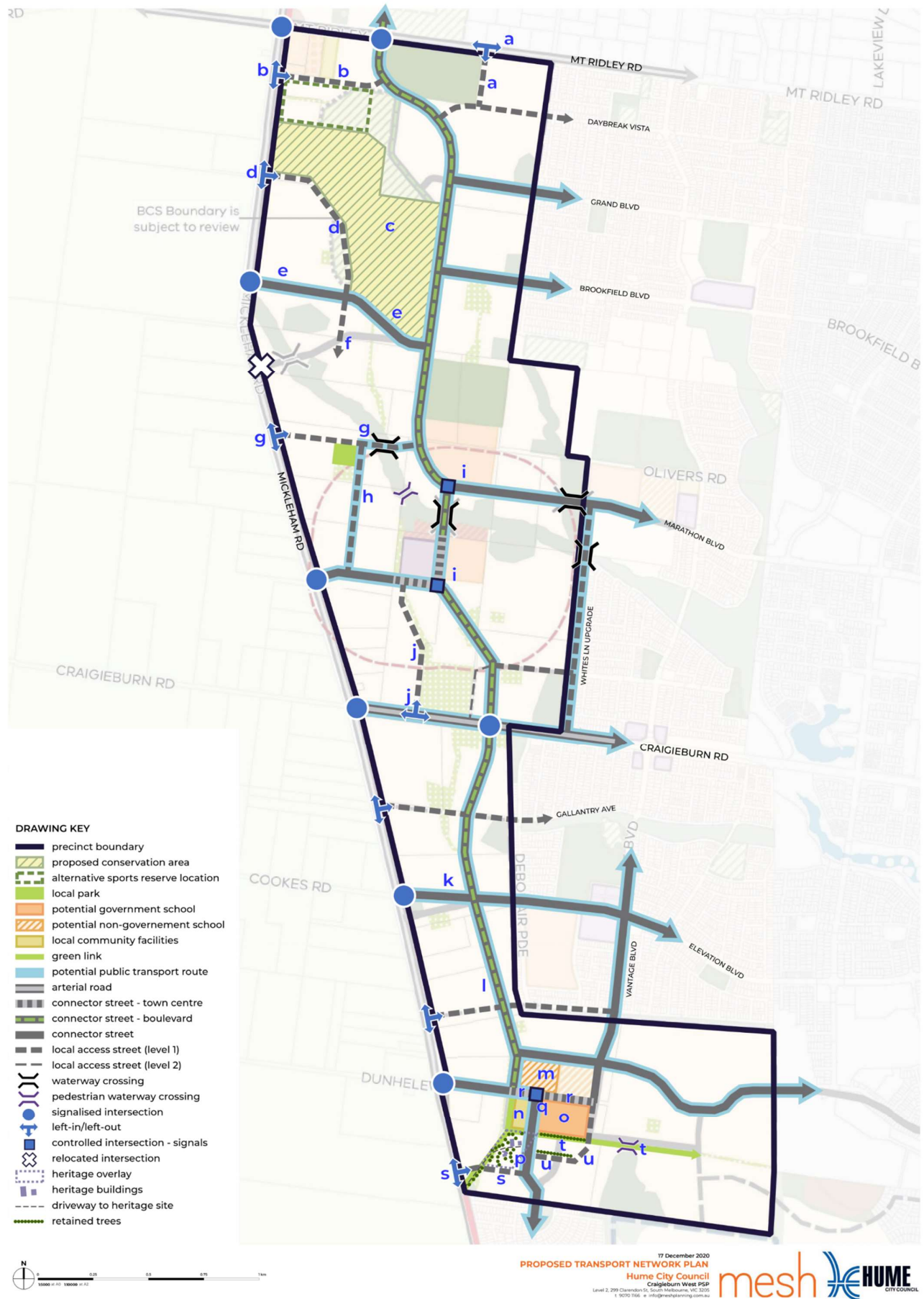
Table 4 provides a summary of discussion points including those above and additional points of comment.

Table 4: Summary of discussion points

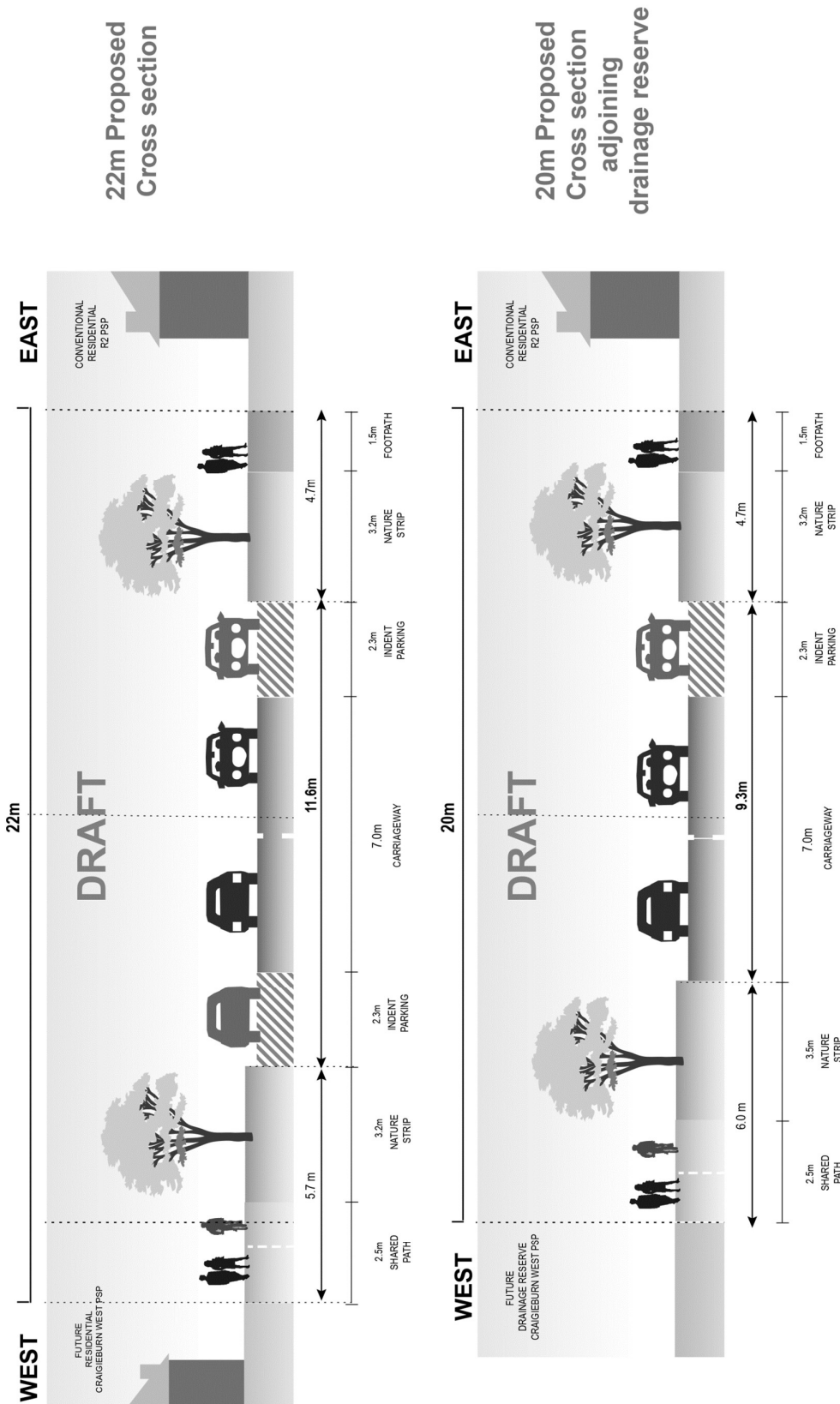
Section/Topic	Comment	Priority
4. Traffic model / 4.1 Overview	It is not clear on the modelling platform used for the assessment; however it is assumed that it is a spreadsheet model. It is not clear what the rationale is for the interim year modelling. Using an ultimate year of 2046 will provide slightly lower volumes on the network compared to 2051.	Minor Minor Minor
4.3.1 Traffic generation of surrounding PSPs / 4.4 Future Base Case Traffic Volumes	It is not clear where the 4,400 vehicles per day using Vantage Boulevard to access Mickleham Road has been derived from. It is not clear from the corresponding figure of peak periods. This number is then used as an assumption to determine the forecast traffic volume on Craigieburn Road. It is also not clear how the distribution split of 90% of traffic travelling south along Mickleham Road and 10% travelling north at the intersection with Elevation Boulevard was derived.	Moderate Minor
4.3.2 Through Traffic Growth	It is not clear where the adopted growth rates (1% for several non-arterial roads, 3% for Craigieburn Road) have been derived. Clarification on the source for these rates is recommended.	Minor
4.5 Proposed Land Uses	It is not clear where the 600sqm per lot assumption has come from. Other yields from PSPs in growth areas are significantly higher when applied to the area. This may need to be updated with new assumptions and following updates to development yields when the PSP is finalised.	Moderate
4.6 Traffic Generation	It is not clear where the statement that 10 vehicle trips per day per dwelling is "generally accepted" and how it was determined that the PSP has density and good public transport access to justify a rate of 9 vehicle trips per day on this basis. The decision to only use a higher residential generation rate and not schools or employment references "other traffic consultants" and that it is a "conservative and appropriate of approach", without reference to previous examples and data points. Notwithstanding, it is agreed that the resultant generation rate is acceptable when compared to other growth area PSPs in the north growth corridor.	Moderate
4.6 Traffic Generation	It is not clear how the directional distributions of the AM and PM peak have been derived. (AM peak hour: 70% outbound, 30% inbound; and PM peak hour: 40% outbound, 60% inbound). A review of the reported survey data and SCATS data show that the PM peak hour distribution is closer to a 50%/50% split.	Moderate
4.7 Traffic Distribution	VISTA has been used to determine trip purposes for typical households, but it is not stated what areas were considered for the adopted purposes (e.g. inner, middle, outer Melbourne). The proposed traffic distribution for uses external to the PSP were described, but the 9% for internal trips was not justified.	Minor
4.10 Traffic Impact	There are no performance plots of the network during the peak period so it is unclear as to how the network performs. These are typically shown in volume to capacity (V/C) plots.	Minor
3.3.3 Overall Road Network (Figure 11: PSP Road Network and Intersections)	It is not clear how the intersections have been determined. Typically, these are provided / determined by the VPA and are generally not considered as part of the PSP planning and analysis. Notwithstanding, their location and treatment are generally suitable for the proposed road network, with the exception of the vicinity of the schools and activity centres where better control for pedestrians and cyclists could be nominated.	Comment

Section/Topic	Comment	Priority
5. Conclusions and Recommendations	The report assesses the future traffic volumes within and connecting the PSP, however it does not include recommendations arising from the modelling to inform the development of the final PSP.	Comment
4.10 Traffic Impact and 5. Conclusions and Recommendations	The report highlights that Vantage Boulevard is over capacity with 11,100 vehicles a day, above the target of 3,000-7,000 vehicles per day. The report also references other consultants work which showed volumes above capacity, which is assumed to be the Cardno Aston Estate traffic report with a projected 11,000 – 12,500 vehicles per day presented in Figure 18 of the report. Additional north-south connectors running parallel may help to accommodate these volumes.	Comment
Staging	The staging of development is a consideration for the subdivision stage, however there may be benefit indicating this on the plan to provide more certainty.	Comment

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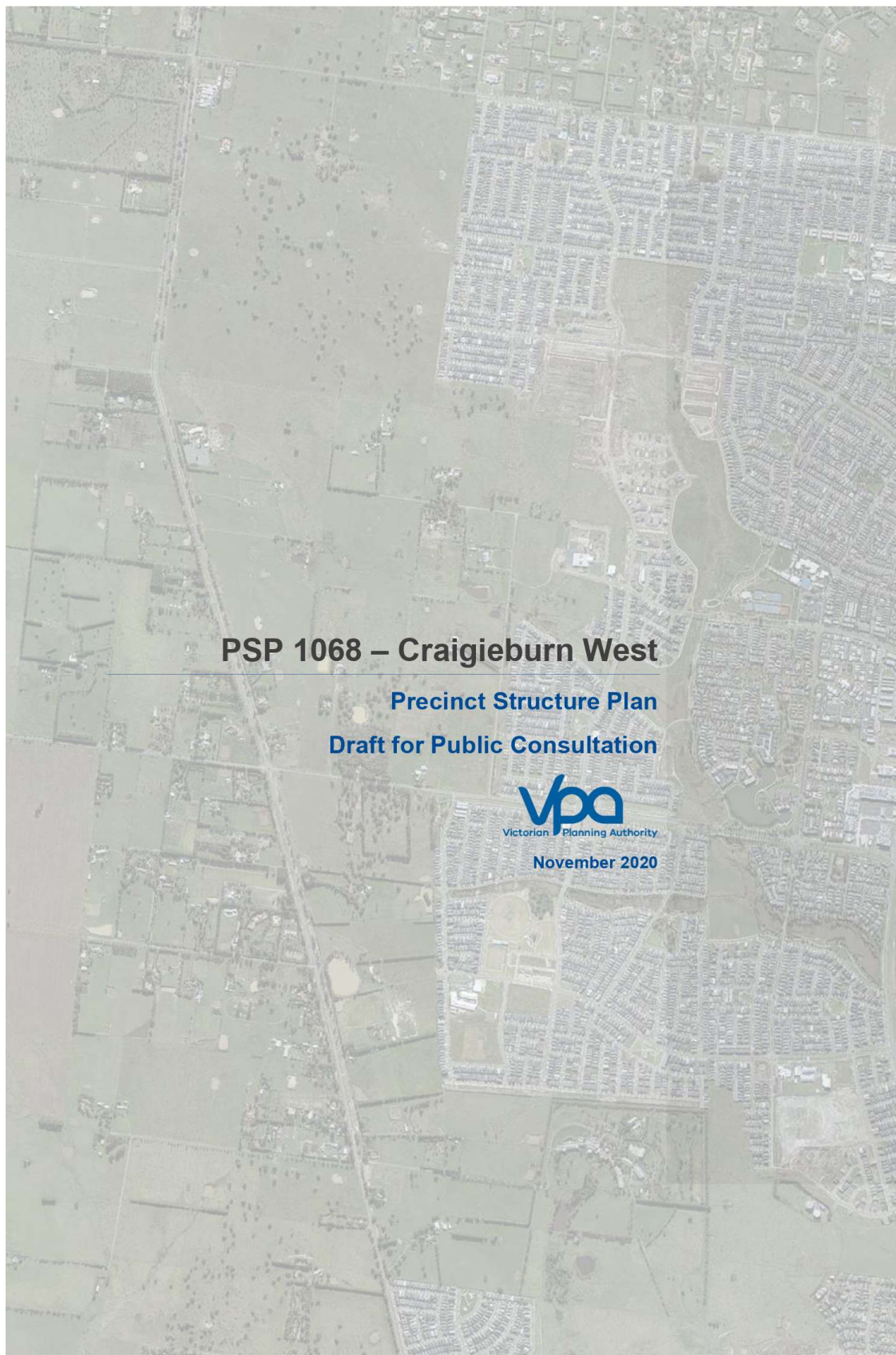
Whites Lane - Craigieburn Road to Olivers Lane - DRAFT
Local Access Street - Level 2 with Shared Path (20 - 22m)

Hume City Council
December 2020

Scale 1:1
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PSP 1068 – Craigieburn West

Precinct Structure Plan
Draft for Public Consultation

vpa
Victorian Planning Authority
November 2020

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1 INTRODUCTION

The Craigieburn West Precinct Structure Plan (the PSP) has been prepared by the Victorian Planning Authority (VPA) with the assistance of Hume City Council, Government agencies, service authorities and major stakeholders.

The PSP is a long-term plan for urban development. It describes how the land is expected to be developed, and how and where services are planned to support the development of new communities. The PSP is a set of decisions about how the land is to be developed, and it:

- Sets out plans to guide the delivery of quality urban environments in accordance with Victorian Government guidelines and policies.
- Enables the transition of non-urban land to urban land.
- Sets the vision for how the land should be developed and the outcomes to be achieved.
- Outlines the projects required to ensure that future residents, visitors and workers within the area can be provided with timely access to services and transport necessary to support a quality and affordable lifestyle.
- Sets out objectives, requirements and guidelines for land use, development and subdivision.
- Provides Government agencies, the Council, developers, investors and local communities with certainty about future development.
- Addresses the requirements of the *Environment Protection and Biodiversity Conservation Act 1999* (EPBC Act 1999) through Federal approval of the Biodiversity Conservation Strategy and Sub Regional Species Strategies for Melbourne's Growth Areas (September 2013).

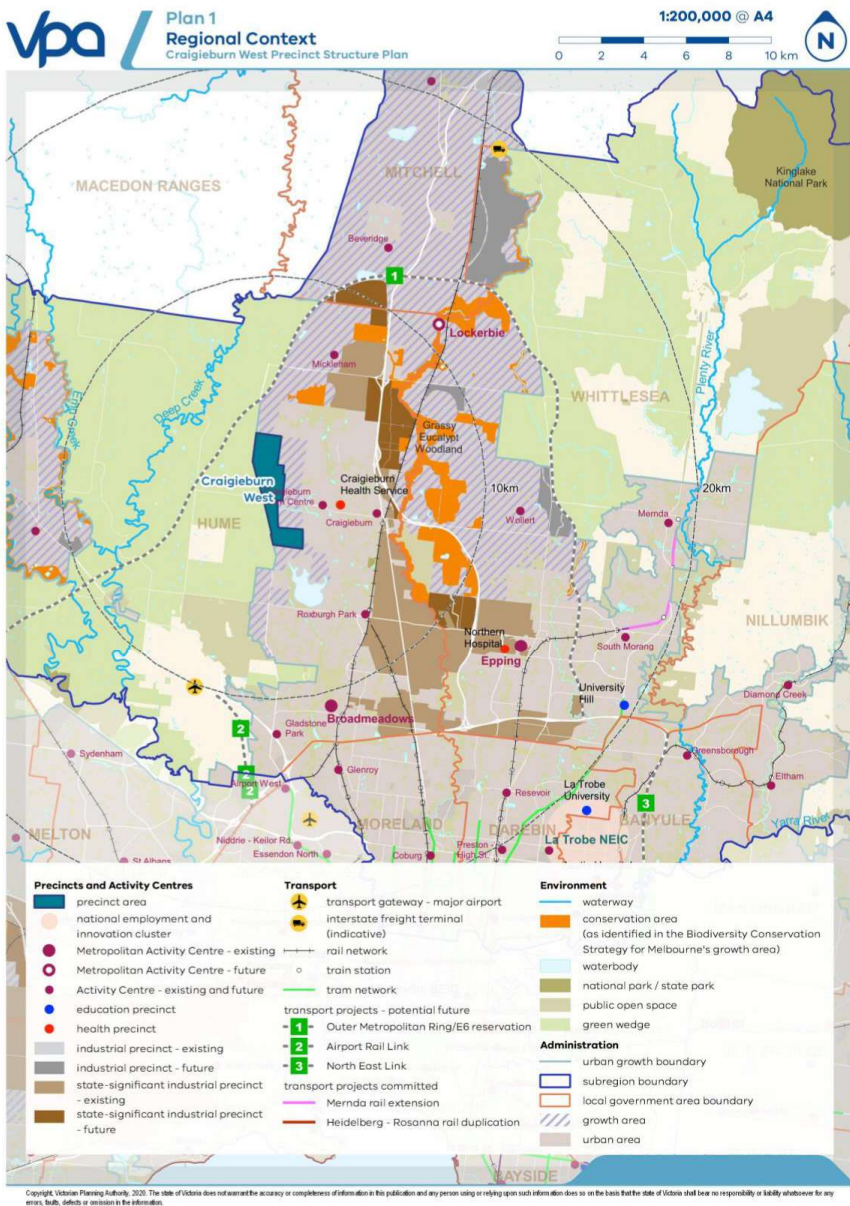
The PSP is informed by:

- Plan Melbourne – Metropolitan Planning Strategy, May 2017
- The Growth Corridor Plans: Managing Melbourne's Growth (Growth Areas Authority, June 2012)
- The Planning Policy Framework as set out in the Hume Planning Scheme
- The Local Planning Policy Framework as set out in the Hume Planning Scheme
- The Biodiversity Conservation Strategy and Sub Regional Species Strategies for Melbourne's Growth Areas (Department of Environment and Primary Industries, June 2013)
- The Precinct Structure Planning Guidelines.

The following documents have been developed in parallel with the PSP to inform and direct the future planning and development of the Precinct:

- The Craigieburn West Infrastructure Contributions Plan (ICP) requires development proponents to contribute toward infrastructure required to support the development of the Precinct.
- The Background Technical Studies undertaken to inform the preparation of the Craigieburn West PSP.

In preparing this PSP, the VPA has worked closely with Hume City Council, Melbourne Water, Department of Transport, DELWP and land owners.



1.1 How to read this document

This PSP guides land use and development where a planning permit is required under Schedule 12 to the Urban Growth Zone (Clause 37.07 of the Hume Planning Scheme), or any other provision of the Hume Planning Scheme that references this PSP.

A planning application and subsequent planning permit must implement the outcomes of the PSP. The outcomes are expressed as the VISION AND OBJECTIVES.

Each element of the PSP contains requirements and guidelines as relevant.

Requirements must be adhered to in developing the land. Where they are not demonstrated in a permit application, requirements will usually be included as a condition on a planning permit whether or not they take the same wording as in the structure plan. A requirement may reference a plan, table or figure in the structure plan.

Guidelines express how discretion will be exercised by the responsible authority in certain matters that require a planning permit. If the responsible authority is satisfied that an application for an alternative to a guideline, implements the outcomes, the responsible authority may consider the alternative. A guideline may include or reference a plan, table or figure in the PSP.

Meeting these Requirements and Guidelines will implement the vision of the PSP.

Conditions that must be included in a planning permit are outlined in Schedule 12 to the Urban Growth Zone (UGZ) in the Hume Planning Scheme.

Development must also comply with other Acts and approvals where relevant e.g. the Environment Protection and Biodiversity Conservation Act 1999 in the case of biodiversity or the Aboriginal Heritage Act 2006 in the case of cultural heritage, amongst others.

Not every aspect of the land's use, development or subdivision is addressed in this structure plan. A responsible authority may manage development and issue permits as relevant under its general discretion.

1.2 Infrastructure contributions plan

Development proponents within Craigieburn West Precinct will be bound by the Craigieburn West Infrastructure Contribution Plan (the ICP).

The ICP sets out requirements for infrastructure funding across the Craigieburn West Precinct.

The ICP will be incorporated in the Hume Planning Scheme.

Development proponents wishing to commence works prior to incorporation of this ICP may enter into agreements with Hume Council under Section 173 of the Planning and Environment Act 1987 to expedite contributions.

Commented [HCC1]: Do we support this based on Sunbury experience – q to Chris and Caroline

1.3 Background information

The Craigieburn West PSP Background Report provides detailed background information relating to the precinct, including its local and metropolitan context, history, landform and topography, biodiversity, drainage, open space, transport infrastructure, employment and community facilities. The report also summarises various background technical studies that have informed the preparation of the PSP Land to which this PSP applies.

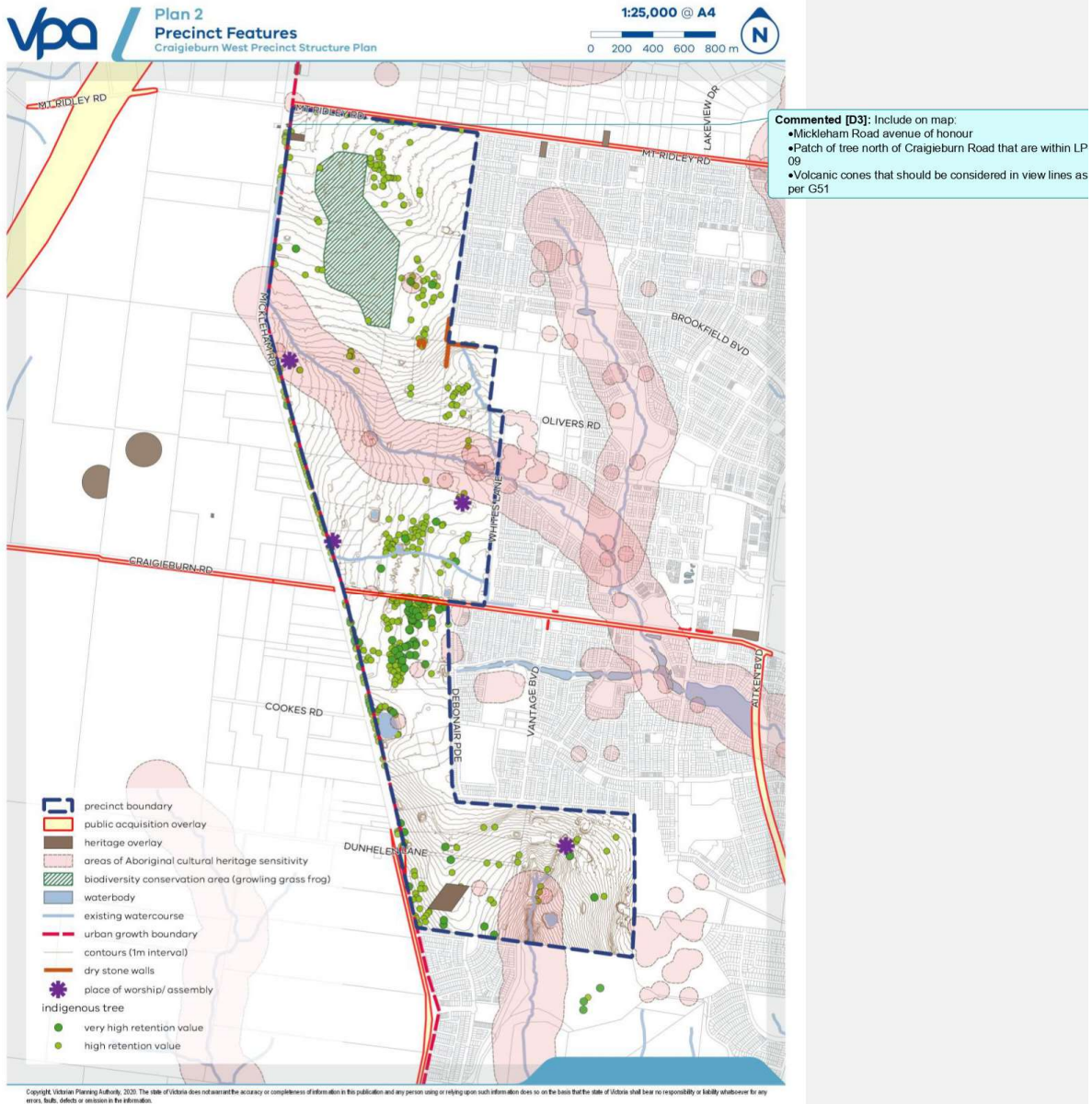
1.4 Land to which this PSP applies

The land to which this PSP applies is shown on Plan 2 and on the Hume Planning Scheme Maps as Schedule 12 to the Urban Growth Zone.

The PSP applies to approximately 562 hectares of land generally bound by Mt Ridley Road to the north, the Craigieburn R2 PSP area to the east, the Greenvale North R1 PSP area to the south and Mickleham Road to the west. The precinct benefits from the strategic planning work undertaken by surrounding PSP areas which will allow for a seamless integration of the future communities.

The precinct is bisected by Craigieburn Road running east/west and the Aitken Creek running north-west/south-east. Notable features of the precinct in addition to Aitken Creek include established tracts of native vegetation, including Biodiversity Conservation Area 29, and views to external landscape features including remnant volcanic cones and hilltops.

Commented [HCC2]: Refer to heritage features; existing places of worship; ugb forms western boundary with green wedge land to the west



2 OUTCOMES

2.1 Vision

Craigieburn West will develop as a series of predominantly residential neighbourhoods supported by a local [Town Centre](#) and adjoining residential areas.

Commented [HCC4]: Consistent language – also referred to as Activity Centre

The precinct will leverage its unusual linear form by creating a series of walkable neighbourhoods arranged along a north-south spine comprising open space links and key road connections. The precinct will also seek to embed heritage and landscape features within and around it by capitalising on opportunities to maximise views to nearby volcanic cones and integration with established native vegetation.

The central spine [incorporating a connector road and a north-south green 'spine' linear reserve](#) will support the primary place-making focus - creating energy and activation. The PSP features schools, community hubs, and diverse housing typologies linked with a range of open spaces, including conservation reserves, active open space and a network of local parks.

The PSP will complete the [delivery of structure planning process](#) for the area, completing the delivery of green links within and beyond the PSP boundaries and [provision of a sensitive built form interface to rural land west of Mickleham Road/Urban Growth Boundary](#).

Commented [HCC5]: Great – see comments on guidance below

The PSP will complete the catchment to surrounding activity centres external to the PSP, including Craigieburn Central, Aston Village and Highlands Village, while also providing for local facilities, including a centralised activity centre co-located with open space and community facilities, and a series of proposed government and potential non-government schools. [The open space network provides a range of recreation options throughout the community and by maintaining existing trees will continue the character of surrounding developed areas.](#)

2.2 Purpose

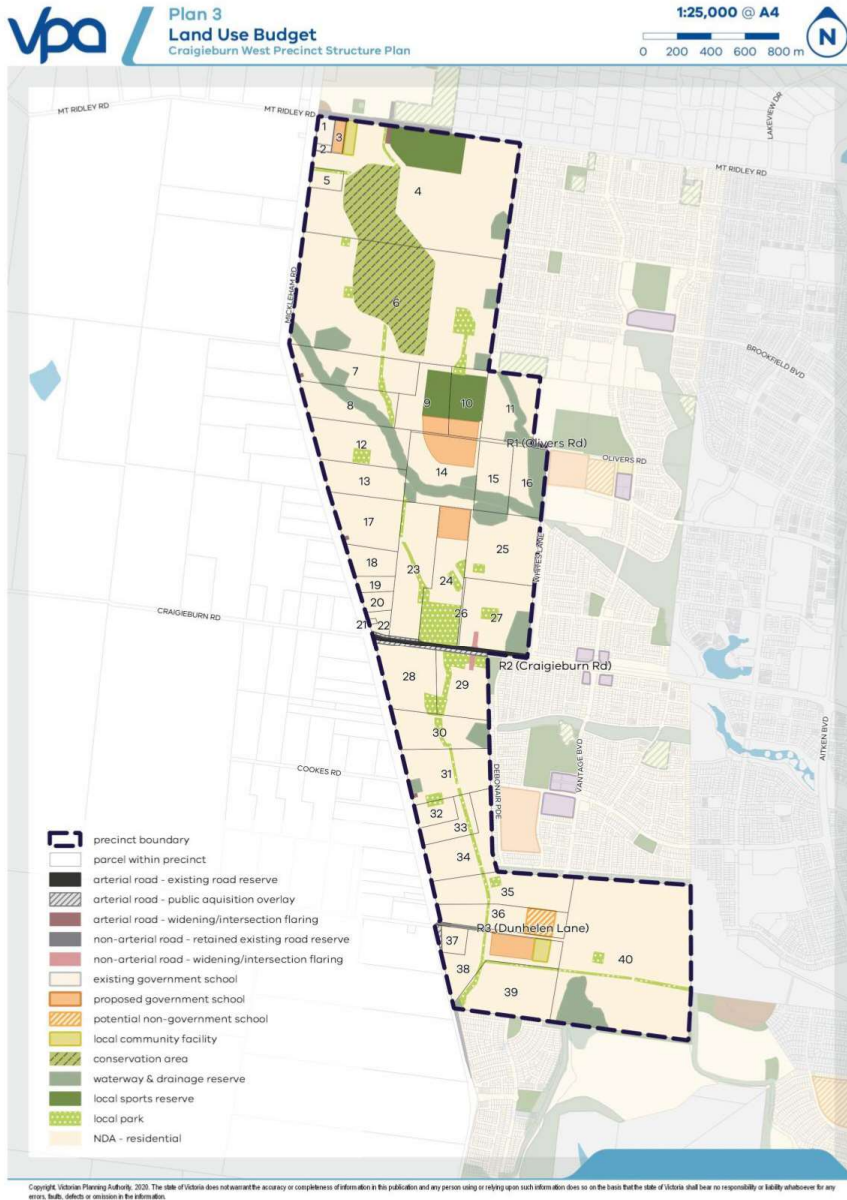
The purpose of the PSP is to embed the vision for the Craigieburn West precinct through the preparation of an orderly and integrated place based spatial plan. To this end, the PSP will:

- Support the timely delivery of integrated transport options, including public and active transport. Active transport is facilitated via a north south green 'spine' linear reserve.
- Support the retail and services catchment of the proposed [Craigieburn West](#) Local Town Centre, Craigieburn Central and town centres proposed in Aston Village and Highlands Village.
- Facilitate the final drainage outcomes for ~~to~~ the development services schemes for the Aitken Creek, Yuroke Creek and Upper Brodies Creek catchments, including the protection of the Greenvale Reservoir.
- Integrate the significant areas of vegetation and biodiversity into the future urban landscape and open space network and promotes connections to greenspace.
- Integrate with and deliver community facilities and open spaces to complement the adjoining precincts of Lindum Vale PSP, Craigieburn R2 PSP and the Greenvale North PSP.

2.3 Objectives

The following objectives describe the desired outcomes of the precinct's development and guide the implementation of the vision:

PSP OBJECTIVES	
01	Housing, subdivision & built form To facilitate housing diversity and choice within Craigieburn West, including densities that support access to local services, jobs and sustainable transport options.
02	Transport & movement To facilitate 20-minute neighbourhoods by providing a transport network that integrates with the adjoining established areas and supports active and public transport options, movement of goods and connections to jobs within Craigieburn West and the surrounding areas.
03	Public realm, open space & heritage To provide a framework for a high amenity and integrated urban environment within Craigieburn West that encourages a sense of place and community, as well as responds to the existing natural, cultural and built form features.
04	Water, utilities & safety To facilitate safe, resilient and water sensitive urban environments in Craigieburn West that respond to climate change, bushfire management and final drainage outcomes including the protection of the Greenvale Reservoir Drinking Water Catchment.
05	Biodiversity & ecosystems To facilitate the retention and protection of Conservation Area 29 and landscape features within Craigieburn West including scattered trees and waterways as key community assets that are integrated with the urban landscape.
06	Education & community infrastructure To identify and facilitate the delivery of adaptable and multi-purpose open spaces, community facilities, schools, and other essential community infrastructure to support development.
07	Centres, employment & economic activity To facilitate investment in an innovative and vibrant local and regional economy within a network of highly accessible activity and employment centres that support jobs and business activity for residents in Craigieburn West and surrounding areas.
08	Precinct infrastructure delivery To identify and guide the timely delivery and staging of key essential infrastructure required for Craigieburn West.

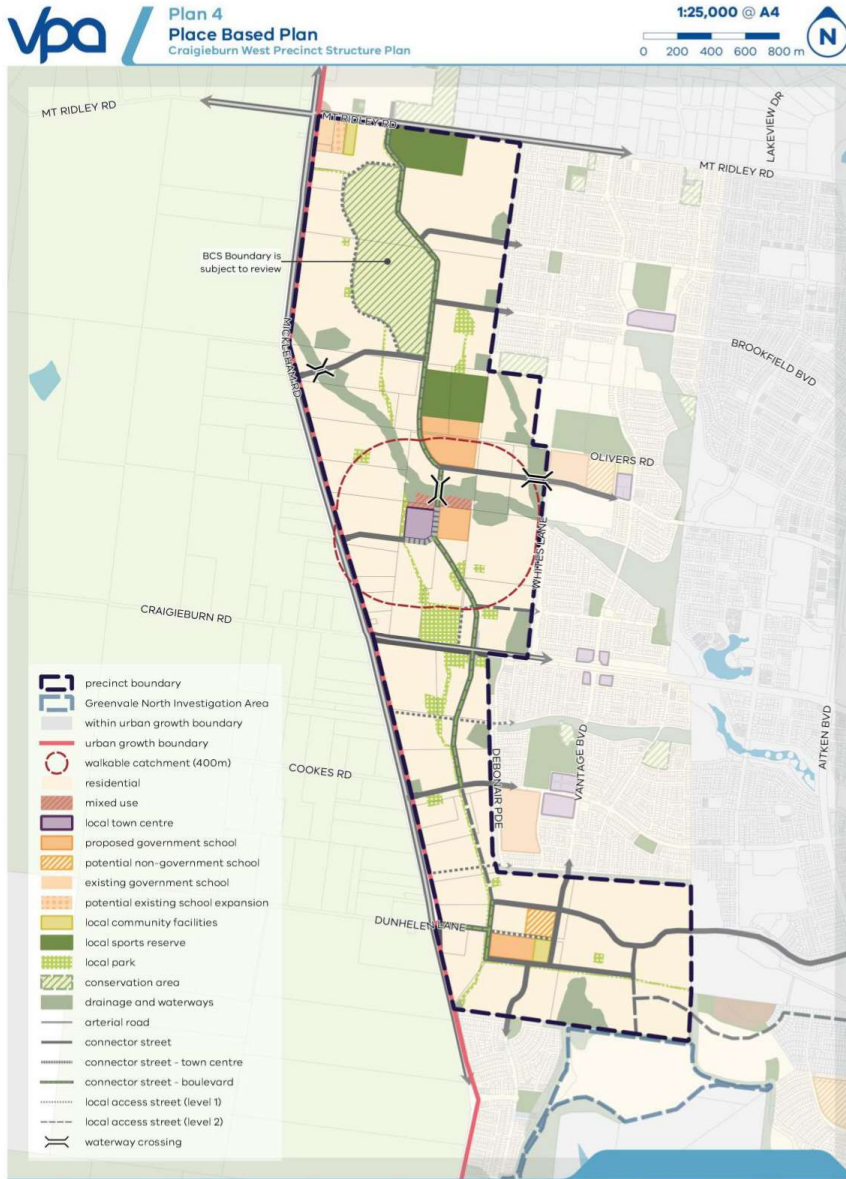




2.4 Precinct land use budget

Table 1: Precinct land use budget

Description	HECTARES	% OF TOTAL	% OF NDA
TOTAL PRECINCT AREA (ha)	562.34		
Transport			
Arterial road – existing road reserve	1.87	0.33%	0.45%
Arterial road – public acquisition overlay	2.16	0.38%	0.52%
Arterial road – new / widening / intersection flaring (ICP land)	0.73	0.13%	0.18%
Non-arterial road – retained existing road reserve	0.66	0.12%	0.16%
Non-arterial road – new / widening / intersection flaring (ICP land)	0.79	0.14%	0.19%
Sub-total transport	6.20	1.1%	1.50%
Community & education			
Existing government school	2.04	0.36%	0.49%
Government school	16.85	3.00%	4.07%
Potential non-government school	2.50	0.44%	0.60%
Local community facility (ICP land)	2.40	0.43%	0.58%
Sub-total education	23.80	4.2%	5.7%
Open space			
Service open space			
Conservation reserve	37.70	6.70%	9.10%
Waterway and drainage reserve	38.55	6.85%	9.30%
Sub-total service open space	76.24	13.56%	18.40%
Credited open space			
Local sports reserve (ICP land)	19.00	3.4%	4.59%
Local network park (ICP land)	22.78	4.1%	5.50%
Sub-total credited open space	41.78	7.4%	10.08%
Total all open space	118.02	21.0%	28.49%
TOTAL NET DEVELOPABLE AREA – (NDA) Ha	414.31	73.68%	
NET DEVELOPABLE AREA – RESIDENTIAL (NDAR) Ha	414.31	73.68%	
NET DEVELOPABLE AREA – EMPLOYMENT (NDAE) Ha	0.00	0.00%	





3 IMPLEMENTATION & DELIVERY

3.1 Housing, subdivision & built form

REQUIREMENTS	
R1	Subdivision layouts, lot diversity and housing typologies must respond to the natural and existing built features of the surrounding developed area, including (but not limited to): <ul style="list-style-type: none"> Topographical features; Aitken Creek and Yuroke Creek tributaries; and Rural landscape interface west of old Mickleham Road.
R2	Subdivision must provide a diverse neighbourhood character by providing a range of lot sizes and dwelling types in appropriate locations throughout the Precinct, including achieving minimum average densities and planned neighbourhood character as specified in Plan 4 and Table 2.
R3	In order to reorient school buildings and access away from Mickleham Road and Mt Ridley Road, subdivision adjacent to the existing Mickleham Primary (and the proposed expansion) as indicated on Place Based Plan (Plan 4) must provide an internal subdivision layout which facilitates access to the School from the internal road network. Local streets adjacent to the school must provide a road easement wide enough to allow for school bus movement while accommodating on-street parking and two way traffic movement in accordance with the the Department of Transport's guidance for public transport and land use development.
R4	Development along Mickleham Road and Mt Ridley Road must provide a sensitive rural interface through design treatments, which include <u>aa landscaped nature strip between the row of housing and road reservation.</u>
R5	An application for subdivision of land into residential lots or development of land for residential or mixed-use purposes must provide affordable housing as defined by the Planning & Environment Act 1987 to the satisfaction of the responsible authority

Commented [D6]: Reference to "old" Mickleham Road potential typo

Commented [D7]: Amend to include reference to the provision of an internal loop road

Commented [HCC8]: Suggest indicative cross section

GUIDELINES	
G1	Subdivisions that retain lots around existing dwellings should be designed to ensure that the future subdivision of retained lots will appropriately integrate with the surrounding subdivision layout.
G2	Subdivision layouts and development should respond to and address the relevant elements of the Urban Design Guidelines for Victoria.
G3	Lots should front (in order of priority where a lot fronts multiple elements): <ul style="list-style-type: none"> Public open space. Local access streets. Connector roads. Arterial roads.
G4	Applications for residential subdivision or development should provide an equivalent of up to 10% of the total number of dwellings forecast to be provided (and may be provided as constructed dwellings or land or otherwise) <u>as affordable housing.</u> The affordable housing should: <ul style="list-style-type: none"> be provided within <u>walkable catchments</u> where practicable; provide for a range of housing typologies to meet demonstrated local need; and provide for very low-, <u>and low-, and moderate-income</u> households
G5	Subdivision should deliver a broad mix of lots that are an appropriate size and shape to support the planned neighbourhood character of the precinct, as specified in Table 2, by: <ul style="list-style-type: none"> Providing a range of lot sizes, widths, depths and densities Providing higher residential densities and more intensive building typologies in locations where they will: <ul style="list-style-type: none"> Support the viability and vibrancy of activity centres, access to community infrastructure and amenities. Have good access to public transport and support walking and cycling.

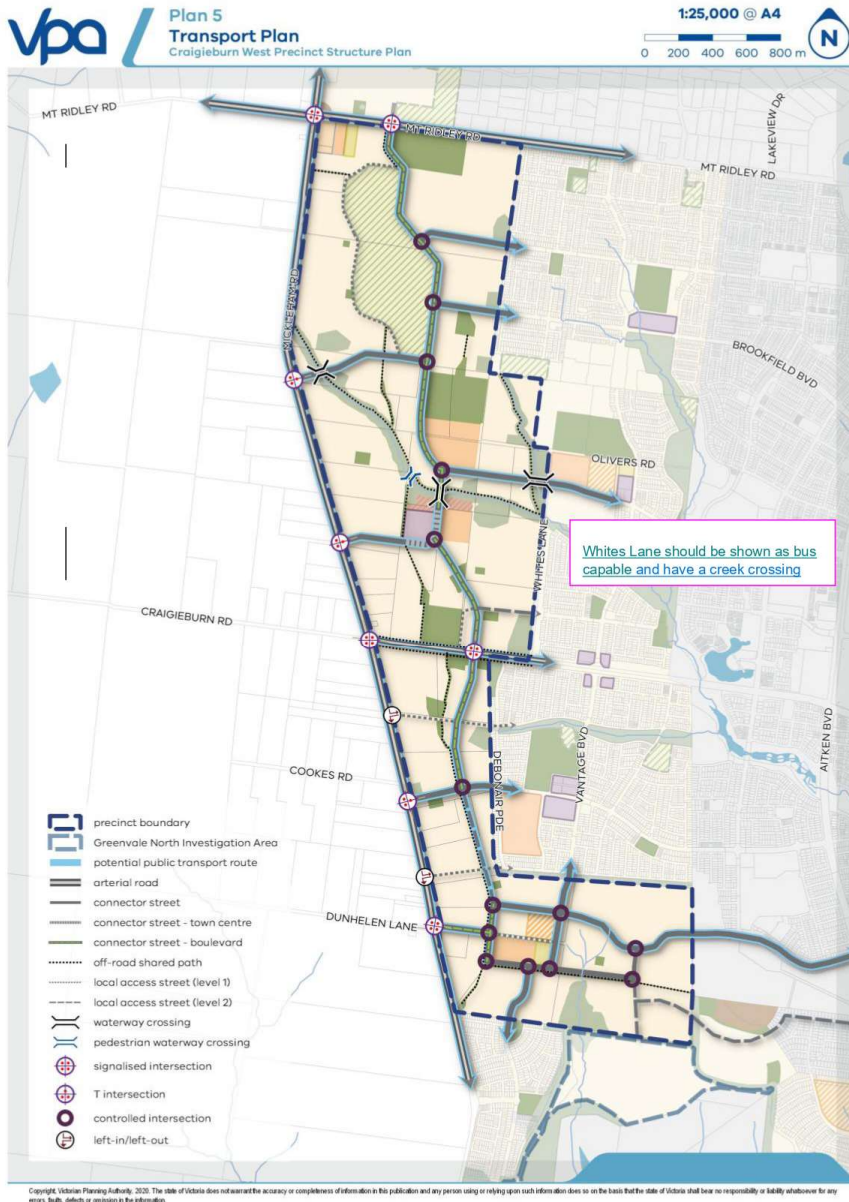
Commented [D9]: Include specific reference to walkable catchments, ie of: activity centres, schools and community infrastructure

Commented [HCC10]: Greatest need for affordable housing Hume is for low and very low income households as market prices accessible to moderate income households

	<ul style="list-style-type: none"> o Make a positive impact to planned neighbourhood character and identity through the incorporation of remnant vegetation (where indicated in Plan 10).
G6	<p>Subdivision should provide for a street separating development from waterways, sporting reserves and local parks and the linear reserve.</p> <p>Where subdivision does not propose a local street separating development, design and layout options should demonstrate:</p> <ul style="list-style-type: none"> • Lots directly fronting open space and landscape value areas should be set back at least 4.0 metres from the waterway corridor and open space. • Lots directly fronting open space should allow for vehicular access via a rear laneway. • A "paper road" should be provided as the primary point of access from a footpath or shared path with a minimum width of 1.5 metres along the lot frontage. • Subdivision design should avoid side or rear fence treatments fronting open space. • Subdivision design should maximise opportunities for informal passive surveillance. • Subdivision design should not limit the use of adjacent open space. <p>All to the satisfaction of the responsible authority and <u>where adjacent to a waterway, the satisfaction of both the responsible authority and Melbourne Water, where adjacent to a waterway.</u></p>
G7	<p>Subdivision applications for super-lots identified for future medium density, high density, or integrated housing should demonstrate:</p> <ul style="list-style-type: none"> • Expected dwelling density in line with Table 2. • Connections and active interfaces with adjacent streets, open space and waterways. • Safe and effective internal vehicle and pedestrian circulation. • Indicative treatments for interfaces with non-residential land uses.
G8	<p>Specialised housing forms, such as retirement living, or aged care should:</p> <ul style="list-style-type: none"> • Respond to and integrate with adjoining development, avoiding inactive interfaces and blank facades to the public street network. • Be located within the walkable catchment shown on Plan 4. • Be accessible by public transport. • Not present a barrier to movement through the surrounding road and pedestrian movement network.
G9	<p>Any retaining structures within public and private spaces (except for those which are part of a building) should be:</p> <ul style="list-style-type: none"> • No more than 1.0 metres in height between a dwelling and a street or public space, or where visible from a street or public space. • Set back at least 1.0 metres from any building envelope. • Staggered, with a minimum 0.75 metre distance between each stagger to allow for the inclusion of landscaping where cutting and filling is deeper than 1.0 metres. • Positioned so that associated drainage infrastructure and structural foundation are fully located within the same lot. <p>Please note: Melbourne Water will not accept ownership or maintenance responsibilities associated with retaining structures in land to be vested to them.</p>

Table 2: Housing density guide and planned neighbourhood character

DEVELOPMENT AREA	PLANNED NEIGHBOURHOOD CHARACTER	MINIMUM AVERAGE DENSITY (DW/NDHA)
Standard residential outside walkable catchment	Development will have a traditional suburban neighbourhood character characterised by buildings up to three storeys in height. Housing will generally comprise detached and semi-detached typologies. However, more intensive forms of development such as terraced homes and townhouses should be provided in proximity to areas of high amenity, or where it can be demonstrated that a positive contribution will be made to neighbourhood character and identity.	18.5
Residential within walkable catchment	Development will have an urban neighbourhood character, characterised by buildings up to four storeys in height. Housing will comprise a variety of typologies, including low-rise apartments buildings, terraced homes and townhouses (including rear-loaded product), and detached dwellings.	26.5





3.2 Transport & movement

3.2.1 Public transport

REQUIREMENTS	
R6	Any road nominated in Plan 5 as a potential public transport route must be constructed (including partial construction where relevant) in accordance with the corresponding cross section in the PSP and the Department of Transport's guidance for public transport and land use development.

GUIDELINES	
G10	Bus stop facilities should be designed as integral parts of roadways adjoining of the town centre and activity generating land uses such as schools, sports fields and employment areas.
G11	The street network should be designed to ensure all households have direct and convenient walking access to public transport services.

3.2.2 Walking & cycling

REQUIREMENTS	
Development of the linear park as shown on Plan 5 and Plan 8 must:	
<ul style="list-style-type: none"> • Provide for a shared path (pedestrian and cycling) which is interconnected with the surrounding footpath and cycle network. • Be designed and located to provide efficient movement of pedestrians and cyclists. • Provide for pedestrian and cyclist priority over vehicular traffic. • Ensure that where a road crosses the linear park, the road is raised with priority given to the linear park. • Utilise the cross section designs in Appendix 4.5 • Have a standard minimum width of 15m or 10m where adjacent to a connector road, unless otherwise agreed to by responsible authority. 	

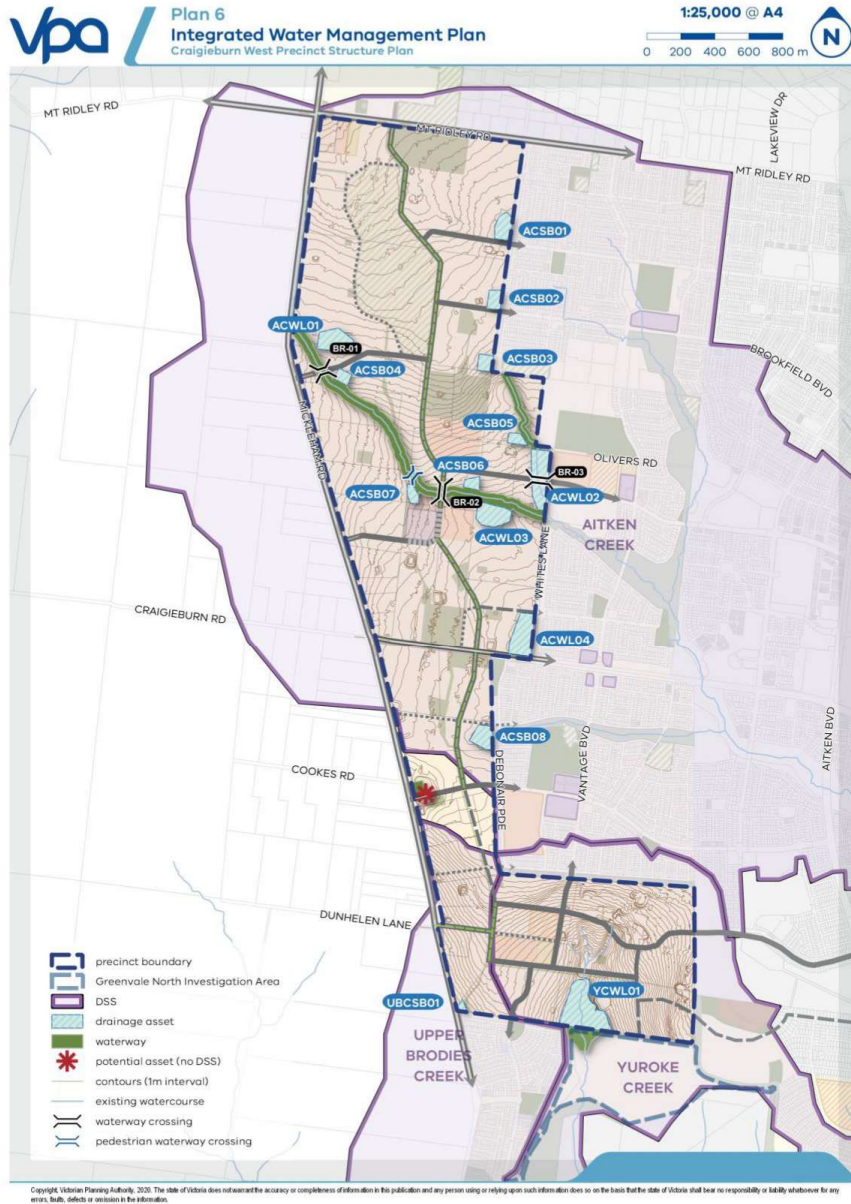
Commented [HCC11]: Cross sections in Appendix 4.5 omit detail for linear park. Council's submission has provided cross sections to include in final psp.

GUIDELINES	
G12	Location of walkways or pedestrian and cycle paths in addition to those described through the standard cross sections should consider the need for appropriate lighting and passive surveillance.
G13	The alignment of dedicated off-road bicycle paths should be designed for cyclists travelling up to 30km/hr.
G14	In addition to waterway crossings shown on Plan 5, development proponents should provide waterway crossings at intervals no greater less than 400m or corresponding with all perpendicular through roads or pedestrian and cycle paths.
G15	High quality walking and cycling links are encouraged to connect destinations within and adjoining the PSP that are not directly serviced by the linear park.



3.2.3 Street network

REQUIREMENTS									
R7	Design of all subdivisions and streets must provide: <ul style="list-style-type: none"> A permeable, direct and safe street network prioritising walking and cycling. Safe and convenient crossing points of connector roads and local streets at all intersections and on key desire lines as well as crossing waterways. Safe pedestrian crossings of arterial roads at all intersections, at key desire lines, and on regular intervals appropriate to the function of the road and public transport provision. Safe and convenient transition between on- and off-road bicycle networks. Convenient access to regional and local points of interest and destinations for effective integration with neighbouring properties, parkland and sports reserves. Direct and convenient walking access to public transport services. 								
R8	Vehicle access to lots fronting arterial roads must be provided from the local internal loop road or rear lane, to the satisfaction of the Road Authority.								
GUIDELINES									
G16	Slip lanes should be avoided in areas of high pedestrian activity (including schools and the Local Town Centre) and only provided at intersections between connector streets and arterial roads where they are necessitated by high traffic volumes but with pedestrian priority crossings.								
G17	Culs-de-sac should not detract from convenient pedestrian and vehicular connections.								
G18	The frequency and impact of vehicular crossovers on verges of connector roads should be minimised by applying a combination of: <ul style="list-style-type: none"> Rear loaded lots with laneway access. Vehicular access from the side streets. Combined or grouped crossovers. Increased lot widths. 								
G19	All signalised intersections <u>where located on arterial roads</u> should be designed having regard to the Department of Transport (DOT) working document <i>Guidance for Planning Road Networks in Growth Areas</i> November 2015 (as updated), to the satisfaction of The Head, <u>Department of Transport for Victoria</u> and the responsible authority.								
G20	Street trees should be provided on both sides of all roads and streets (excluding laneways) at regular intervals appropriate to tree size at maturity, unless otherwise agreed by the responsible authority. <table border="1"> <thead> <tr> <th>Average interval</th><th>Tree size</th></tr> </thead> <tbody> <tr> <td>8–10 metres</td><td>Small (less than 10 metre canopy)</td></tr> <tr> <td>10–12 metres</td><td>Medium (10–15 metre canopy)</td></tr> <tr> <td>12–15 metres</td><td>Large (canopy larger than 15 metres)</td></tr> </tbody> </table> <p>The design and siting of street trees should address relevant council policies & guidelines.</p>	Average interval	Tree size	8–10 metres	Small (less than 10 metre canopy)	10–12 metres	Medium (10–15 metre canopy)	12–15 metres	Large (canopy larger than 15 metres)
Average interval	Tree size								
8–10 metres	Small (less than 10 metre canopy)								
10–12 metres	Medium (10–15 metre canopy)								
12–15 metres	Large (canopy larger than 15 metres)								
G21	A variety of road cross sections should be utilised in a subdivision layout to create differentiation and neighbourhood character. <p>Alternative cross sections should ensure that:</p> <ul style="list-style-type: none"> Minimum required carriageway dimensions are maintained to ensure safe and efficient operation of emergency vehicles on all streets as well as buses on connector streets. The performance characteristics of standard cross sections as they relate to pedestrian and cycle use are maintained. Relevant minimum road reserve widths for the type of street are maintained, unless otherwise approved by the responsible authority. 								



3.3 Water, utilities & bushfire safety

3.3.1 Integrated water management

REQUIREMENTS	
R9	Development must give effect to relevant policies and strategies being implemented by the responsible authority, Melbourne Water and Yarra Valley Water, including the Healthy Waterways Strategy and any approved integrated water management plan.
R10	Stormwater conveyance and treatment (including interim solutions) must be designed to avoid or mitigate the risk of erosion from sodic/dispersive soils and in accordance with the relevant Development Services Scheme and Plan 6 to the satisfaction of Melbourne Water and the responsible authority. Note: this may result in variation to the Melbourne Water DSS as shown on Plan 6.
R11	Final designs and boundaries of constructed wetlands, retarding basins, stormwater quality treatment infrastructure, and associated paths, boardwalks, bridges, and planting, must include appropriate treatments to provide protection for dispersive soils where these are present and be designed to the satisfaction of both Melbourne Water and the responsible Authority.
R12	Development staging <u>must should</u> provide for the delivery of ultimate waterway and drainage infrastructure, including stormwater quality treatment. Where this is not possible, development proposals must demonstrate how any interim solution adequately manages and treats stormwater generated from the development and how this will enable delivery of an ultimate drainage solution, to the satisfaction of Melbourne Water and the responsible authority.
R13	Stormwater runoff from the development must meet the performance objectives of the CSIRO Best Practice Environmental Management Guidelines for Urban Stormwater prior to discharge to receiving waterways and as outlined on Plan 6, unless otherwise approved by Melbourne Water and the responsible authority. Proposals that exceed the performance objectives are highly encouraged and can be considered, to the satisfaction of the relevant authorities.
R14	The design and layout of connector street network and open spaces (including linear links) must ensure the long-term viability of vegetation (especially existing mature River Red Gums) and optimise water use efficiency through the use of overland flow paths and stormwater harvesting for passive irrigation and Water Sensitive Urban Design initiatives.
R15	Applications must demonstrate, through the preparation of Integrated Water management Management Plans: <ul style="list-style-type: none"> Waterways and integrated water management design enables land to be used for multiple recreation and environmental purposes. Overland flow paths and piping within road reserves will be connected and integrated across property/parcel boundaries. Melbourne Water and the responsible authority freeboard requirements for overland flow paths will be adequately contained within the road reserves. Relevant Integrated Water Management (IWM) requirements of this PSP will be achieved to the satisfaction of the retail water authority, including the supply of recycled water where required by the relevant water authority.

Commented [HCC12]: Support similar wording for road delivery in xx

GUIDELINES	
G22	Where practical, development should include integrated water management initiatives to reduce reliance on potable water and increase the utilisation of storm and wastewater contributing to a sustainable urban environment.
G23	Where practical, integrated water management systems should be designed to: <ul style="list-style-type: none"> Maximise habitat values for local flora and fauna species. Enable future harvesting and/or treatment and re-use of stormwater. Protect and manage MNES values, particularly within conservation areas, in relation to water quality and suitable hydrological regimes (both surface and groundwater). Recognise and respond to Aboriginal cultural heritage significance.
G24	The design and layout of roads, road reserves, and public open space should optimise water use efficiency and long-term viability of vegetation and public uses through the use of overland flow paths,

Commented [HCC13]: Move these points into requirements as discretion of guidelines not appropriate

	Water Sensitive Urban Design initiatives such as street swales, rain gardens and/or locally treated storm water for irrigation to contribute to a sustainable and green urban environment.
G25	Where practical, and where primary waterway or conservation functions are not adversely affected, land required for integrated water management initiatives should be integrated with the precinct open space and recreation system and as depicted on Plan 86 and Table 35.
G26	Site specific Integrated Water Management initiatives are encouraged in medium density, commercial and public use developments.
G27	Drainage infrastructure should be designed to avoid and mitigate the impact of earthworks on the health and viability of retained river red gums.
G28	Subdivision and development in areas identified as being affected by sodic and dispersive soils should be managed to avoid or mitigate the potential risk of erosion, both in the master planned design response to the subdivision, during construction phase, and on an ongoing basis.
G29	<p>Stormwater runoff in areas identified as being affected by sodic and dispersive soils should be designed to manage the potential risk of erosion.</p> <p>Potential management methods may include but <u>are not</u> limited to:</p> <ul style="list-style-type: none"> • Widening the buffer distances between the core riparian zone and the outside vegetated buffers that allows sufficient tolerances for channel migration. • Diversion of water away from sodic and dispersive materials. • Minimising potential convergence and/or ponding of surface flows. • Compacting to reduce pore spaces and minimise water movement through material. • Physical and chemical soil ameliorants. • Maintenance of topsoil across undisturbed land, preferably with grasses to provide surface soil stability and root anchorage. • Minimise the amount of time land is exposed (e.g. by staging development). • Ensure that culverts and drains excavated into dispersive subsoils are capped with non-dispersive topsoil, gypsum stabilised and vegetated.



3.3.2 Utilities

REQUIREMENTS	
R16	All existing above ground electricity cables (excluding substations and cables with voltage 66kv or greater) must be placed underground as part of the upgrade of existing roads or subdivision works.
R17	All new electricity supply infrastructure (excluding substations and cables with voltage 66kv or greater) must be provided underground.
R18	Above ground utilities must be identified at the subdivision design stage to ensure effective integration with the surrounding neighbourhood, to minimise amenity impacts and be designed to the satisfaction of the Relevant Authority. Where that infrastructure is intended to be located in public open space, the land required to accommodate that infrastructure will not be counted as contributing to public open space requirements specified and will be additional to the areas designated in Table 45.
R19	Utilities and other infrastructure must avoid traversing Conservation Area 29.

GUIDELINES	
G30	The delivery of underground services should be coordinated, located and bundled (utilising common trenching) to maintain the cross section widths of pedestrian paths and nature strips as shown in the PSP and to facilitate trees and other planting within road reserves.
G31	Utilities should be placed outside of conservation areas, natural waterway corridors or on the outer edges of these corridors in the first instance. Where services cannot avoid crossing or being located within a conservation area or natural waterway corridor, they must be located to avoid disturbance to existing waterway values, native vegetation, significant landform features and heritage sites, to the satisfaction of Melbourne Water and the responsible authority.
G32	All new above-ground utilities, including temporary utilities, should be located outside of key view lines and screened with vegetation, as appropriate.
G33	Trunk services should be placed along <u>general alignments</u> as advised by the relevant servicing authorities.
G34	Design and location of underground services should be guided by Appendix 4.6.
G35	Utility easement to the rear of lots should only be provided where there is no practical alternative.

Commented [HCC14]: Clarify which general alignments this refers to

Table 3: Water infrastructure

Drainage Scheme	Asset ID	Asset Type		Area (ha)
Aitken Creek	ACSB-01	Sediment Basin	TBC – Council/MWC	1.36
Aitken Creek	ACSB-02	Sediment Basin	TBC – Council/MWC	0.89
Aitken Creek	ACSB-03	Sediment Basin	TBC – Council/MWC	0.82
Aitken Creek	ACSB-04	Sediment Basin	TBC – Council/MWC	0.60
Aitken Creek	ACSB-05	Sediment Basin	TBC – Council/MWC	0.59
Aitken Creek	ACSB-06	Sediment Basin	TBC – Council/MWC	0.56
Aitken Creek	ACSB-07	Sediment Basin	TBC – Council/MWC	0.59
Aitken Creek	ACSB-08	Sediment Basin	TBC – Council/MWC	1.43*
Aitken Creek	ACWL-01	Wetland	TBC – Council/MWC	2.15
Aitken Creek	ACWL-02	Wetland	TBC – Council/MWC	1.46*
Aitken Creek	ACWL-03	Wetland	TBC – Council/MWC	2.21
Aitken Creek	ACWL-04	Wetland	TBC – Council/MWC	2.92
SUB TOTAL				15.59
Upper Brodies Creek	UBCSB-01	Sediment Basin	Council	0.22
SUB TOTAL				0.22
Yuroke Creek	YCWL-01	Wetland/Retarding Basin	Melbourne Water	5.26
SUB TOTAL				5.26
DSS "Gap"	N/A	Retarding & Sediment Basin	TBC	0.52
SUB TOTAL				0.52
TOTAL				21.59

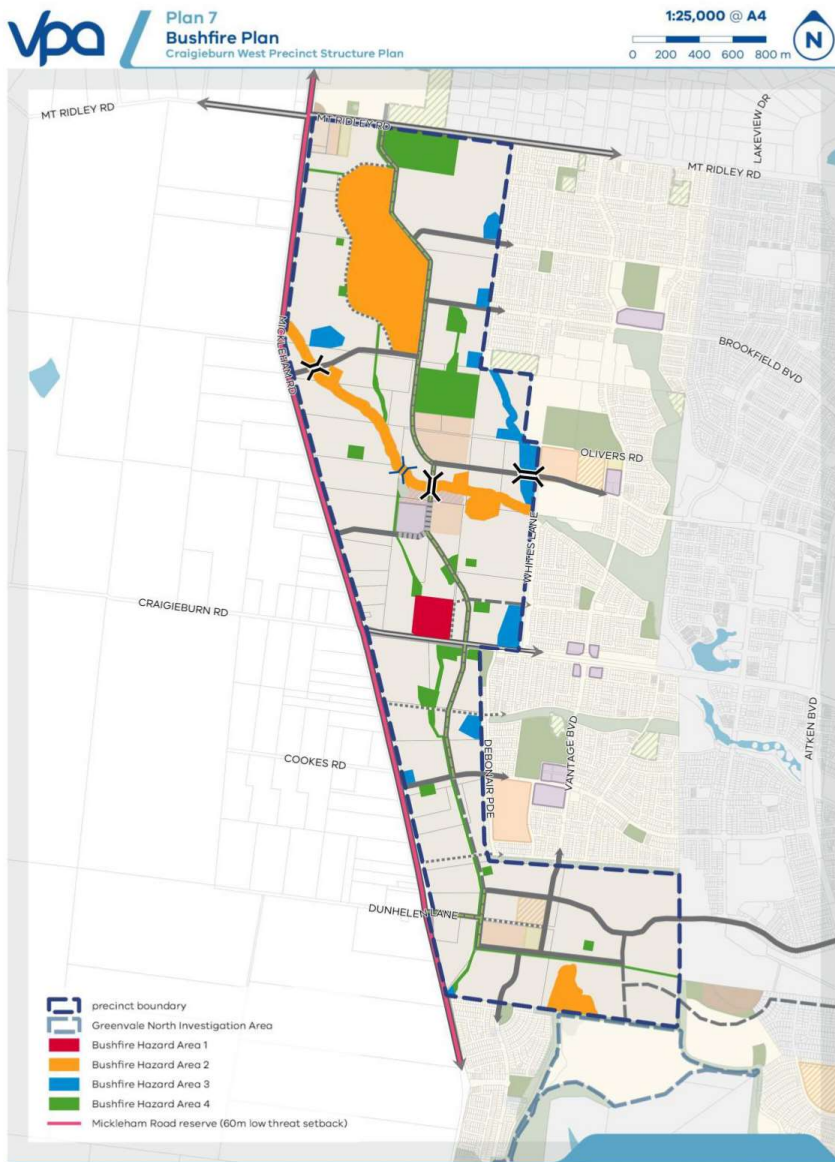
Commented [D15]: Council will provide details of responsibility of asset ownership through consultation on DSS by Melbourne Water

Note: The areas identified in this table are subject to change/confirmation during the functional and detailed design stage to the satisfaction of Melbourne Water and the responsible authority.

* Negotiations with landowner are still ongoing regarding the final design and land-take for these assets.

* 3.26 total, 1.8ha included waterway corridor.

MWC= Melbourne Water Corporation



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Victorian Planning Authority

3.3.3 Bushfire management & safety

REQUIREMENTS	
R20	Vegetation within bushfire hazard areas shown on Plan 7 must be managed in accordance with <u>the corresponding vegetation class in Table 4.</u>
R21	Development adjoining bushfire hazards shown on Plan 7 must be setback in accordance with Table 4
R22	<p>Where a setback from a bushfire hazard area is required by Table 4, unless otherwise agreed by the responsible authority and relevant fire authority, vegetation within the setback must be managed as follows:</p> <ul style="list-style-type: none"> Grass must be short cropped and maintained during the declared fire danger period. All leaves and vegetation debris must be removed at regular intervals during the declared fire danger period. Within 10 metres of a building, flammable objects must not be located close to the vulnerable parts of the building. Plants greater than 10 centimetres in height must not be placed within 3m of a window or glass feature of the building. Shrubs must not be located under the canopy of trees. Individual and clumps of shrubs must not exceed 5 sq. metres in area and must be separated by at least 5 metres. Trees must not overhang or touch any elements of the building. The canopy of trees must be separated by at least 2 metres. There must be a clearance of at least 2 metres between the lowest tree branches and ground level.
RXX	<u>Buildings must not be constructed within the setback created as a buffer to bushfire hazard areas.</u>

COMMENTED

[D16]: How can this requirement be implemented by Council?

COMMENTED

[HCC17]: How are these outcomes implemented?

COMMENTED

[HCC18]: As above

COMMENTED

[HCC19]: Alternative to the highlighted points in R22

FORMATTED

No bullets or numbering

GUIDELINES	
G36	All vegetation outside of a bushfire hazard area shown on Plan 7 should be managed to ensure a low risk of bushfire.
G37	Subdivision adjoining a bushfire hazard area should include a publicly accessible perimeter road.
G38	Subdivision should include a network of streets that provide multiple evacuation routes away from bushfire risks and areas of bushfire hazard.
G39	Where a setback is required from a bushfire hazard, the setback should be provided on public land where practical.
G40	All fencing adjoining Bushfire Hazard areas 1, 2 & 3 shown on Plan 7 should be made from non-combustible materials.
G41	Landscape design and plant selection in open spaces, including waterways and drainage corridors, should not increase bushfire risk.

COMMENTED

[HCC20]: Is there any statutory trigger that will allow this guideline to be considered and managed?

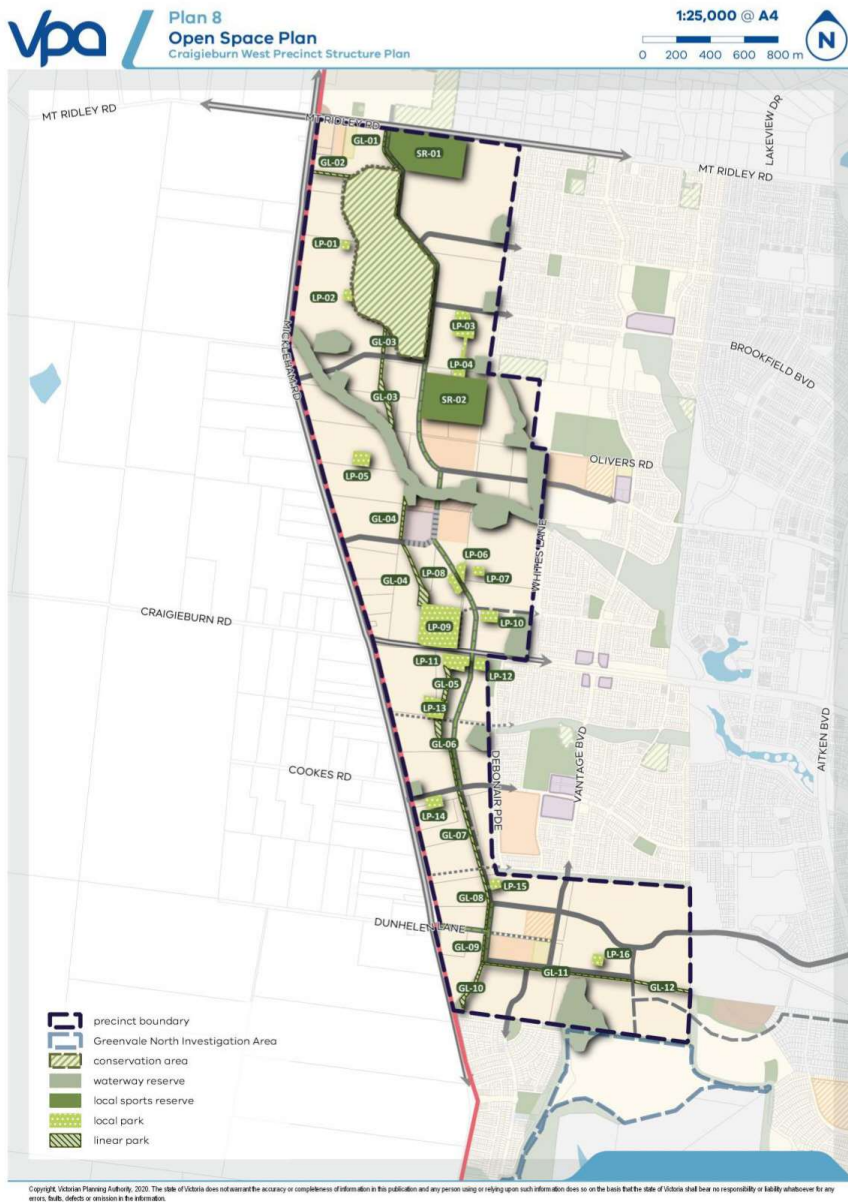
COMMENTED

[HCC21]: Is there any statutory trigger that will allow this guideline to be considered and managed?

CRAIGIEBURN WEST PRECINCT STRUCTURE PLAN – DRAFT FOR PUBLIC CONSULTATION – NOVEMBER 2020
27



Setback distance from bushfire hazard area	33m	19m	0m	0m
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3.4 Public realm, open space & heritage

3.4.1 Open space and natural system

REQUIREMENTS	
R23	Trees in streets, civic places and the open space network must be provided in accordance with Council's policies and guidelines, and: <ul style="list-style-type: none"> • Complement the existing native indigenous and exotic species. • Be larger species to facilitate continuous canopy cover. • Be planted in modified and improved soil to support tree establishment. • Be appropriately sized to nature strips, nearby utilities and buildings. • Suited to local conditions.
R24	The first development proponent to lodge a permit application for land which contains a section of the linear park as outlined on Plan 8 must undertake a master plan for that section of the entire linear park, unless otherwise agreed by the responsible authority. The masterplan may be prepared in separate stages (i.e. north and south of Craigieburn Road) to the satisfaction of the responsible authority using the cross sections in Appendix xx.
R25	Development of the linear park as shown on Plan 8 must: <ul style="list-style-type: none"> • Accommodate the full Tree Protection Zone of all River Red Gums shown as must be retained on Plan 10 within the linear park. • Ensure pedestrian access is provided to all residential lot frontages via a road or paper road.
GUIDELINES	
G42	Local parks should be generally be provided where shown on Plan 8 and as outlined in Table 4.5.
G43	Alternative locations and configurations for local parks (other than the linear park) may be considered, subject to: <ul style="list-style-type: none"> • Addressing the required locational attributes as outlined in Table 4.5. • Not diminishing the quality or usability of the space. • Not adversely impacting on the overall diversity of the precinct open space network. • Being equal to or more than the passive open space provision shown in Table 4.5 • Still being supported by the preferred path network outlined in Plan 5.
G44	Existing high-quality vegetation should be retained within public space, including road reserves and open spaces, where safe and practicable.
G45	The open space network should: <ul style="list-style-type: none"> • Maximise the amenity and value of service open space through the provision of shared paths, trails and other recreational elements. • Respond to the values of adjoining open space, waterways, and Aboriginal and post-contact heritage. • Provide flexible recreational opportunities that allow for the anticipated range of sporting reserves, and local parks and recreational uses required by the community.
G46	Where a local park illustrated on Plan 8 spans multiple parcels, the first development proponent to lodge a permit application for land containing the park should prepare a master plan for the entire park. Consultation with all relevant landowners should be undertaken as part of the master plan preparation.
G47	Any fencing of open space should be low scale and visually permeable to facilitate public safety and surveillance.
G48	Public recreation and open space areas should be located adjacent to significant landscape value areas and waterways to create and or enhance any buffer area.
G49	Where possible, salvaged rocks should be incorporated into the design of waterways, retaining structures, fences and other landscape features.

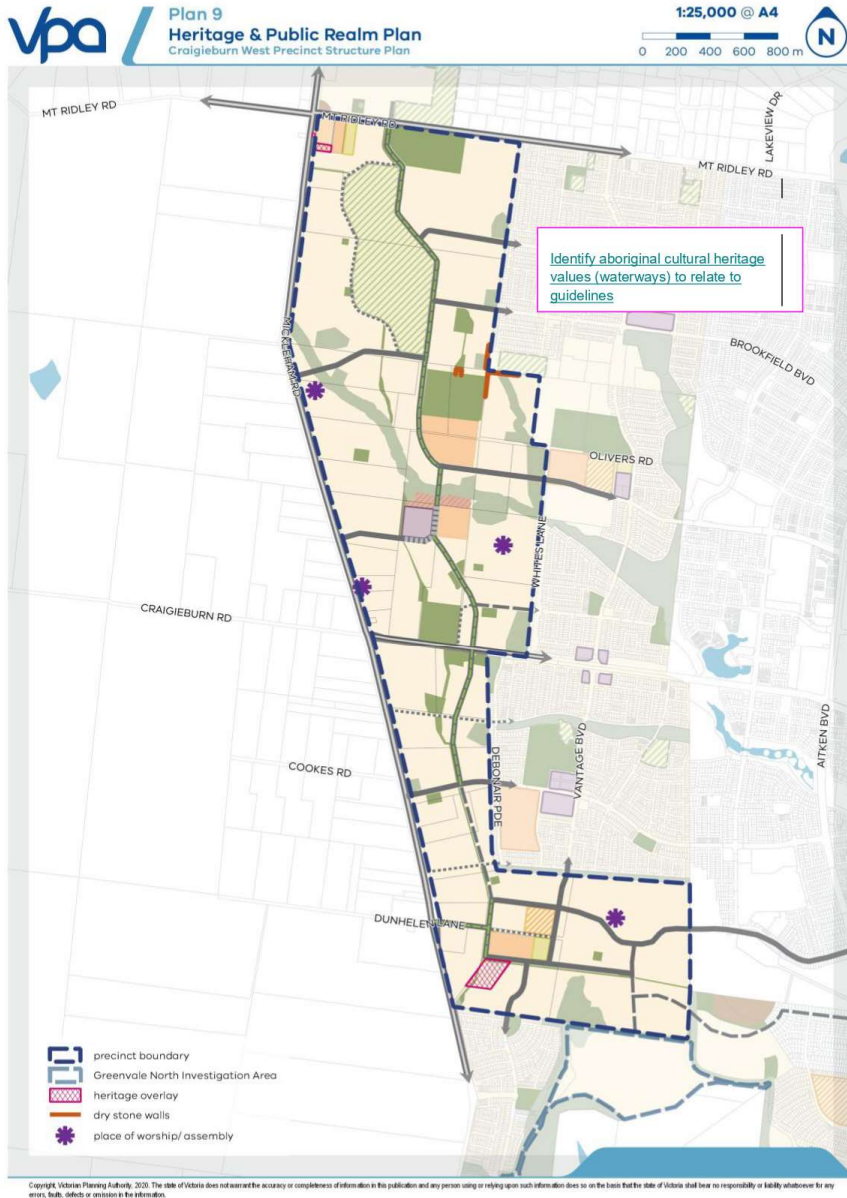
Commented [D22]: Include: consultation with all relevant landowners should be undertaken as part of the master plan preparation.

Commented [D23]: Current PBP shows LP-14 spanning multiple parcels. HCC proposes shifting LP-14 to be within single property adjoining the green link. If LP-14 remains in its current location G46 should become a requirement

Table 5: Credited open space delivery guide

PARK ID	AREA	TYPE	LOCATIONAL ATTRIBUTES	RESPONSIBILITY
ACTIVE OPEN SPACE				
SR-01	9.5	Local Sports Reserve	Located in the north of the precinct to serve the Lindum Vale Catchment and accessible via the connector street network.	Hume City Council
SR-02	9.5	Local Sports Reserve	Located mid-section, north of the town centre and co-located with the proposed government High School, to serve the immediate catchment, and accessible via the connector street network.	Hume City Council
LOCAL PARKS				
LP-01	0.20	Local Park	Located to retain existing vegetation (refer Plan 10) within a passive open space.	Hume City Council
LP-02	0.30	Local Park	Located to retain existing vegetation (refer Plan 10) within a passive open space adjacent to the Conservation Area	Hume City Council
LP-03	1.58	Local Park	Located to retain existing vegetation (refer Plan 10) within a passive open space adjacent to the east/west connector street.	Hume City Council
LP-04	0.79	Local Park	Located to retain existing vegetation (refer Plan 10) within a passive open space adjacent to the east/west connector street.	Hume City Council
LP-05	0.75	Local Park	Located to provide passive open space central the surrounding residential community.	Hume City Council
LP-06	0.36	Local Park	Located to retain existing vegetation (refer Plan 10) within a passive open space adjacent to the north/south boulevard connector street.	Hume City Council
LP-07	0.30	Local Park	Located to retain existing vegetation (refer Plan 10) within a passive open space.	Hume City Council
LP-08	0.44	Local Park	Located to retain existing vegetation (refer Plan 10) within a passive open space adjacent to the north/south boulevard connector street.	Hume City Council
LP-09	5.49	Local Park	Located to retain existing vegetation (refer Plan 10) within a passive open space north of Craigieburn Road.	Hume City Council
LP-10	0.60	Local Park	Located to retain existing vegetation (refer Plan 10) within a passive open space.	Hume City Council
LP-11	1.17	Local Park	Located to retain existing vegetation (refer Plan 10) within a passive open space adjacent to Craigieburn Road and the north south boulevard connector road intersection.	Hume City Council
LP-12	0.50	Local Park	Located to retain existing vegetation (refer Plan 10) within a passive open space adjacent to Craigieburn Road and the north south boulevard connector road intersection.	Hume City Council

LP-13	1.30	Local Park	Located to retain existing vegetation (refer Plan 10) within a passive open space.	Hume City Council
LP-14	0.60	Local Park	Located to retain existing vegetation (refer Plan 10) within a passive open space.	Hume City Council
LP-15	0.30	Local Park	Located to provide passive open space central the surrounding residential community.	Hume City Council
LP-16	0.35	Local Park	Located to provide passive open space central the surrounding residential community.	Hume City Council
GREEN LINK (LINEAR PARK)				
GL-01	0.28	Linear Park	Located to provide a linear park link into Lindum Vale (north of Mt Ridley Road)	Hume City Council
GL-02	0.34	Linear Park	Located to provide a linear park link towards Mickleham Road.	Hume City Council
GL-03	1.21	Linear Park	Located to retain existing vegetation (refer Plan 10) and link the linear park network from the BCS shared path to the Aitken Creek shared path network.	Hume City Council
GL-04	1.40	Linear Park	Located to retain existing vegetation (refer Plan 10) in a passive open space and link the linear park network from the Aitken Creek shared path network to Local Town Centre and LP-09.	Hume City Council
GL-05	0.54	Linear Park	Located to retain existing vegetation (refer Plan 10) and link the linear park network from the LP-11 to LP-13.	Hume City Council
GL-06	0.73	Linear Park	Located to retain existing vegetation (refer Plan 10) and link the linear park network from LP-13 to the east-west connector (Elevation Boulevard extension).	Hume City Council
GL-07	0.70	Linear Park	Located to provide a linear park link between the east-west connector (Elevation Boulevard extension) and east-west local access street.	Hume City Council
GL-08	0.45	Linear Park	Located to provide a linear park link between the east-west local access street and the Boulevard Connector (Dunellen lane extension).	Hume City Council
GL-09	0.16	Linear Park	Located to provide a linear park link between the Boulevard Connector (Dunellen lane extension) and GL-10 & GL-11.	Hume City Council
GL-10	0.55	Linear Park	Located to retain existing vegetation (refer Plan 10) and link the linear park network from GL-09 & GL-11 towards Mickleham Road.	Hume City Council
GL-11	0.89	Linear Park	Located to retain existing vegetation (refer Plan 10) and provide an east-west linear park link adjacent the connector street network between GL-09 & GL-10.	Hume City Council
GL-12	0.50	Linear Park	Located to provide an east-west linear park link between GL-11 and Mount Aitken to the east.	Hume City Council



3.4.2 Heritage & public realm

REQUIREMENTS	
R26	<p>A sensitive interface to the Dunhelen House & Barn (HO31) must be provided to enhance the heritage significance of the site as identified in the Heritage Overlay in the Hume Planning Scheme. In particular, the road layout, subdivision design and development must:</p> <ul style="list-style-type: none"> • Ensure the heritage site becomes a feature of the precinct. • Provide a buffer between the heritage buildings and new dwellings. • Ensure the building is contained on a parcel that provides an appropriate curtilage to the building. • Maintain convenient access to the site. • Ensure the parcel containing heritage buildings has access to the internal subdivision street network. <p>All to the satisfaction of the responsible authority.</p>
R27	<p>Where a Place of Worship/assembly is proposed to be retained, subdivision and development adjacent to existing and future Places of Worship and Places of Assembly as indicated on Plan 9 must:</p> <ul style="list-style-type: none"> • Ensure the site becomes a feature of the precinct. • Provide a buffer between the Places of Worship & Assembly and new dwellings. • Ensure the building is contained on a parcel that provides an appropriate curtilage to the building. • Maintain convenient access to the site. • Ensure the parcel containing the Places of Worship & Assembly has access to the internal subdivision street network. <p>All to the satisfaction of the responsible authority.</p>

GUIDELINES	
G50	Dunhelen House & Barn (HO31) should be considered for adaptive reuse for public or commercial uses.
G51	Significant landscape features, such as high points, vegetation, open space and waterways, should be used as focal points for view lines along streets.
G52	Subdivision design should incorporate natural and constructed design elements which respond to local heritage, neighbouring land uses and topography to assist in place making and the achievement of a "sense of place".
G53	Aboriginal and historic cultural heritage should be recognised through the design of public places, infrastructure and interpretive installations. Opportunity should be explored through cultural heritage interpretation trails along public path networks in areas of known historic cultural history or areas of Aboriginal cultural heritage sensitivity, in consultation with relevant stakeholders.
G54	Signage or interpretive opportunities should be integrated into the public realm to contribute to the knowledge and understanding of the local area's Aboriginal cultural and historic cultural history. Integration opportunities should be provided in consultation with the Wurundjeri Aboriginal Corporation, aligning with the Aboriginal Cultural Values Assessment undertaken for the Craigieburn West PSP.
G55	Subdivision design should respond sensitively to the visual setting and character of heritage places.
G56	<p>Dry stone walls, where assessed to be of high value, should be retained, and:</p> <ul style="list-style-type: none"> • Be situated within public open space or a street reserve, to the satisfaction of the responsible authority • Be incorporated into subdivision design to minimise disturbance to the walls (e.g. utilisation of existing openings for vehicle and pedestrian access) • Have a suitable landscape interface to minimise maintenance requirements (e.g. mulch, garden bed or gravel), which does not encourage public access immediately adjacent to the retained walls • Be checked by a suitably qualified professional for works required to preserve the structural integrity of the wall in a manner suitable for the future context.
G57	Any reinstatement or repair of dry-stone walls should be undertaken by a suitably qualified professional and is to be consistent with the construction style of the original wall, with edges around



	wall openings made secure (cemented) to the satisfaction of the responsible authority. Reinstatement is to use stone from (in order of priority): <ul style="list-style-type: none">• The original wall in that location (including fallen stone adjacent to the wall).• A nearby section of the wall approved to be removed.• Any adjacent land containing wall parts which can be recovered.• Any walls approved for removal in the nearby area (including any stone which has been stockpiled by the responsible authority).• A list of suitably qualified professionals can be obtained from the responsible authority and the Dry Stone Walls Association of Australia.
G58	A consistent suite of lighting and furniture should be used across neighbourhoods, appropriate to the type and role of street or public space.

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3.5 Biodiversity, vegetation & landscape character

REQUIREMENTS

R28	Any proposed development or works within BCS Conservation Area 29 must obtain the approval of the Department of Environment, Land, Water and Planning.
R29	All proposed development adjacent to BCS Conservation Area 29 must provide a minimum 20m edge road along all boundaries to the satisfaction of the Department of Environment, Land, Water and Planning.
R30	Where local parks and recreation areas occur adjacent to BCS Conservation Area 29, they must be designed and managed to complement the outcomes required in Section 5 of the BCS relating to Conservation Area No. 29.
R31	Paths located within the BCS Conservation Area 29 must be designed to avoid and minimise disturbance to native vegetation and habitat for matters of national environmental significance and be located in accordance with the BCS Conservation Area Concept Plan to the satisfaction of the Department of Environment, Land, Water and Planning.
R32	Development abutting the BCS Conservation Area 29 must be in accordance with the Conservation Interface Plan to the satisfaction of the Department of Environment, Land, Water and Planning.
R33	Vegetation shown on Plan 10 as Vegetation for Retention must be retained and incorporated into either the open space network or the public realm.

GUIDELINES

G59	Where practicable, existing vegetation should be retained, protected and enhanced to provide habitat and movement corridors for local fauna.
G60	Appropriately managed and sensitively designed community access to conservation and landscape values areas should be provided where practicable, and where protection of the primary conservation and landscape values can be maintained.
G61	The layout and design of waterways, wetlands and retarding basins (including the design of paths, bridges and boardwalks, and the stormwater drainage system) should integrate with biodiversity and natural systems to the satisfaction of Melbourne Water and other relevant responsible authorities.



3.6 Education & community infrastructure

3.6.1 Community facilities & education

REQUIREMENTS	
R34	new e Education facilities must have a minimum of two road frontages (three preferred), with one connector road abutting the school with a road easement wide enough to allow for school bus movement while accommodating on-street parking and two way traffic movement.
R35	Any connector road or access street abutting a community or education facility must be designed to achieve slow vehicle speeds and provide designated pedestrian crossing points in the vicinity of the school site.

Commented [HCC24]: The urban structure doesn't locate connector adjacent to Mickleham PS – this requirement can't be met and a unique requirement may be needed

GUIDELINES	
G62	Education, community facilities and sports reserves should be accessible by active and public transport routes and provide active street frontages.
G63	Subdivision and development should facilitate integration of schools, sports reserves and community facilities where they are co-located and promote: <ul style="list-style-type: none"> Integration with neighbouring facilities to maximise efficiencies through the sharing of car parking and other complementary infrastructure. Out-of-hours use, street activation and permeability. Safe and convenient pedestrian and cyclist shared path access.
G64	Educational, community or civic infrastructure not shown on Plan 11 should be located within or proximate to a town centre, local convenience centre, community hub or council community building, as appropriate.
G65	Emergency services facilities should have access to the arterial road network to maximise coverage and reduce response times.
G66	Public health and justice services should be located within or adjacent to a community hub or town centre and with access to public transport.
G67	Where the responsible authority is satisfied that land shown as a non-government school site is unlikely to be used for a school at ultimate development of the PSP, that land must be used for an alternative purpose that is compatible with the surrounding land uses and the provisions of the applied zone. Justification should be provided in accordance with the VPA's guidance note titled <i>Development of Non-Government School Sites for an Alternative Purpose</i> .
G68	The indicative layout of community facilities and open space as illustrated in Plan 11 may be altered to the satisfaction of the relevant responsible authorities.



3.7 Centre, employment & economic activity

3.7.1 Town centres

Table 6: Craigieburn West town centre hierarchy – internal to precinct

INTERNAL TOWN CENTRE	RETAIL FLOOR SPACE	COMMERCIAL FLOOR SPACE	LOCATION AND USES
Craigieburn West Activity Town Centre – LTC 1	6,000m ²	1000m ²	Located centrally in the Craigieburn West PSP, accessible from Mickleham Road and located north of Craigieburn Road with frontage to the connector road network. Provides a full line supermarket and specialty shops, with the ability to support non-retail local services.

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REQUIREMENTS	
R36	Development of the Local Town Centre (LTC 1) shown on Plan 4(Place Based Plan) must provide the floor space outlined in Table 5, an appropriate design response to the Performance Requirements and Guidelines of Table 6, and the Local Town Centre Design Principles in Appendix 4.3.
Rx x	masterplan
Rx x	Applications involving the development of all Local Town Centres and Local Convenience Centres must demonstrate how the proposed design has appropriately considered and responded to the Design Guidelines in Appendix 4.3 and 4.4, having regard to local context and the functional requirements of the proposed activity.

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GUIDELINES	
G69	Subdivision layouts should provide for a range of lot configurations to cater for various uses, including small local enterprises.
G70	Additional local convenience centres may be considered subject to demonstrating that they do not compromise the role and function of the nearby Local Town Centres, to the satisfaction of the responsible authority.

Table 7: Craigieburn West Local Town Centre – performance requirements & guidelines

TOWN CENTRE ELEMENT	PERFORMANCE REQUIREMENTS	PERFORMANCE GUIDELINES
Key design elements	<ul style="list-style-type: none"> Must address all relevant elements of the Urban Design Guidelines of Victoria. Must integrate the town centre core with the surrounding neighbourhoods, public transport and community facilities. Most provide a centralised town square with an open frontage to a connector road to be provided that will act as forecourt to the community facility and focal point for surrounding retail, commercial and hospitality uses. The main connector streets to be designed to include dense canopy shade tree provision, outdoor dining and pedestrian activity and on-street parking. 	<ul style="list-style-type: none"> Should respond to the surrounding site features, including the wetlands, waterways, open space areas, surrounding hilltops, and other points of interest to create views and connections and the stories of the place. Should provide a neighbourhood with a pedestrian and active transport priority, with a focus on creating a resilient and accessible 20-minute neighbourhood. Mixed-use precincts should provide retail and/or office at ground level, and office, commercial and residential above ground level. Minimise barriers to pedestrian and bicycle access to the centre, notably across the north south connector road and loading and car parking areas. Development blocks should be based on a permeable layout to enable flexibility to suit a variety of land uses and allow viable short-term development as well as efficient long-term evolution. Potential Regional Skate/Play space to be located in or adjacent to the linear reserve.
Retail core (Local Town Centre)	<ul style="list-style-type: none"> Must provide an area of 3 net developable hectares for the provision of the Local Town Centre. Must provide active frontages that address connector streets and boulevard connectors as a main street frontage. Must be accessible via a connector street from Mickleham Road. Must provide for the strong integration of the centre with the surrounding residential and education facilities, with a high level of surveillance along the primary streets for pedestrian access to the centre. 	<ul style="list-style-type: none"> Should be located adjacent to and integrate with the linear park and utilise the active transport network. Pedestrian and cyclist focused shared zone to be delivered on north-south connector street and the linear park, to integrate the town centre core with the surrounding neighbourhoods, public transport and community facilities. Specialty retail and mixed use to sleeve the supermarket and other anchor retail. Locate buildings which achieve high levels of articulation along the connector streets, including (as appropriate) clear glazing and regular entrances, an appropriate range of building material/colour palette themes and architectural design treatments (including opportunities for signage integration into building design). Development on the south side of the east-west connector should provide capacity for ground floor specialist suites (office, medical services, etc) and independent retail and hospitality tenancies. Floor to ceiling heights should allow for adaptive use with a minimum of 3.6m on ground floor.
Mixed use	<ul style="list-style-type: none"> Must provide a minimum mixed-use area of 10 net developable hectares. 	<ul style="list-style-type: none"> Provide a transition between the retail and commercial core and the residential areas. Development on the south side of the east-west connector should provide capacity for ground floor specialist suites (office, medical services, etc) and independent retail and hospitality tenancies. Floor to ceiling heights

Commented [HCC25]: Support guideline. Update PBP to indicate mixed use precinct on the south side of the e-w connector adjacent to Town Centre

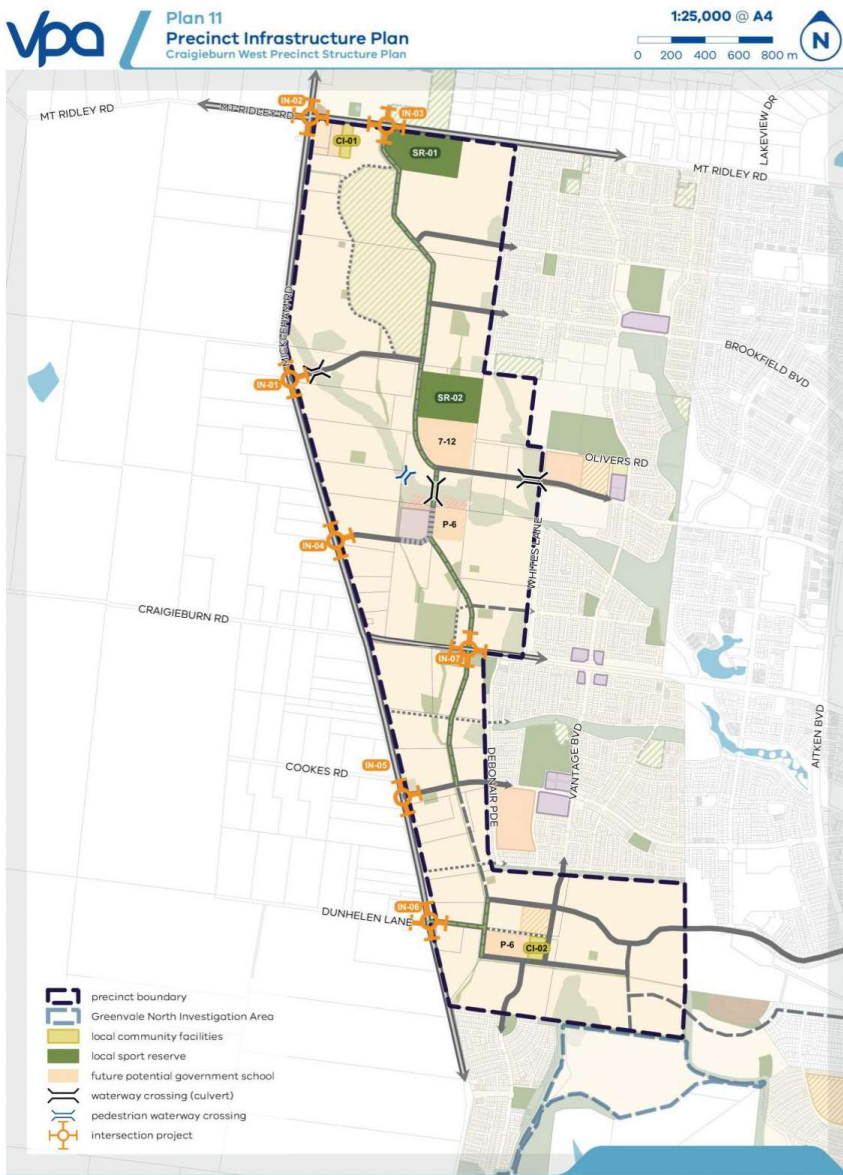
Commented [HCC26]: Move into guidelines for Mixed use T element

Commented [HCC27]: Area to be based on mixed area shown on PBP and area south of town centre between linear park and n-s connector

Victorian Planning Authority



should allow for adaptive use with a minimum
of 3.6m on ground floor.



3.8 Precinct infrastructure delivery

3.8.1 Development staging

REQUIREMENTS

- F** Development staging must provide for the timely provision and delivery of:
- Boulevard connector streets, connector streets, connector street waterway crossings and connector street intersections with arterial roads.
 - Street links between properties, constructed to the property boundary.
 - On- and off-road pedestrian and bicycle network paths, including the linear park.
 - Essential infrastructure.
 - Land for community infrastructure, sports fields and local open space.

GUIDELINES

G71	The staging of development should provide for the early delivery of sports fields, community facilities, local parks and playgrounds within each neighbourhood and may be delivered in stages in consultation with the responsible authority.
G72	<p>Staging of development should be determined largely by the development proposals on land within the precinct and the availability of infrastructure services. Development applications should demonstrate how the development will:</p> <ul style="list-style-type: none">• Integrate with adjoining developments, including the timely provision of roads and path connections, to a practical extent.• Provide open space and amenity to new residents in the early stages of the development, where relevant.• Provide for continuous sealed road access to each new allotment constructed to an urban standard.• Deliver any necessary trunk service extensions, including confirmation of the agreed approach and timing by the relevant service provider.• Avoid and minimise impacts to BCS conservation areas with regard to the location of essential and other services.
G73	<p>Infrastructure projects identified in the Plan 11 should be delivered as per the timing priority identified in the timing column of Appendix 4.1.</p> <p>Where infrastructure is proposed to be delivered outside or ahead of the sequence identified in Appendix 4.1, the onus is on the developer to fund the infrastructure works as 'Works In Kind'.</p> <p>Note: Project delivery timing outlined in Appendix 4.1 is indicative and subject to periodic review by the relevant responsible authority.</p>
G74	<p>Development staging should have regard to:</p> <ul style="list-style-type: none">• Proximity to existing or proposed development fronts or serviced land.• Proximity to significant existing public transport infrastructure or public transport service.• Proximity to existing or committed community infrastructure, such as schools.• Proximity to new or existing arterial or connector road infrastructure.• Its role in facilitating delivery of the above infrastructure. <p>Staging that meets alternative criteria to the above may be considered by the responsible authority where an applicant satisfactorily demonstrates that development will not be isolated from basic and essential infrastructure and services.</p>

3.8.2 Subdivision works

REQUIREMENTS	
R38	<p>Subdivision of land within the PSP must provide and meet the cost for all local infrastructure, other than that provided for within the Craigieburn West ICP. This includes (but is not limited to):</p> <ul style="list-style-type: none"> • Boulevard connector streets, connector streets and local streets. • Local bus stop infrastructure (where locations have been agreed in writing by Head, Department of Transport). • Landscaping, including canopy tree planting, of all existing and future roads and local streets. • Intersection works and traffic management measures along arterial roads, connector streets, and local streets. • Council approved fencing and landscaping (where required) along arterial roads and reserves. • Shared pedestrian and bicycle paths along local arterial roads, connector roads, utilities easements, local streets, waterways and within local parks including bridges, intersections, and barrier crossing points. • Bicycle parking. • Appropriately scaled lighting along all roads, major shared and pedestrian paths, and traversing public open space. • Basic improvements to local parks and open space (refer to open space delivery below). • Local drainage system. • Construction of culverts for waterway crossings of boulevard connector streets, connector streets and local streets. • Local street or pedestrian path crossings of waterways unless outlined as the responsibility of another agency in the Precinct Infrastructure Plan. • Infrastructure as required by utility service providers including water, sewerage, drainage (except where the item is funded through a Development Services Scheme), electricity, gas, and telecommunications. • Construction of shared paths along waterways and open space. • Remediation and / or reconstruction of dry-stone walls, where required.
R39	<p>All public open space including the linear park/green link (where not otherwise provided via the Craigieburn West ICP) must be finished to a standard that satisfies the requirements of the responsible authority prior to the transfer of the public open space, including but not limited to:</p> <ul style="list-style-type: none"> • Removal of all existing and disused structures, foundations, pipelines, stockpiles, and any soil contamination. • Basic levelling including the supply and spread of minimum 75mm topsoil and subsoil if required on the proposed areas of open space to provide a stable free draining surface. • Clearing of rubbish, weeds and rocks, levelled, topsoiled and grassed with warm climate grass (unless conservation reserve requirements dictate otherwise). • Provision of water tapping, potable and recycled water connection points. • Sewer, gas and electricity connection points must also be provided to land identified as sports reserve. • Planting of trees and shrubs (with drought tolerant species). • Adequate protection of existing trees that are to be retained including exclusion zones as appropriate. • Vehicular exclusion devices (fence, bollards, or other suitable method). • Maintenance access points. • Construction of minimum 1.5 metre wide pedestrian paths around the perimeter of the reserve, connecting and linking into any other surrounding paths or points of interest, except where shown as a shared paths on Plan 9. • Installation of park furniture including seating, drinking fountains, shelters, tables, local scale playgrounds and other local scale play elements such as half basketball courts and hit-up walls, soccer goals, rubbish bins and appropriate paving to support these facilities, consistent with the type of public open space listed in the open space delivery guide at Table 54.
R40	<p>Sports reserves must be vested in the relevant authority in the following condition:</p> <ul style="list-style-type: none"> • Free from surface and protruding rocks and structures • Reasonably graded and/or top soiled to create a safe and regular surface, with a maximum 1:6 gradient • Bare, patchy and newly graded areas seeded, top-dressed with drought resistant grass.

	Consistent with the Craigieburn West ICP, where these works are not considered to be temporary, works are eligible for a works-in-kind credit against an ICP obligation. Works associated with adjacent road construction, such as earthworks for a road embankment, are not eligible for works-in-kind credit.
R41	Where an inter-parcel connection is intended or indicated in the PSP, streets must be constructed to property boundaries at the relevant stage of development required or approved by the responsible authority. Provision should be made for temporary vehicle turning until the inter-parcel connection is delivered.

4 APPENDICES

4.1 Precinct infrastructure table

CATEGORY	ICP REF. NO.	TITLE	DESCRIPTION	LEAD AGENCY	COMPONENT INCLUDED IN ICP			TIMING	APPORTIONMENT
					ULTIMATE LAND	INTERIM CONSTRUCTION	ULTIMATE CONSTRUCTION		
INTERSECTION PROJECTS									
Intersection	IN-01	Mickleham Road and east-west Connector Street	Construction of signalised T intersection.	Hume City Council	Yes	Yes	No	S-M	100%
Intersection	IN-02	Mickleham Road and Mount Ridley Road	Construction of signalised 4-way intersection.	Hume City Council	Yes	Yes	No	M-L	50%
Intersection	IN-03	Mount Ridley and north-south Boulevard Connector	Construction of signalised 4-way intersection.	Hume City Council	Yes	Yes	No	S	25%
Intersection	IN-04	Mickleham Road and east-west Connector Street	Construction of a signalised T intersection.	Hume City Council	Yes	Yes	No	S-M	100%
Intersection	IN-05	Mickleham Road and east-west Connector Street (extension of Elevation Blvd).	Construction of a signalised T intersection.	Hume City Council	Yes	Yes	No	S-M	100%
Intersection	IN-06	Mickleham Road and east-west Boulevard Connector Street (extension of Dunhelen Lane)	Construction of signalised 4-way intersection.	Hume City Council	Yes	Yes	No	S-M	100%
Intersection	IN-07	Craigieburn Road and north-south Boulevard Connector	Construction of signalised 4-way intersection.	Hume City Council	Yes	Yes	No	S-M	100%

BRIDGE PROJECTS									
Bridge	BR-01	Connector Street road waterway crossing Aitken Creek waterway.	Construction of a single carriageway crossing of Aitken Creek	Melbourne Water	No	No	No	S	n/a
Bridge	BR-02	Connector Street road waterway crossing Aitken Creek waterway.	Construction of a single carriageway crossing of Aitken Creek	Melbourne Water	No	No	No	M	n/a
Bridge	BR-03	Connector Street road Bridge/culvert across Aitken Creek waterway.	Construction of a single carriageway crossing of Aitken Creek	Melbourne Water	No	No	No	S-M	n/a
COMMUNITY BUILDING & EDUCATION PROJECTS									
Community Infrastructure	CI-01	Northern Level 2 Community Facility	Purchase of land and construction of a Community Centre collocated with Mickleham Primary School expansion.	Hume City Council	Yes	N/A	Yes	M-L	40%
Community Infrastructure	CI-02	Southern Level 2 Community Facility	Purchase of land and construction of a southern government primary school.	Hume City Council	Yes	N/A	Yes	M	100%
School	N/A	Government year P-6 (Mickleham Primary School expansion)	Purchase of additional land and construction of government year P-6 school (Mickleham Primary School)	Dept. of Education and Training	No	No	No	S-M	n/a
School	N/A	Government year P-6	Purchase of land and construction of government year P-6 school collocated local town centre.	Dept. of Education and Training	No	No	No	M-L	n/a
School	N/A	Government year 7-12 school	Purchase of land and construction of government year 7-12 school collocated with SR-02.	Dept. of Education and Training	No	No	No	M-L	n/a
School	N/A	Government year P-6 school	Purchase of land and construction of government year P-6 school collocated with community facilities.	Dept. of Education and Training	No	No	No	M-L	n/a
School	N/A	Non-Government year P-6 school	Purchase of land and construction of non-government year P-6 school collocated with government school and community facilities.	Private Education Provider	No	No	No	M-L	n/a

OPEN SPACE & CONSERVATION AREAS										
Sporting Reserve	SR-01	Sports fields	Purchase of land and construction of sports fields, hard courts and multipurpose pavilion.	Hume City Council	Yes	N/A	Yes	M-L	50%	
Sporting Reserve	SR-02	Sports fields	Purchase of land and construction of sports fields, hard courts and multipurpose pavilion	Hume City Council	Yes	N/A	Yes	M-L	100%	
Local Park	LP-01	Passive Open Space	Provision of land for a local park	Hume City Council	Yes	No	No	S-M	100%	
Local Park	LP-02	Passive Open Space	Provision of land for a local park	Hume City Council	Yes	No	No	S-M	100%	
Local Park	LP-03	Passive Open Space	Provision of land for a local park	Hume City Council	Yes	No	No	S-M	100%	
Local Park	LP-04	Passive Open Space	Provision of land for a local park	Hume City Council	Yes	No	No	S-M	100%	
Local Park	LP-05	Passive Open Space	Provision of land for a local park	Hume City Council	Yes	No	No	S-M	100%	
Local Park	LP-06	Passive Open Space	Provision of land for a local park	Hume City Council	Yes	No	No	S-M	100%	
Local Park	LP-07	Passive Open Space	Provision of land for a local park	Hume City Council	Yes	No	No	S-M	100%	
Local Park	LP-08	Passive Open Space	Provision of land for a local park	Hume City Council	Yes	No	No	S-M	100%	
Local Park	LP-09	Passive Linear Open Space	Provision of land for a local park	Hume City Council	Yes	No	No	S-M	100%	
Local Park	LP-10	Passive Open Space	Provision of land for a local park	Hume City Council	Yes	No	No	S-M	100%	
Local Park	LP-11	Passive Open Space	Provision of land for a local park	Hume City Council	Yes	No	No	S-M	100%	
Local Park	LP-12	Passive Linear Open Space	Provision of land for a local park	Hume City Council	Yes	No	No	S-M	100%	
Local Park	LP-13	Passive Open Space	Provision of land for a local park	Hume City Council	Yes	No	No	S-M	100%	
Local Park	LP-14	Passive Open Space	Provision of land for a local park	Hume City Council	Yes	No	No	S-M	100%	
Local Park	LP-15	Passive Open Space	Provision of land for a local park	Hume City Council	Yes	No	No	S-M	100%	

Local Park	LP-16	Passive Open Space	Provision of land for a local park	Hume City Council	Yes	No	No	S-M	100%
Local Park (Green Link)	GL-01	Linear Open Space	Provision of land for a linear local park	Hume City Council	Yes	No	No	S-M	100%
Local Park (Green Link)	GL-02	Linear Open Space	Provision of land for a linear local park	Hume City Council	Yes	No	No	S-M	100%
Local Park (Green Link)	GL-03	Linear Open Space	Provision of land for a linear local park	Hume City Council	Yes	No	No	S-M	100%
Local Park (Green Link)	GL-04	Linear Open Space	Provision of land for a linear local park	Hume City Council	Yes	No	No	S-M	100%
Local Park (Green Link)	GL-05	Linear Open Space	Provision of land for a linear local park	Hume City Council	Yes	No	No	S-M	100%
Local Park (Green Link)	GL-06	Linear Open Space	Provision of land for a linear local park	Hume City Council	Yes	No	No	S-M	100%
Local Park (Green Link)	GL-07	Linear Open Space	Provision of land for a linear local park	Hume City Council	Yes	No	No	S-M	100%
Local Park (Green Link)	GL-08	Linear Open Space	Provision of land for a linear local park	Hume City Council	Yes	No	No	S-M	100%
Local Park (Green Link)	GL-09	Linear Open Space	Provision of land for a linear local park	Hume City Council	Yes	No	No	S-M	100%
Local Park (Green Link)	GL-10	Linear Open Space	Provision of land for a linear local park	Hume City Council	Yes	No	No	S-M	100%
Local Park (Green Link)	GL-11	Linear Open Space	Provision of land for a linear local park	Hume City Council	Yes	No	No	S-M	100%
Local Park (Green Link)	GL-12	Linear Open Space	Provision of land for a linear local park	Hume City Council	Yes	No	No	S-M	100%

4.2 Parcel specific land use budget table

PSP property ID	Total area (ha)	Transport		Community & Education				Open Space			Total net developable area (hectares)	Net developable area % of property				
		Arterial road		Other transport			Service open space		Credited open space							
		Arterial road – existing road reserve	Arterial road – public acquisition overlay	Arterial road – new / widening / intersection flaring (ICP land)	Non-arterial road – related existing road reserve	Non-arterial road – new / widening / intersection flaring (ICP land)	Existing government school	Government school	Potential non-government school	ICP community facilities			Conservation reserve	Waterway & drainage reserve	Local sports reserve (ICP land)	Local network park (ICP land)
1	1.63	–	–	–	–	–	1.63	–	–	–	–	–	–	–	0.00	0.00%
2	0.42	–	–	–	–	–	0.42	–	–	–	–	–	–	–	0.00	0.00%
3	1.36	–	–	–	–	–	–	1.36	–	–	–	–	–	–	0.00	0.00%
4	76.80	–	–	0.38	–	–	–	0.10	1.20	14.14	1.36	9.50	0.33	48.79	64.83%	
5	2.24	–	–	–	–	–	–	–	–	–	–	–	0.29	1.96	87.29%	
6	79.44	–	–	–	–	–	–	–	–	23.56	4.76	2.83	2.83	48.30	60.79%	
7	14.80	–	–	0.12	–	–	–	–	–	–	–	2.39	–	0.44	11.86	80.10%
8	11.34	–	–	–	–	–	–	–	–	–	–	2.64	–	0.54	8.16	71.93%

9	10.28	-	-	-	-	-	-	-	1.55	-	-	-	-	4.06	-	-	45.45%
10	8.25	-	-	-	-	-	-	-	1.81	-	-	-	-	5.44	0.27	0.73	8.80%
11	12.30	-	-	-	-	-	-	-	-	-	-	-	2.74	-	-	9.56	77.71%
12	13.34	-	-	-	-	-	-	-	-	-	-	-	1.74	-	0.75	10.84	81.28%
13	8.85	-	-	-	-	-	-	-	-	-	-	-	0.17	-	-	8.67	98.05%
14	16.97	-	-	-	-	-	-	-	4.70	-	-	-	3.99	-	0.06	8.23	48.49%
15	8.23	-	-	-	-	-	-	-	-	-	-	-	2.25	-	-	5.98	72.65%
16	8.23	-	-	-	-	-	-	-	-	-	-	-	4.65	-	-	3.68	44.75%
17	11.25	-	-	0.12	-	-	-	-	-	-	-	-	-	-	-	11.13	98.95%
18	4.61	-	-	-	-	-	-	-	-	-	-	-	-	-	-	4.61	100.00%
19	2.04	-	-	-	-	-	-	-	-	-	-	-	-	-	-	2.04	100.00%
20	2.02	-	-	-	-	-	-	-	-	-	-	-	-	-	-	2.02	100.00%
21	0.14	-	-	-	-	-	-	-	-	-	-	-	-	-	-	0.14	100.00%
22	1.68	-	0.14	-	-	-	-	-	-	-	-	-	-	-	-	1.55	91.91%
23	16.40	-	0.23	-	-	-	-	-	-	-	-	-	0.004	-	1.07	15.09	92.05%
24	16.44	-	0.02	-	-	-	-	-	3.50	-	-	-	-	-	6.56	6.36	38.69%
25	16.73	-	-	-	-	-	-	-	-	-	-	-	1.40	-	0.30	15.03	89.86%
26	0.41	-	-	-	-	-	-	-	-	-	-	-	-	-	-	0.41	100.00%
27	16.77	-	-	-	-	-	0.39	-	-	-	-	-	2.92	-	0.60	12.86	76.69%
28	12.76	-	0.74	-	-	-	-	-	-	-	-	-	-	-	0.75	11.28	88.37%
29	12.19	-	0.85	-	-	-	0.33	-	-	-	-	-	-	-	2.70	8.51	69.81%
30	11.08	-	-	-	-	-	-	-	-	-	-	-	1.43	-	0.59	81.78%	
31	13.98	-	-	0.12	-	-	-	-	-	-	-	-	0.52	-	0.63	12.72	90.98%
32	3.24	-	-	-	-	-	-	-	-	-	-	-	-	-	0.24	3.01	92.70%
33	4.85	-	-	-	-	-	-	-	-	-	-	-	-	-	0.36	4.49	92.55%
34	11.73	-	-	-	-	-	-	-	-	-	-	-	-	-	0.34	11.39	97.11%
35	14.96	-	-	-	-	-	-	-	-	-	-	-	-	-	0.54	14.40	96.25%

36		10.63	-	-	-	-	0.03	-	-	2.50	-	-	-	-	0.13	7.97	74.99%
37		2.65	-	0.38	-	-	0.04	-	-	-	-	-	-	-	-	2.23	84.33%
38		13.31	-	0.01	-	-	-	-	3.50	-	1.20	-	-	-	0.26	8.34	62.66%
39		15.76	-	-	-	-	-	-	-	-	-	0.22	-	-	0.91	14.63	92.83%
40		68.45	-	-	-	-	-	-	-	-	-	-	5.26	-	1.28	61.91	90.45%
SUB-TOTAL		558.55	0.00	2.16	0.73	0.00	0.79	2.04	16.52	2.50	2.40	37.70	38.33	19.00	22.78	413.60	74.05%
T																	
R1 (Olivers Rd)		1.20	-	-	-	-	-	-	0.33	-	-	-	0.22	-	-	0.66	54.42%
R2 (Craigieburn Rd)		1.87	1.87	-	-	-	-	-	-	-	-	-	-	-	-	0.00	0.00%
R3 (Dunthelen Lane)		0.72	-	-	-	0.66	-	-	-	-	-	-	-	-	-	0.06	7.97%
SUB-TOTAL		3.79	1.87	0.00	0.00	0.66	0.00	0.00	0.33	0.00	0.00	0.00	0.22	0.00	0.00	0.71	18.80%
TOTALS PSP Craigieburn West		562.34	1.87	2.16	0.73	0.66	0.79	2.04	16.85	2.50	2.40	37.70	38.55	19.00	22.78	414.31	73.68%

4.3 Craigieburn West Local Town Centre - design principles

LOCAL TOWN CENTRES	
<p>Principle 1</p> <p>Provide every neighbourhood with a viable Local Town Centre as a focus of the community with a fine grain, closely spaced distribution pattern.</p>	<ul style="list-style-type: none"> • Deliver a fine grain distribution pattern of highly accessible Local Town Centres generally on a scale of one Local Town Centre for every neighbourhood of 8,000 to 10,000 people. • Locate Local Town Centres with a distribution pattern of around one Local Town Centre for every square mile (approx. 2.6km²) of residential development. • Deliver a network of economically viable Local Town Centres including a supermarket and supporting competitive local shopping business, medical, leisure, recreation and community needs while allowing opportunities for local specialisation.
<p>Principle 2</p> <p>Locate Local Town Centres on a connector street intersection with access to an arterial road and transit stop.</p>	<ul style="list-style-type: none"> • Locate the Local Town Centre on an arterial/connector intersection and ensure that the Local Town Centre is central to the residential catchment that it services while optimising opportunities for passing trade. • Locate the Local Town Centre with future railway stations or other forms of transit stops to benefit the Local Town Centre, to offer convenience for public transport passengers, and to minimise walking distance between transit stops and the town centre core. • Other Local Town Centre locations may be considered where the location results in the Local Town Centre being central to the residential catchment that it serves and/or the location incorporates natural or cultural landscape features such as rivers and creeks, tree rows, topographic features or other heritage structures which assist in creating a sense of place.
<p>Principle 3</p> <p>Locate Local Town Centres in an attractive setting so that most people live within a walkable catchment of a Local Town Centre and relate to the centre as the focus of the neighbourhood.</p>	<ul style="list-style-type: none"> • Ensure that 80-90% of households are within a 1km walkable catchment of a local or higher order Town Centre. • Locate Local Town Centres in attractive settings and incorporate natural or cultural landscape features such as creeks and waterways, linear open space, pedestrian and cycle links and areas of high aesthetic value. • The design of the Local Town Centre should respect existing views and vistas to and from the Local Town Centre location.
<p>Principle 4</p> <p>Provide a full range of local community and other facilities including a supermarket, shops, medical and recreation uses.</p>	<ul style="list-style-type: none"> • Land uses should be located generally in accordance with the locations and general land use terms identified on the Local Town Centre Concept Plan. • Promote designs which offer a high degree of community interaction and provision of a vibrant and viable mix of retail, recreation and community uses. • Encourage clustering of uses in precincts such as a 'medical precinct' where similar or synergistic uses should be sited together to promote stronger trading patterns.

	<ul style="list-style-type: none"> • Encourage smaller grain individual tenancies and land ownership patterns to attract participation of local business investment and encourage opportunities for greater diversity. • Incorporate flexible floor spaces (including floor to ceiling heights) into building design to enable localised commercial uses to locate amongst the activity of the local town centre. • The local town centre should generally be anchored by one full line supermarket and supported by specialty stores unless otherwise noted on the Local Town Centre Concept Plan. • Supermarkets and other commercial or community anchors or secondary anchors within the local town centre should generally be located diagonally opposite one another across the main street and/or town square to promote pedestrian desire lines that maximise movement within the public realm. • A small access mall that address a supermarket/other 'large box uses' may be considered as part of the overall design. Such access malls may have a limited number of internalised shops. The primary access to the mall should be from the main street and/or the town square. • Active building frontages should address the main- street and town square to maximise exposure to passing trade and promote pedestrian interaction. • Provide retail and/or office at ground level, and office, commercial and residential above ground level in Mixed Use precincts • Locate childcare, medical centres and specialised accommodation (for example, aged care, nursing home, student accommodation, and serviced apartments) within or at the edge of the local town centre to contribute to the centre's activity and the resident's access to services. • Locate car parking areas centrally to development sites and to the rear and or side of street-based retail frontages. • Design car parking areas to accommodate flexible uses and allow for long term development opportunities. • Provide public toilets in safe and accessible locations within the managed area of the property.
<p>Principle 5</p> <p>Focus on a public space as the centre of community life.</p>	<ul style="list-style-type: none"> • Provide a public space which acts as the central meeting place within the local town centre. This space may take the form of a town square, town park, public plaza space, public market place or a similar locally responsive option designed to function as the identifiable 'centre' or 'heart' with a distinctive local character for both the local town centre and the broader residential catchment. • Key uses of the LTC are to be positioned where they front public spaces to ensure it is a dynamic and activated space. • Design flexible and adaptable public spaces so that a range of uses can occur within them at any one time. Such uses may include people accessing daily shopping and business needs as well as social interaction, relaxation, celebrations and temporary uses (such as stalls, exhibitions and markets). • Design the public space so that it is well integrated with pedestrian and cycle links around and through the local town centre so that it acts as a 'gateway' to the activity of the centre.

	<ul style="list-style-type: none"> The main public space or town square should have a minimum area of 500 square metres. Smaller public spaces which are integrated within the built form design, surrounded by active frontages and facilitate high levels of pedestrian movement are also encouraged. Footpath widths within and around the public space as well as along the main street should be sufficient to provide for universal access as well as outdoor dining and smaller gathering spaces.
Principle 6 Integrate local employment and service opportunities in a business-friendly environment.	<ul style="list-style-type: none"> Provide a variety of employment and business opportunities through the provision of a broad mix of land uses and commercial activities. Provide a range of options and locations for office-based businesses. Consider appropriate locations for small office/home office housing which maximise the access and exposure to the activity of the local town centre. Provide services and facilities to support home based and smaller businesses within the local town centre. Consider using these uses to sleeve loading areas and car parks where feasible.
Principle 7 Include a range of medium and high-density housing and other forms of residential uses within and surrounding the local town centre.	<ul style="list-style-type: none"> Provide medium and high-density housing in and around the local town centre for passive surveillance and contributions to the centre's life and amenity. Provide medium and high-density housing in locations of high amenity in and around the local town centre, connected to the activity of the local town centre through strong pedestrian and cycle links. Provide a range of housing types for a cross section of the community (such as retirement living) in and around the local town centre. Design the local town centre to avoid potential land use conflicts between residential and commercial uses by focusing on retail operations on the main street and around the town square and locating residential uses predominantly at the edge and/or on upper levels. Refer to the Small Lot Housing Code for further information about housing requirements for small lots around local town centres.
Principle 8 Design the Local Town Centre to be pedestrian friendly and accessible by all modes including public transport, while enabling private vehicle access.	<ul style="list-style-type: none"> Design the local town centre to provide easy, direct and safe access for pedestrians, cyclists, public transport modes, private vehicles, service and delivery vehicles with priority given to pedestrian movement, amenity, convenience and safety. Provide a permeable street network, walkways and public spaces that provide linkages throughout the centre and designated pedestrian crossing points. Design the main and other streets to comply with the relevant cross sections found within the PSP. A speed environment of 40 kilometres per hour or less should be designed for the length of the main street. Provide public transport infrastructure facilities in convenient locations for commuters. Provide bus stops in accordance with the Public Transport Guidelines for Land Use and Development, to the satisfaction of the Department of Transport. Provide bicycle parking within the street network and public spaces in highly visible locations and close to key destinations.

	<ul style="list-style-type: none"> Design supermarket and other 'large format' buildings so they do not impede on the movement of people around the local town centre. Encourage pedestrian movement along the length of the street and through public spaces by locating key buildings at strategic points/sites. Design buildings so they have a positive relationship with the interface to the public street network and does not impede on the pedestrian movement. Design car parking areas with adequate positioning and lighting to ensure passive surveillance and public. Provide dedicated pedestrian routes and areas of landscaping within off street car park areas. Provide on-street car parking to encourage short stay/convenience uses. Group and limit the number of car park access crossovers. Design heavy vehicle access points to limit the pedestrian and vehicle conflict. Loading and deliveries should be located to the rear and or side of street-based retail frontages. All streets, public spaces and car parks to be lit to Australian standards and with pedestrian (generally white) light. Lighting should be designed to avoid unnecessary spill to the side or above. Secondary access to the supermarket from car parking areas should only be considered where it facilitates convenient trolley access and does not diminish the role of the primary access from the main street and or town square.
<p>Principle 9</p> <p>Create a sense of place with high quality engaging urban design.</p>	<ul style="list-style-type: none"> Design developments to complement and enhance the character of the surrounding area by responding to key visual cues associated with the topography and other natural features of the local town centre location and its surrounds. Minimise amenity and noise impacts resulting from the mix of uses by maintaining appropriate separation and transitional areas between retail and housing activities using open space, road networks and community facilities. Use materials and design elements that contribute to a cohesive and legible character for the local town centre as a whole. Designate sites in prominent locations (such as at key intersections, surrounding public spaces and terminating key view lines and vistas) for significant buildings or landmark structures. Ensure that the design of corner sites, where the main street meets an intersecting and/or arterial road by: <ul style="list-style-type: none"> providing built form that anchors the main street to the intersecting road. This can be achieved through increased building height, scale and articulated frontages incorporating either 2 storey buildings or 2 storey elements (such as awnings and roof lines) providing an active ground floor frontage and active floor space components to the main street frontage providing a consistent covered walkway or veranda for weather protection in the design of building frontages on main pedestrian routes. Align built form with the property boundary to define the street edge.

	<ul style="list-style-type: none"> • Provide visually rich, interesting and well-articulated street interfaces and all visible side or rear facades finished in suitable materials and colours that contribute to the character of the local town centre. • The design and siting of supermarkets and other 'large format retail uses should provide an appropriate response to the entire public domain. This includes but is not limited to car parking areas, predominant routes and street level interfaces. • Design supermarket and secondary anchors with frontages that directly address the main street and/or town square so that the use integrates with and promotes activity within the main street and public spaces/ thoroughfares. • Design supermarkets or large format retail uses with a direct frontage to the main street using clear glazing to allow view lines into the store from the street. (Planning permits for buildings and works should condition against the use of "white washed" or frosted glass windows, excessive window advertising and obtrusive internal shelving or 'false walls' offset from the glazing). • Retail uses along street frontages should generally include access points at regular intervals to encourage activity along the length of the street. • Public spaces should be oriented to capture north sun and protect from prevailing winds and weather • Landscaping of all interface areas should be of a high standard as an important element to complement the built form design. • Urban art should be incorporated into the design of the public realm. • Street furniture should be located in areas that are highly visible and close to or adjoining pedestrian desire lines/gathering spaces and designed to add visual interest to the Local Town Centre. • Wrapping or sleeving of car parking edges with built form, to improve street interface, should be maximised. • Car parking areas should provide for appropriate landscaping with planting of canopy trees and dedicated pedestrian thoroughfares. • Screening of centralised waste collection points should minimise amenity impacts on adjoining areas and users of the centre. • Where service areas are accessible from car parks, they should present a well-designed and secure facade to public areas. • Mechanical plant and service structure roofs should be included within roof lines or otherwise hidden from view.
<p>Principle 10</p> <p>Promote localisation, sustainability and adaptability.</p>	<ul style="list-style-type: none"> • The Local Town Centre should promote the localisation of services which will contribute to a reduction of travel distance to access local services and less dependence on private vehicles. • The Local Town Centre should be designed to be sympathetic to its natural surrounds by: <ul style="list-style-type: none"> • investigating the use of energy efficient design and construction methods for all buildings • including Water Sensitive Urban Design principles such as integrated stormwater retention and reuse (e.g. toilet flushing and landscape irrigation) • promoting safe and direct accessibility and mobility within and to and from the Local Town Centre

- including options for shade and shelter through a combination of landscape and built form treatments
- ensuring buildings are naturally ventilated to reduce the reliance on plant equipment for heating and cooling
- promoting passive solar orientation in the configuration and distribution of built form and public spaces
- grouping waste collection points to maximise opportunities for recycling and reuse
- promoting solar energy for water and space heating, electricity generation and internal and external lighting
- investigating other opportunities for the built form to reduce greenhouse gas emissions associated with the occupation and the ongoing use of buildings.
- Ensure the local town centre and building design has an inbuilt capacity for growth.

4.4 Local Convenience Centre performance criteria

LOCAL CONVENIENCE CENTRES

Principle 1

Local Convenience Centres should facilitate access to goods, services, community facilities and opportunities for social interaction.

- Local Convenience Centres should make a positive contribution to the structure and planned future character of neighbourhoods
- Local Convenience Centres should reinforce neighbourhood legibility and sense of place
- Local Convenience Centres should support the delivery of more diverse and higher density forms of housing.

Principle 2

Local Convenience Centres should provide for the convenience needs of the local community.

- Local Convenience Centres should be of a form, scale and design quality that reinforces their role as focal points for the community.
- Commercial activities within Local Convenience Centres should be limited to a range and scale that meets the convenience needs of local residents and passers-by.
- Local Convenience Centres should be adaptable to a range of uses and allow activities to change over time.
- Local Convenience Centres should make a positive contribution to the visual quality and interest of streets and other public open spaces.

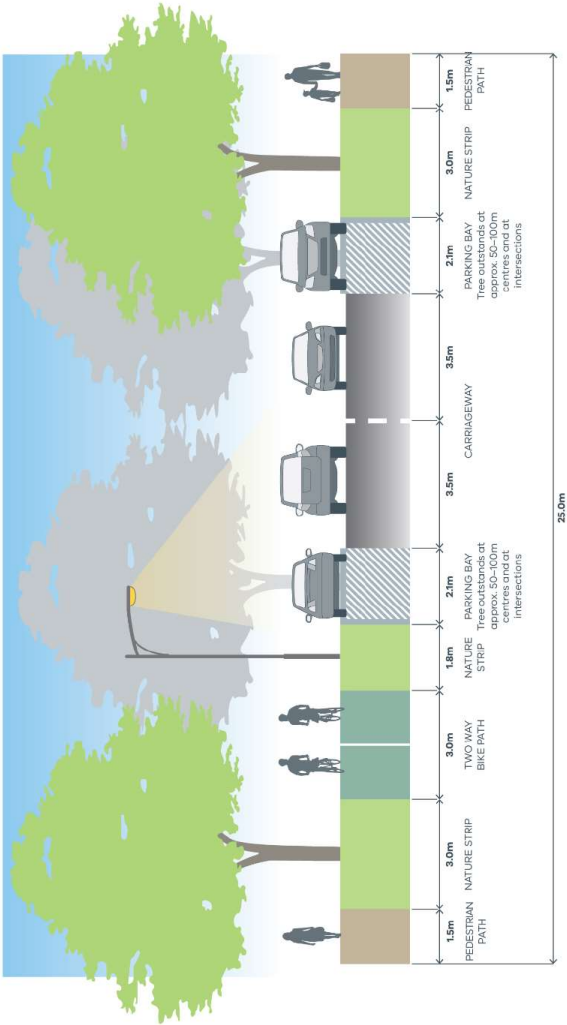
Principle 3

Local Convenience Centres should be safe, accessible environments that provide for a range of transportation choices.

- Local Convenience Centres should make a positive contribution to pedestrian amenity, movement, safety and convenience for people of all ages and abilities.
- At grade parking and vehicle access should be located and designed in such a manner as to avoid or mitigate adverse effects on pedestrian safety and amenity, and the streetscape.
- Bicycle parking should be provided within the street network and public spaces in highly visible locations and close to pedestrian desire lines and key destinations.
- Bus stops should be designed in accordance with the Public Transport Guidelines for Land Use and Development, to the satisfaction of the Department of Transport.

4.5 Street cross sections



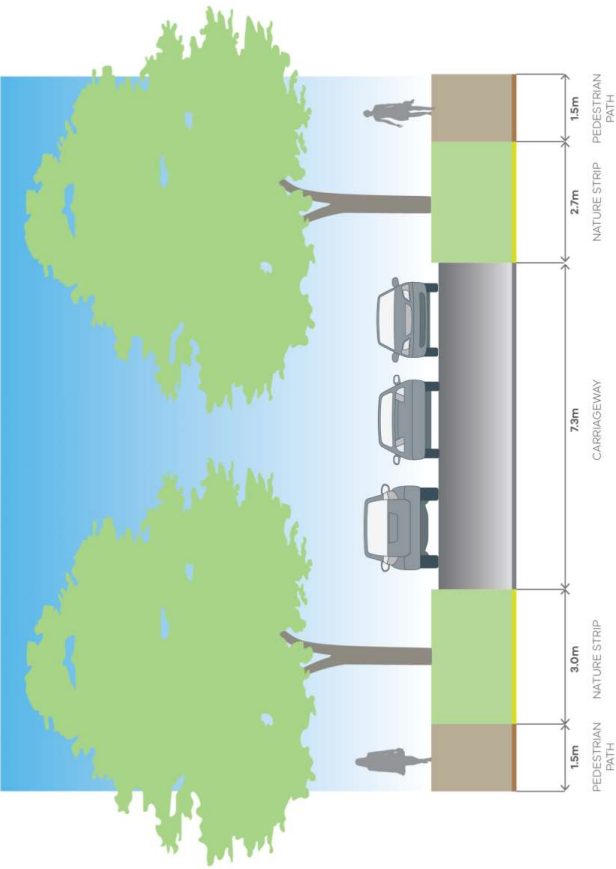


NOTES:

- Minimum street tree mature height: 15 metres.
- All kerbs are to be B2 Barrier Kerb.
- Where roads about school drop-off zones and thoroughfares, grassed nature strip should be replaced with pavement. Canopy tree planting must be incorporated into any additional pavement.
- Verges widths may be reduced where roads about open space with the consent of the responsible authority.
- Variations to indicative cross-section may include water sensitive urban design (WSUD) outcome. These could include but are not limited to bioretention basins, permeable pavements, and vegetated swales. Such variations must be to the satisfaction of the responsible authority.

Connector Street (25.0m)
Residential





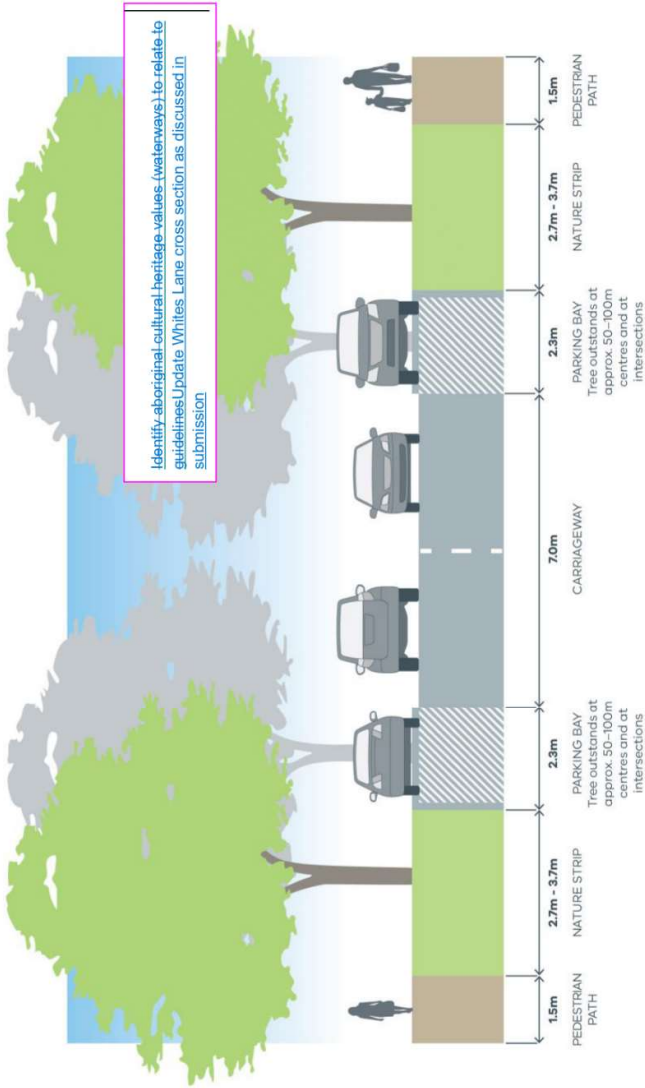
NOTES:

- Minimum street tree mature height 12 metres
- All kerbs are to be D2 Barrier Kerb as per Figure 008 in Engineering Design and Construction Manual for Subdivision in Growth Areas (April 2011)
- Verge widths may be reduced where roads about open space with the consent of the responsible authority.

Local Access Street Level 1 (16.0m)

vpa
Victorian Planning Authority



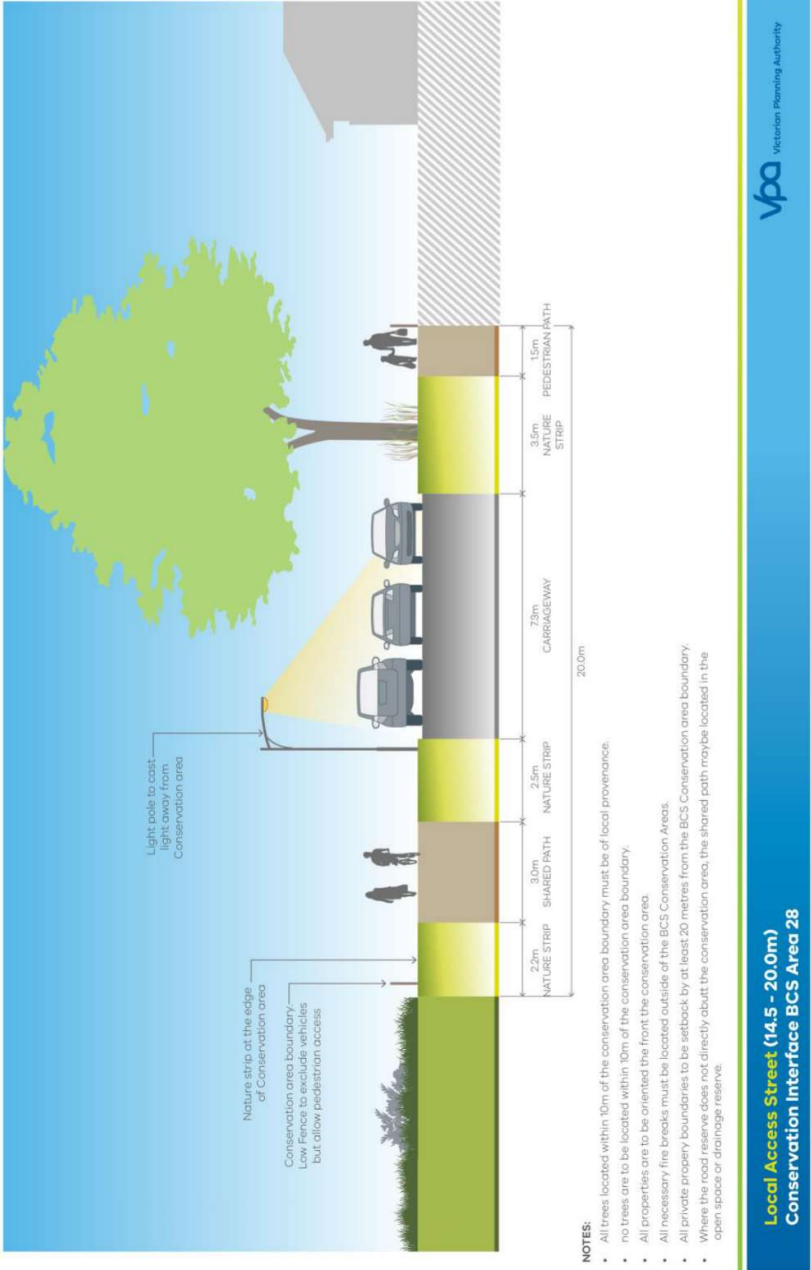


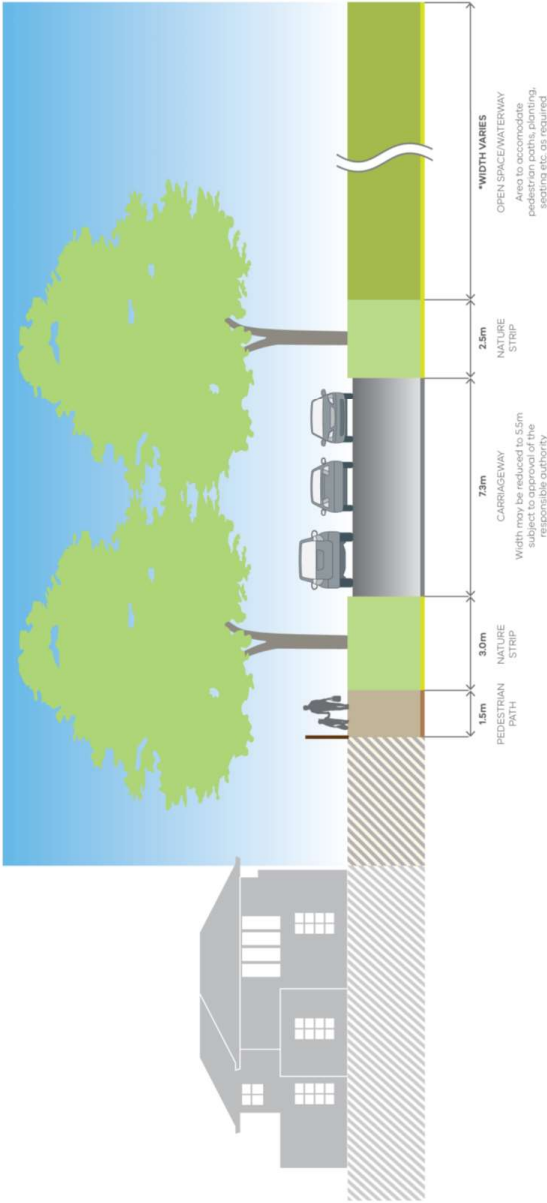
NOTES:

- Minimum street tree mature height 12 metres
- All kerbs are to be B2 Barrier Kerb
- Verges widths may be reduced where roads about open space with the consent of the responsible authority.

Local Access Street Level 2 (20 - 22m) White Lane
Residential

vpa
Victorian Planning Authority





NOTES:

- Where active interfaces to waterways are not provided, waterway corridor widths will be increased in order to ensure maintenance access, to the satisfaction of Melbourne Water
- Where a 2.5m nature strip adjacent to the open space/waterway is not needed for tree planting and/or provision of services, the width may be reduced, to the satisfaction of the Responsible Authority
- 3m wide nature strip may be reduced to the satisfaction of the Responsible Authority and subject to servicing infrastructure.

Waterways & Open Space Interface



4.7 Service placement guidelines

4.7.1 Standard road cross sections

Figures 003 and 004 in the Engineering Design and Construction Manual for Subdivision in Growth Areas (April 2011) outline placement of services for a typical residential street environment. This approach is appropriate for the majority of the 'standard' road cross sections outlined in Appendix C containing grassed nature strips, footpaths and road pavements.

4.7.2 Non-standard road cross sections

To achieve greater diversity of streetscape outcomes in Melbourne's growth areas, which enhances character and amenity of these new urban areas, non-standard road cross sections are required. Non-standard road cross sections will also be necessary to address local needs, such as fully sealed verges for high pedestrian traffic areas in town centres and opposite schools. This PSP contains suggested non-standard 'variation' road cross sections, however other non-standard outcomes are encouraged. For non-standard road cross sections where service placement guidance outlined in Figure 003 and 004 in the Engineering Design and Construction Manual for Subdivision in Growth Areas (April 2011) is not applicable, the following service placement guidelines will apply.

	Under pedestrian pavement	Under nature strips	Directly under trees ¹	Under kerb	Under road pavement	Within allotments	Notes
SEWER	Preferred	Possible	Possible	No	Possible	Possible ³	
POTABLE WATER	Possible ⁴	Preferred	Preferred	No	No	No	Can be placed in combined trench with gas
RECYCLED WATER	Possible ⁴	Preferred	Preferred	No	No	No	
GAS	Possible ⁴	Preferred	Preferred	No	No	No	Can be placed in combined trench with potable water
ELECTRICITY	Preferred ⁴	Possible	Possible	No	No	No	Pits to be placed either fully in footpath or nature strip
FTTH/TELCO	Preferred ⁴	Possible	Possible	No	No	No	Pits to be placed either fully in footpath or nature strip
DRAINAGE	Possible	Possible	Possible	Preferred	Preferred	Possible ³	
TRUNK SERVICES	Possible	Possible	Possible	Possible	Preferred	No	
NOTES	<ol style="list-style-type: none"> 1 Trees are not to be placed directly over property service connections. 2 Placement of services under road pavement is to be considered when service cannot be accommodated elsewhere in road reserve. Placement of services beneath edge of road pavement/parking bays is preferable to within traffic lanes. 3 Where allotment size/frontage width allows adequate room to access and work on a pipe. 4 Where connections to properties are within a pit in the pedestrian pavement/footpath. 						

4.7.3 General principles for service placement

General principles for service placement:

- place gas and water on one side of road, electricity on the opposite side
- place water supply on the high side of road
- place services that need connection to adjacent properties closer to these properties
- place trunk services further away from adjacent properties
- place services that relate to the road carriageway (e.g. drainage, street light electricity supply) closer to the road carriageway Maintain appropriate services clearances and overlap these clearances wherever possible

4.8 Centres External to the Precinct

Table 8: Town Centre hierarchy – external to Craigieburn West Precinct

EXTERNAL TOWN CENTRE	RETAIL FLOOR SPACE	LOCATION AND ANCILLARY USES
Craigieburn Central Major Activity Centre	50,000 m ²	Situated on the north-eastern corner of Craigieburn Road and the Aitken Boulevard, and services the wider Craigieburn area as the Major Activity Centre for the wider region.
Craigieburn R2 Northern 'Neighbourhood' Activity Centre	5,300m ²	Serves the Craigieburn R2 PSP area north of Craigieburn Road, existing residential to the east and part of Craigieburn West PSP in the north. Integrated with a Community Centre and provides for one major, one possible minor supermarket and specialty shops and local services including offices, medical, childcare, banking etc.
Craigieburn R2 Southern 'Neighbourhood' Activity Centre	5,700m ²	Serves the Craigieburn R2 PSP area south of Craigieburn Road, existing development to the east and part of Craigieburn West PSP in the south. Integrated with a Community Centre, provides a supermarket and specialty shops and local services including childcare, medical, offices, dining.
Craigieburn R2 Northern Local Activity Centre	1,000 m ²	Serves a catchment approximately half the size of a Neighbourhood Activity Centre. Provides a small 'convenience' supermarket and specialty retail shops, along with office, banking and other local services, with opportunity for a residential component.
Craigieburn R2 Southern Local Activity Centre	500m ²	Serves a catchment approximately half the size of a Neighbourhood Activity Centre. Provides small shops, local office uses and other local services, with the opportunity for a residential component.

4.9 Employment generation in precinct

Table 9: Anticipated employment creation within the precinct

LAND USE	MEASURE	TOTAL QUANTITY IN PSP	ESTIMATED JOBS
Community facility CI-01	10 jobs/hectare	1.20	12
Community facility CI-02	10 jobs/hectare	1.20	12
All community facilities	10 jobs/hectare	2.4	24
Local Town Centre LTC1	40 jobs/hectare	3.00	120
All town centres	40 jobs/hectare	3.00	120
Mixed-use	40 jobs/hectare	2.16	86
All mixed-use	40 jobs/hectare	2.16	86
Government primary school (including existing Mickleham Primary)	40 jobs/campus	3	120
Government secondary school	90 jobs/campus	1	90
Non- government primary school	30 jobs/campus	1	60
All education facilities	40 jobs/hectare	5	270
Home based business	0.05 jobs/dwelling	8,308	415
TOTAL ESTIMATED			915

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Cocathume

SCHEDULE 12 TO CLAUSE 37.07 URBAN GROWTH ZONE

Shown on the planning scheme map as **UGZ12**.

CRAIGIEBURN WEST PRECINCT STRUCTURE PLAN

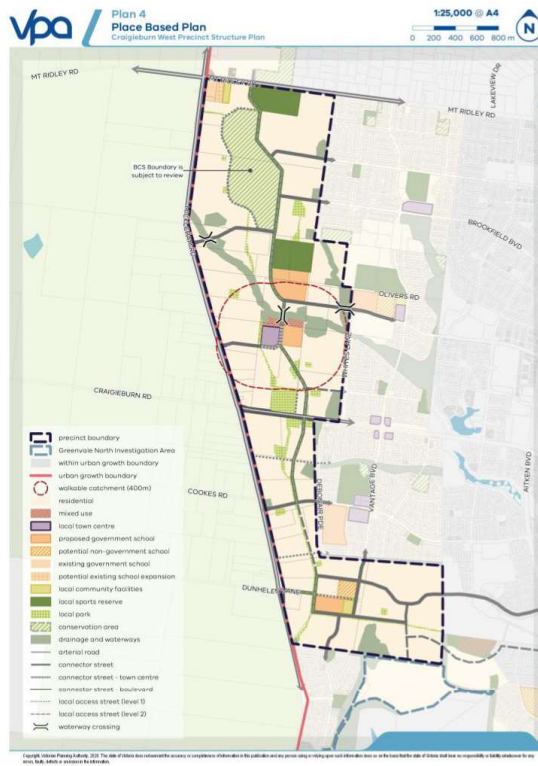
1.0

The Plan

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Proposed
Cocathume

Plan 1 below shows the future urban structure proposed in the *Craigieburn West Precinct Structure Plan, November 2020*. It is a reproduction of Plan 4 in the *Craigieburn West Precinct Structure Plan, November 2020*.

Plan 1 to Schedule 12 to Clause 37.07



HUME PLANNING SCHEME

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Cooxhume

Use and development

2.1

DD/MM/YYYY
Proposed
Cooxhume

The land

The provisions of this schedule apply to the land within the 'precinct boundary' shown on Plan 1 of this schedule and shown as UGZ12 on the planning scheme maps.

Note:

If land shown on Plan 1 is not zoned UGZ12, the provisions of this zone do not apply.

2.2

DD/MM/YYYY
Proposed
Cooxhume

Applied zone provisions

Table 1 allocates the land use/development shown on Plan 1 of this schedule with a corresponding zone from this scheme.

Where the use/development in the left column is carried out or proposed generally in accordance with the incorporated *Craigieburn West Precinct Structure Plan*, the use, construction of a building and construction and carrying out of works provisions of the corresponding zone in the right column apply.

A reference to a planning scheme zone in an applied zone must be read as if it were a reference to an applied zone under this schedule.

For example: The Commercial 2 Zone specifies 'Shop' as a Section 1 Use with the condition, 'The site must adjoin, or have access to, a road in a Road Zone.' In this instance the condition should be read as, 'The site must adjoin, or have access to, a road in a Road Zone or an applied Road Zone in the Urban Growth Zone schedule applying to the land.'

Table 1: Applied zone provisions

Land shown on plan 1 of this schedule Local town centre	Applied zone provisions Clause 34.01 Commercial 1 Zone
Land shown on plan 1 of this schedule Residential on a lot wholly within the local town centre walkable catchment	Applied zone provisions Clause 32.07 Residential Growth Zone
Land shown on map 1 of this schedule All other land	Applied zone provisions Clause 32.08 General Residential Zone

Commented [D1]: How does this applied zone work? Only one lot is entirely within the walkable catchment currently and does this apply only to residential lots?
Suggest rewording to clarify.

2.3

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Specific provisions – Use of land

Table 2: Use of land

Section 1 - Permit not required

Use	Condition
Child care centre	On land identified as 'local community facilities' in the incorporated <i>Craigieburn West Precinct Structure Plan</i> .
Hall	
Indoor recreation centre	
Library	
Medical Centre	
Restricted Recreation Facility	

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Use	Condition
Primary school Secondary school	On land identified as 'potential non-government school' in the incorporated <i>Craigieburn West Precinct Structure Plan</i> .
Minor sports and recreation facility	On land identified as 'local sports reserve' in the incorporated <i>Craigieburn West Precinct Structure Plan</i> .
Shop – where the applied zone is Commercial 1 Zone	The combined leasable floor area of all Shop must not exceed 6,000 square metres and must be located on land identified as 'Local Town Centre' in the incorporated <i>Craigieburn West Precinct Structure Plan</i> .
Any use listed in Clause 62.01	Must meet requirements of Clause 62.01.

Section 2 - Permit required

Use	Condition
Any other use not in Section 1 or 3 in the Table of uses in the applicable applied zone	

Section 3 – Prohibited

Use
Any other use in Section 3 in the Table of uses of the applicable applied zone

2.4 Specific provision – Subdivision

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None specified.

2.5 Specific provision – Buildings and works

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Dwellings on a lot less than 300 square metres

A permit is not required to construct or extend one dwelling on a lot with an area less than 300 square metres where a site is identified as a lot to be assessed against the Small Lot Housing Code via a restriction on title, and it complies with the Small Lot Housing Code incorporated pursuant to Clause 72.04 of the Hume Planning Scheme.

Buildings and works for future local parks and local community facilities

A permit is not required to construct a building or construct or carry out works for a local park, local sports reserve or local community facility provided the use or development is carried out generally in accordance with the incorporated *Craigieburn West Precinct Structure Plan* and with the prior written consent of Hume City Council.

Buildings and works for a school

A permit is required to construct a building or construct or carry out works associated with a Primary School or Secondary school on land shown as 'potential non-government school' unless exempt under Clauses 62.02-1 and 62.02-2.

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Bulk Earthworks

A permit is required for bulk earthworks, unless a report has been prepared to the satisfaction of the responsible authority demonstrating that sodic and/or dispersive soils are not present in the works area.

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Application requirements

The following application requirements apply to an application for a permit under Clause 37.07, in addition to those specified in Clause 37.07 and elsewhere in the scheme and must accompany an application, as appropriate, to the satisfaction of the responsible authority.

If in the opinion of the responsible authority an application requirement listed below is not relevant to the assessment of an application, the responsible authority may waive or reduce the requirement.

Public Infrastructure Plan

For an application to use or subdivide land or construct a building or construct or carry out works, a Public Infrastructure Plan which addresses the following:

- What land may be affected or required for the provision of infrastructure works;
- The provision, staging and timing of road works internal and external to the land consistent with any relevant traffic report or assessment;
- What, if any, infrastructure set out in the infrastructure contributions plan applying to the land is sought to be provided as “works in lieu” subject to the consent of the collecting agency;
- The provision of public open space and land for any community facilities; and
- Any other matter relevant to the provision of public infrastructure required by the responsible authority.

Subdivision – Residential development

For a residential subdivision, a site and context description and design response as required in Clause 56.

For an application subdivide ten or more lots, in addition to the above, or for the construction of ten or more dwellings:

- A written statement that sets out how the application implements the incorporated *Craigieburn West Precinct Structure Plan*.
- A land use budget setting out the amount of land allocated to the proposed uses and expected population, dwelling and employment yields.
- A plan showing access arrangements for properties adjacent to all existing and future arterial roads.
- A drainage and integrated water management plan.
- An arboricultural report identifying all trees on the site and a tree retention plan identifying how the application responds to Plan 10– Native vegetation retention and removal and any tree protection requirements and guidelines within the incorporated *Craigieburn West Precinct Structure Plan*;
- Potential bus route and bus stop locations prepared in consultation with the Head of Public Transport Victoria;
- Where relevant, demonstration of how the subdivision will respond sensitively to the heritage significance of Dunhelen House and Barn (Heritage Overlay – HO31) and Mickleham State School No.1051 (Heritage Overlay – HO35) and their surrounding areas;

HUME PLANNING SCHEME

- A Stormwater Management Strategy that assesses the existing surface and subsurface drainage conditions on the site, addresses the provision, staging and timing of stormwater drainage works, including temporary outfall provisions, to the satisfaction of Hume City Council and Melbourne Water;
- A Bushfire Site Management Plan that addresses bushfire risk during, and where necessary, after construction which is approved by the responsible authority. The plan must specify, amongst other things:
 - The staging of development and the likely bushfire risks at each stage;
 - An area of land between the development edge and non-urban areas consistent with the separation distances specified in AS3959-2018, where bushfire risk is managed to enable the development, on completion, to achieve a BAL-12.5 construction standard in accordance with AS3959-2018;
 - The land management measures to be undertaken by the developer to reduce the risk from fire within any surrounding rural or undeveloped landscape to protect residents and property from the threat of grassfire and bushfire;
 - Provision of adequate access and egress for early subdivisions to minimise grass and bushfire risks to new residents prior to the full completion of the PSP.

Preliminary Site Investigation

For an application to use or subdivide land or construct a building or construct or carry out works for a sensitive use (residential use, child care centre, pre-school centre or primary school) must be accompanied by an Preliminary Site Investigation of the land prepared by a suitably qualified environmental professional to the satisfaction of the responsible authority, that:

- takes account of the report titled *Craigieburn West Precinct Structure Plan Preliminary Land Contamination Assessment* prepared by Landserv Environment, December 2018;
- is prepared in accordance with Schedule B2 of the National Environment Protection (Assessment of Site Contamination) Measure 1999 (as amended 2013) (NEPM); and
- Provide a determination as to whether the environmental condition of the land is suitable for the proposed use/s or whether an environmental audit of the land is recommended having regard to the Potentially Contaminated Land General Practice Note June 2005, DSE.

Geotechnical and Groundwater Assessment

For an application to subdivide land or construct or carry out earthworks, a geotechnical and groundwater assessment prepared by a suitably qualified professional to the satisfaction of the responsible authority that:

- takes into account of the report titled *Hydrogeological, Salinity, Acid Sulphate Soil and Geotechnical Assessment – Craigieburn West PSP* prepared by Beveridge Williams, September 2020;
- includes a physical groundwater investigation through the areas identified with shallow (<5 m) and potential saline waters (as indicated on Figures 4 and 5 of the *Hydrogeological, Salinity, Acid Sulphate Soil and Geotechnical Assessment* prepared by Beveridge Williams, September 2020.
- includes gauging/sampling to confirm the groundwater flow direction and hydrogeological conditions to confirm risk of groundwater intrusion during excavation or bulk earthworks

Details of any proposed dam backfilling including water quality testing and proposed fill.

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Sodic and dispersive soils management plan

For an application to subdivide land or construct or carry out bulk earthworks must, a sodic and dispersive soils management plan, prepared by a suitably qualified professional, that describes:

- The existing site conditions, including:
 - extent of sodic and dispersive soils based on topsoil and subsoil samples in the works area.
 - land gradient.
 - erosion risk mapping.
 - the extent of any existing erosion, landslip or other land degradation.
- Soils investigation, undertaken by a soil scientist;
- The extent of any proposed earthworks;
- Recommendations for soil management practices (including fill) with consideration of anticipated sodic and dispersive soil exposure;
- The management of drainage during all stages of development (including run-off);
- The staging of development;
- Any training and supervisions processes proposed for construction contractors to ensure compliance with the sodic and dispersive soils management plan;
- Proposed document monitoring and reporting processes that ensure works are undertaken in accordance with the sodic and dispersive soils management plan;
- Any treatment of soil proposed to be removed from the site;
- Any post-construction monitoring and/or management requirements; and
- Recommendations that inform a site management plan including:
 - The management, volume and location of any stockpiles.
 - Vehicle access and movement within the site area.
 - Any treatment to manage the soil while works are undertaken.
 - Treatments to rehabilitate areas that are disturbed during site works.
 - Any soil treatment to manage the soil to reduce risk to existing or current infrastructure and dwellings.

Affordable Housing Assessment

For an application for residential subdivision or to construct a dwelling or residential building an affordable housing report, prepared by a suitably qualified person, that includes:

- Anticipated demographic characteristics of the suburb;
- Anticipated household composition (i.e. singles, couples, families with dependants);
- What proportion of housing is estimated to be provided for the income ranges specified at 3AA(4) of the Act; and
- A response to matters addressed in the Ministerial Notice under 3AA(2) of the Act.

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Heritage Places

For an application to subdivide land or to construct a building or construct or carry out works on land at 1240 Mickelham Road, Greenvale and 1880 Mickleham Road, Mickleham (Mickleham State School No.1051) must include the following to the satisfaction of the responsible authority:

- a Heritage Conservation Management Plan (HCMP) for the heritage place; and
- A statement that takes into account the approved HCMP and explains how the significance of the identified heritage features has been considered in the design of the development, including:
 - incorporation within open space/public realm;
 - design of perimeter fencing;
 - surrounding residential interfaces;
 - interpretive signage, information boards relating to buildings that have been retained, restored, rebuilt or removed.

Dry Stone Wall Assessment

For an application to subdivide 220 and 250 Olivers Road, Mickleham, a Dry Stone Wall assessment, prepared by a suitably qualified professional, to the satisfaction of the responsible authority that describes:

- the quality and retention values of the dry stone wall,
- how the subdivision meets the incorporated *Craigieburn West Precinct Structure Plan*.

Kangaroo Management Plan

For an application to subdivide land, a Kangaroo Management Plan prepared to the satisfaction of the Secretary to the Department of Environment, Land, Water and Planning that:

- addresses the recommendations of the *Eastern Grey Kangaroo Strategic Management Plan: Craigieburn West Precinct Structure Plan (PSP 1068)*, *Craigieburn* prepared by Ecology and Heritage Partners, dated November 2020; and includes:
 - Strategies to avoid land locking kangaroos, including staging of subdivision;
 - Strategies to minimise animal and human welfare risks;
 - Management and monitoring actions to sustainably manage a population of kangaroos within a suitable location; and
 - Actions to address the containment of kangaroos to ensure adequate animal welfare.

4.0

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Conditions and requirements for permits

Condition - subdivision permits that allow the creation of a lot of less than 300 square metres

Any permit for subdivision that allows the creation of a lot less than 300 square metres must contain the following conditions:

- Prior to certification of the Plan of Subdivision for the relevant stage, a plan must be submitted for approval to the satisfaction of the responsible authority. The plan must identify the lots that will include a restriction on title allowing the use of the

HUME PLANNING SCHEME

provision of the Small Lot Housing Code incorporated pursuant to Clause 72.04 of the Hume Planning Scheme; and

- The Plan of Subdivision submitted for certification must identify whether type A or type B of the Small Lot Housing Code applies to each lot to the satisfaction of the responsible authority.

Condition – Subdivision or buildings and works permits where land is required for community facilities, public open space or road widening

A permit for subdivision or buildings and works, where land is required for community facilities, public open space or road widening must include the following conditions:

- The costs associated with effecting the transfer or vesting of land required for community facilities, public open space or road widening must be borne by the permit holder.
- Land required for community facilities, public open space or road widening must be transferred to or vested in the relevant public agency with any designation (e.g. road, reserve or lot) nominated by the relevant agency.

Conditions and requirements - Kangaroo Management Plan

A permit granted for subdivision of land must include the following conditions:

Before the certification of the plan of subdivision, a Kangaroo Management Plan must be approved by the Secretary to the Department of Environment, Land, Water and Planning (DELWP). The approved plan will form part of the permit.

Condition - Environmental Management Plans

A planning permit to subdivide land, construct a building, or construct or carry out works within 30 metres of land shown as a conservation area in the incorporated *Craigieburn West Precinct Structure Plan* must include the following condition:

The subdivision, buildings or works must not commence until an Environmental Management Plan has been approved to the satisfaction of the Secretary to the Department of Environment, Land, Water and Planning.

Condition - Salvage and translocation

A planning permit to subdivide land, construct a building, or construct or carry out works must include the following condition:

The Salvage and Translocation Protocol for Melbourne's Growth Corridors (Department of Environment, Land, Water and Planning, 2017) must be implemented in the carrying out of development to the satisfaction of the Secretary to the Department of Environment, Land, Water and Planning.

Requirement - Protection of conservation areas and native vegetation during construction

A permit to subdivide land, construct a building or carry out works, where the incorporated *Craigieburn West Precinct Structure Plan* shows the land, or abutting land, including a conservation area or a patch of native vegetation or a scattered tree must contain the following conditions:

Before the commencement of buildings or works within or on or within 30m of land abutting a conservation area shown on Plan 4 of the incorporated *Craigieburn West Precinct Structure Plan* as conservation area, the permit holder must erect a vegetation protection fence around any conservation area, patch of native vegetation or scattered tree identified for retention, or vegetation identified for salvage in the incorporated *Craigieburn West Precinct Structure Plan*. The fence must be:

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- highly visible
- at least 2 metres in height
- sturdy and strong enough to withstand knocks from construction vehicles
- in place for the whole period of construction occurring within 30m of the conservation area, patch of native vegetation or scattered tree
- located at a minimum distance from:

Element	Minimum distance from element
Conservation area	0.5 metres
Scattered tree	12 x Diameter at Breast Height
Patch of native vegetation	2 metres

During the undertaking of buildings or works, all activities must be excluded from occurring within the protection fencing, unless otherwise agreed to by the Department of Environment, Land, Water and Planning.

Construction stockpiles, fill, machinery, vehicle parking, excavation and works or other activities associated with the buildings or works must be designed and constructed to ensure that the conservation area, scattered trees or patches of native vegetation identified for retention in the Precinct Structure Plan are protected from adverse impacts during construction.

Requirement - Land Management Co-operative Agreement

A permit to subdivide land on land shown in the incorporated *Craigieburn West Precinct Structure Plan* as including a conservation area shown on Plan 4 of the precinct structure plan as a BCS conservation area, must ensure that, before the issue of a statement of compliance for the last stage of the subdivision, the owner of the land:

- Enters into an agreement with the Secretary to the Department of Environment, Land, Water and Planning under section 69 of the *Conservation Forests and Lands Act 1987*, which:
 - Must provide for the conservation and management of that part of the land; and
 - May include any matter that such an agreement may contain under the *Conservation Forests and Lands Act 1987*.
- Makes application to the Registrar of Titles to register the agreement on the title to the land.
- Pays the reasonable costs of the Secretary to the Department of Environment, Land, Water and Planning in the preparation, execution and registration of the agreement.

The requirement for a Land Management Co-operative Agreement in this clause does not apply to land of any lot or part of a lot within the conservation area shown on Plan 4 of the incorporated *Craigieburn West Precinct Structure Plan* as a conservation area, that:

- is identified in a Precinct Structure Plan as public open space and is vested, or will be vested, in the council as a reserve for the purposes of public open space; or
- is identified Precinct Structure Plan as a drainage reserve and is vested, or will be vested, in Melbourne Water Corporation or the council as a drainage reserve; or
- is the subject of an agreement with the Secretary to the Department of Environment, Land, Water and Planning to transfer or gift that land to:
 - the Secretary to the Department of Environment, Land, Water and Planning;
 - the Minister for Environment and Climate Change; or
 - another statutory authority

to the satisfaction of the Secretary to the Department of Environment, Land, Water and Planning.

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Condition – Environmental Audit

Any permit for the use and development of land for a sensitive use (residential use, child care centre, pre-school centre or primary school) and where the Preliminary Site Investigation recommends that an environmental audit is required, must contain the following conditions:

- Prior to the commencement of the use or buildings and works associated with the use (or the certification or issue of a statement of compliance under the *Subdivision Act 1988*) the applicant must provide: (a) A Certificate of Environmental Audit in accordance with Section 53Y of the *Environment Protection Act 1970*; or (b) A Statement of Environmental Audit under Section 53Z of the *Environment Protection Act 1970*. A Statement must state that the site is suitable for the use and development allowed by this permit.
- All the conditions of the Statement of Environmental Audit must be complied with to the satisfaction of the responsible authority. Written confirmation of compliance must be provided by a suitably qualified environmental professional or other suitable person acceptable to the responsible authority. In addition, sign off must be in accordance with any requirements in the Statement conditions regarding verification of works.

Condition - Public transport

Unless otherwise agreed by Head, Transport for Victoria, prior to the issue of Statement of Compliance for any subdivision stage, bus stop hard stands with direct and safe pedestrian access to a pedestrian path must be constructed:

- In accordance with the Public Transport Guidelines for Land Use and Development; and compliant with the Disability Discrimination Act – Disability Standards for Accessible Public Transport 2002.
- At locations approved by Public Transport Victoria, at no cost to Public Transport Victoria, and to the satisfaction of Head, Transport for Victoria.

Condition - Road network

Any permit for subdivision or building and works must contain the following conditions:

- Prior to the certification of a plan of subdivision, the plan of subdivision must show the land affected by the widening of the road reserve which is required to provide road widening and/or right of way flaring for the ultimate design of any adjacent intersection.
- Land required for road widening including right of way flaring for the ultimate design of any intersection within an existing or proposed arterial road must be transferred to or vested in council at no cost to the acquiring agency unless funded by the Craigieburn West Infrastructure Contributions Plan.

Condition - Public Infrastructure Plan

Any permit for subdivision must contain the following condition:

- Prior to the certification of a plan of subdivision or at such other time which is agreed between Council and the owner, if required by the responsible authority or the owner, the owner must enter into an agreement or agreements under section 173 of the Act which provides for:
 - The implementation of the Public Infrastructure Plan approved under this permit.
 - The timing of any payments to be made to a person in respect of any infrastructure project having regard to the availability of funds in the Infrastructure Contributions Plan.

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Requirements – Sodic and dispersive soil site management plan

A permit to subdivide land or to undertake earthworks must include a condition that requires a site management plan be prepared that implements the recommendations identified in the sodic and dispersive soil management plan, to the satisfaction of the Responsible Authority.

5.0
DD/MM/YYYY
Proposed
Cxxshume

Exemption from notice and review

None specified.

6.0
DD/MM/YYYY
Proposed
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Decision guidelines

The following decision guidelines apply to an application for a permit under Clause 37.07, in addition to those specified in Clause 37.07 and elsewhere in the scheme which must be considered, as appropriate, by the responsible authority:

Affordable Housing

For an application for residential subdivision or a to construct a dwelling:

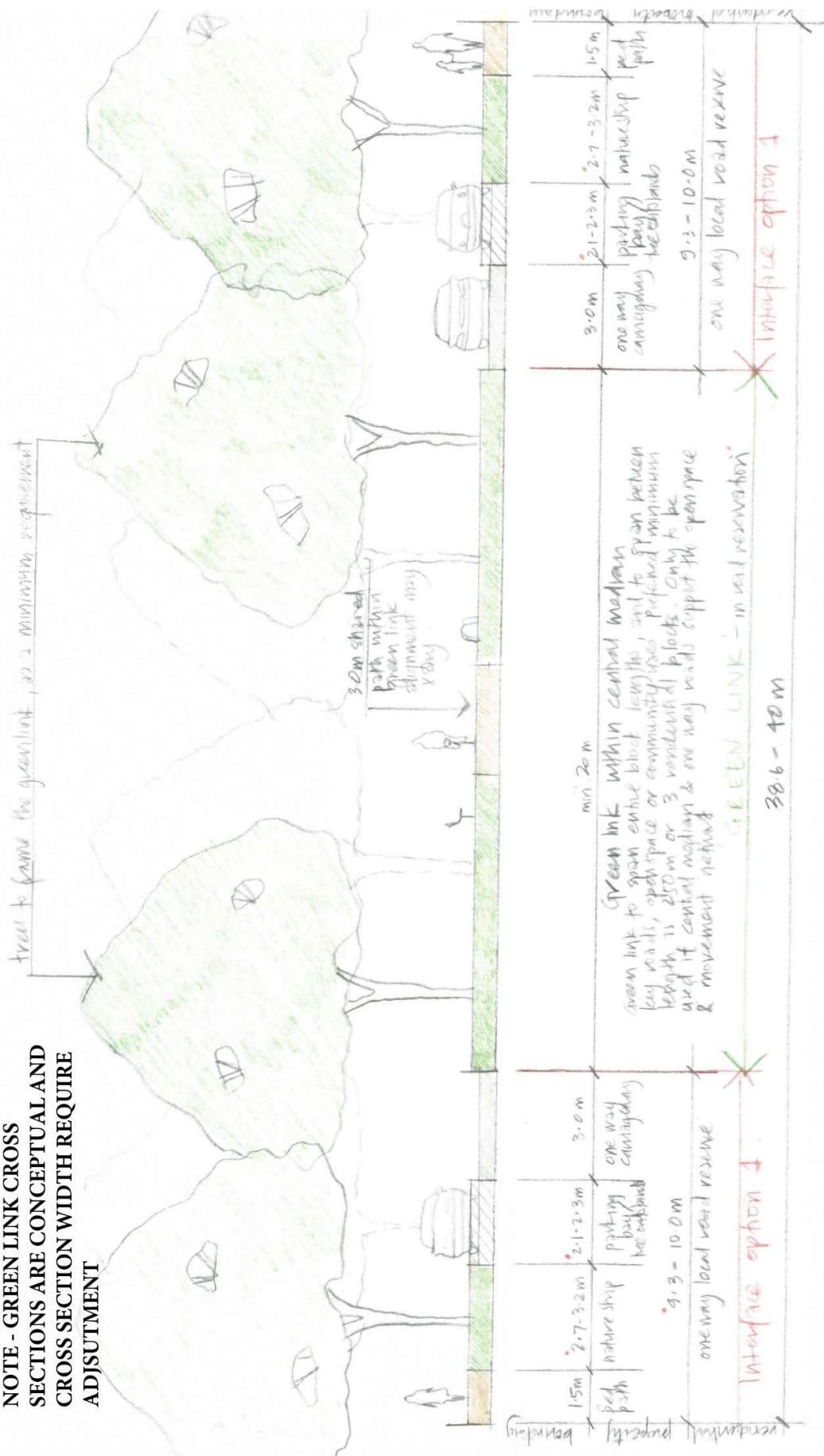
- Whether the proposed subdivision application has demonstrated that the proposal will contribute towards the provision of affordable housing;
- The Ministerial Notice under 3AA(2) of the Act, as amended from time to time.

7.0
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Proposed
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Signs

Sign requirements are at Clause 52.05. All land within the Craigieburn West Precinct Structure Plan area is included in the category specified in its applied zone at Clause 2.2 of this schedule.

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green link cross section (38.6 - 40 m) - Residual interval both sides -
green link accommodated within a 30m central median in a local road context
Changie 6m west esp - Data for discussion only

Photograph
1900

[illegible]

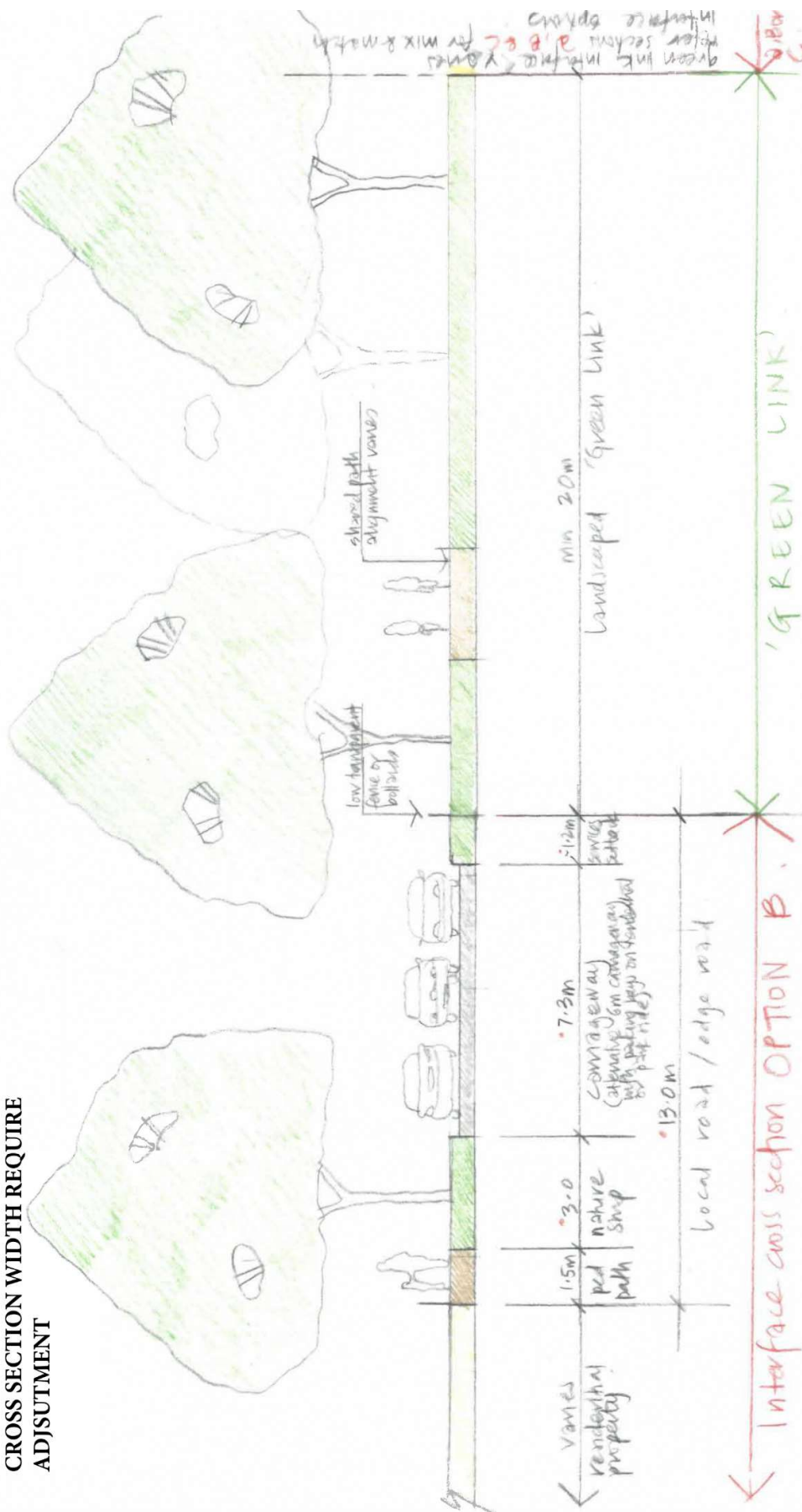
varies
redundant property directly forming
green link. Break in holding to occur
main 'corn' to facilitate access +
permeability to green link

- Interface cross section option:
 - Visitor parking to be provided in side streets or streets intersecting with the green link. Alternatively visitor parking may be accommodated within cross section / interface and opposite side of green link.

Mix & match. Green ink intended - vendor has! Intake to Green
Rear loaded medium density with paper was postage to Green
CHALLENGER West - Unit for document only

mean/
Home City Council

SECTIONS ARE CONCEPTUAL AND CROSS SECTION WIDTH REQUIRE ADJUSTMENT



Mix & match Green link Interface B - randomised interface both sides

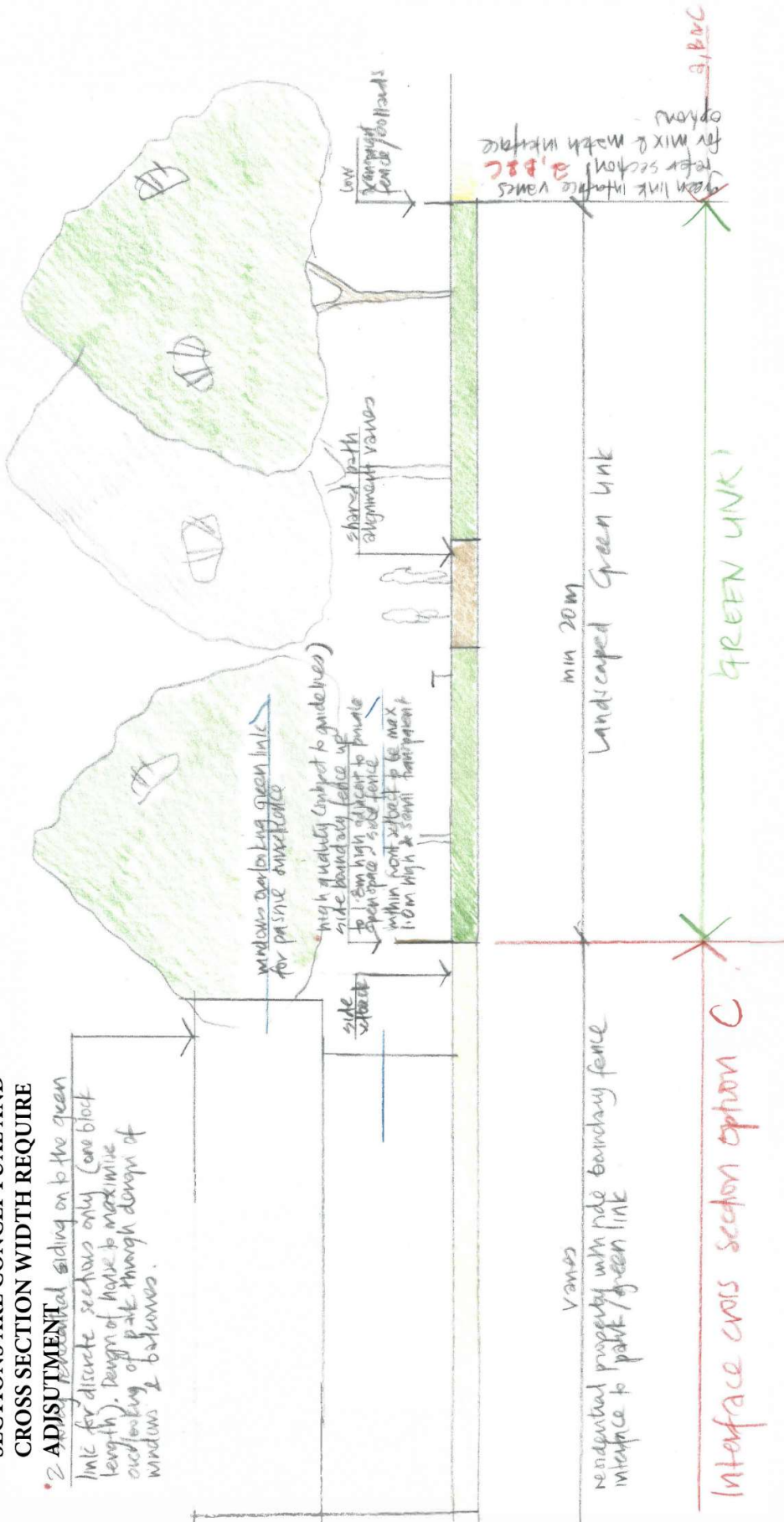
Local edge ppad - ppt for discussion only

mooh /
Humidity / Canal

NOTE - GREEN LINK CROSS

**SECTIONS ARE CONCEPTUAL AND
CROSS SECTION WIDTH REQUIRE**

- 2 ADJUSTMENT
link for discrete sections only (one block length). Design of house to maximise overlooking of park through design of windows & balconies.



C Mix & match Green link interface C - residential interface both sides
Residential lot with side boundary edge to green link
Craigieburn West PSP - Draft for discussion only
mean/
Hume City Council

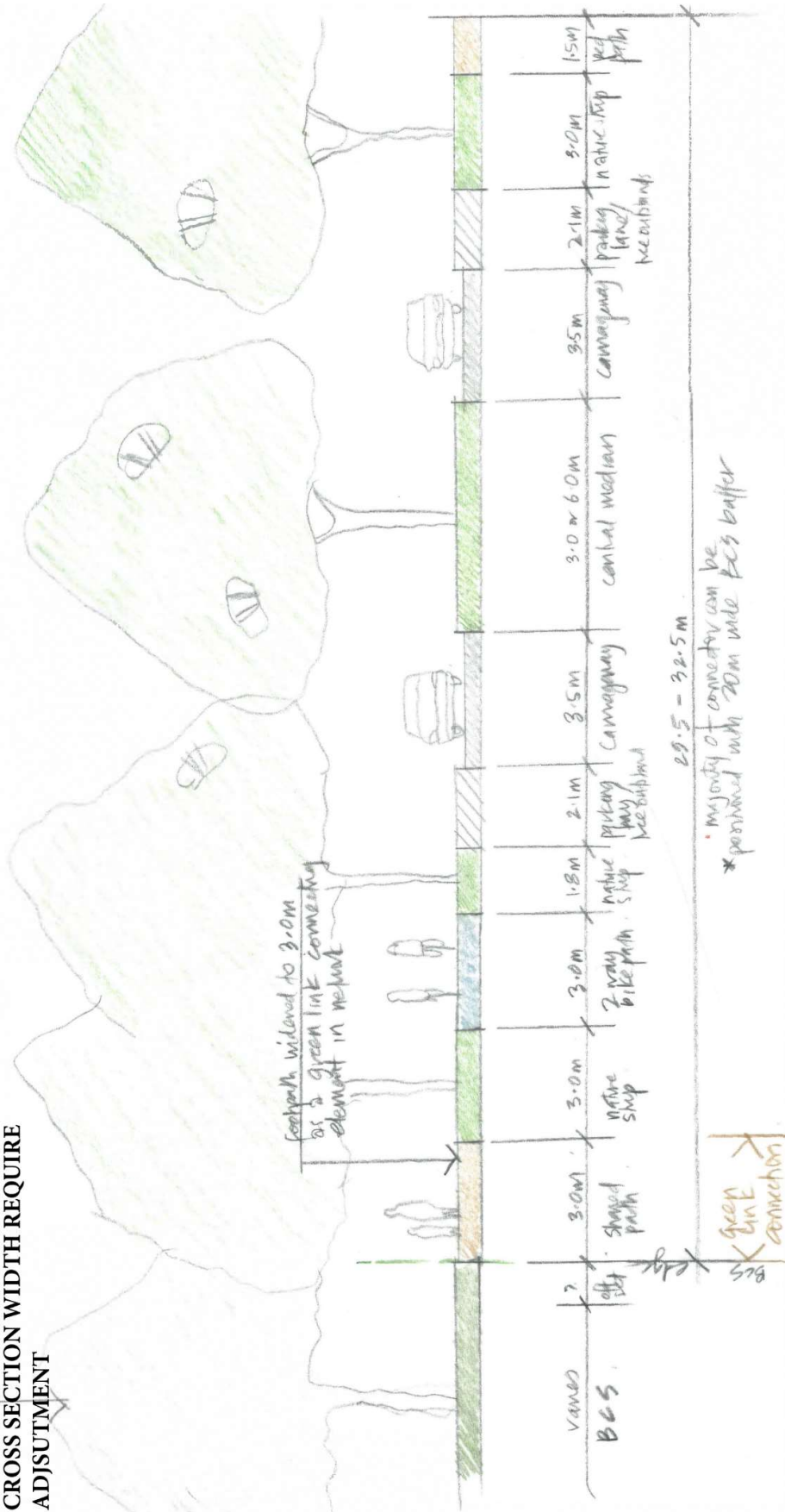


Green link key shared path connection (local road adjacent to BUS)
Shared path indicated in 20m local road in BUS buffer
Carriageway West side

Q

mean /
HCC

NOTE - GREEN LINK CROSS
SECTIONS ARE CONCEPTUAL AND
CROSS SECTION WIDTH REQUIRE
ADJUSTMENT



E Green link key shared path connection (Bld connector adjacent to BCS)
shared path incorporated into the Boulevard connector
Craigieburn West per Draft for discussion only
medn/
HCC

for mix of 7



35. 3 BM

F Green link cross section (35-38m)
Greenlink incorporated into the Bonivard connector
Changeboard next PSP - Draft for discussion only

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Hon Richard Wynne MP

Minister for Planning
Minister for Housing

8 Nicholson Street
East Melbourne, Victoria 3002

Cr Carly Moore
Mayor
Hume City Council
PO Box 119
DALLAS VIC 3047

Ref: MIN076223



Dear Mayor

CRAIGIEBURN WEST PRECINCT STRUCTURE PLAN

Thank you for your letter of 11 September 2020 about my decision to direct the Victorian Planning Authority (VPA) to expedite the finalisation of the Craigieburn West Precinct Structure Plan (PSP) and associated planning scheme amendment.

I note that your council's support for an expediated process is contingent upon a budget commitment from the Victorian Government for the duplication of Mickleham Road, between Somerton and Donnybrook roads, and for it to be delivered prior to or concurrent with development of the PSP. I also acknowledge that your council seeks assurance that the preparation of the PSP will include appropriate community consultation and that it will be informed about the particulars of the associated Infrastructure Contributions Plan (ICP).

The Department of Environment, Land, Water and Planning (DELWP), the VPA and the Department of Transport are working together to better inform a prioritisation of state transport improvements and services to support growth in the northern growth corridor. The relevant growth area councils will be updated on the outcomes of these investigations and recommendations in due course. These agencies are aware that Mickleham Road is an important part of the network and its upgrading will be required over time to accommodate growth.

The VPA's Fast Track Program will allow for a four-week public consultation period of draft plans and the opportunity for community discussion and feedback on the outcomes proposed for the Craigieburn West precinct through submissions. An engagement strategy is being prepared to support public consultation, and I can confirm that the VPA intends to hold a range of online forums to allow the community to have input into the draft PSP. I invite your council to discuss the proposed engagement strategy with the VPA and to provide advice on any particular needs of the Hume community at this time.

As detailed in my letter to you of 23 August 2020, the duplication of Mickleham Road will not form part of the proposed Craigieburn West ICP. The ICP will include the provision of new intersections along the Mickleham Road corridor to access the Craigieburn West precinct, along with the provision of a range of other community and transport infrastructure items considered 'allowable' under the Ministerial Direction on the Preparation and Content of Infrastructure Contribution Plans. Although the ICP is still being prepared, I am advised that costing estimates for its project items indicate that it will be a standard levy ICP. Pending any changes to the PSP or its infrastructure items, the VPA will provide the draft costing schedules and supporting methodology to your council for its consideration prior to the public consultation. Should a supplementary levy be required, the full ICP document will be exhibited in accordance with requirements set out by the *Planning and Environment Act 1987* and the Ministerial Direction.



If you have any questions about the work being done to inform a prioritisation of transport network and services improvements, please contact Rachel Dapiran, Executive Director, Planning, Infrastructure and Technical, VPA, on (03) 9651 9669 or email rachel.dapiran@vpa.vic.gov.au.

If you have any questions about the ICP and proposed engagement strategy for the Craigieburn West PSP, please contact Dean Rochfort, Executive Director, Outer Melbourne, VPA, on (03) 9651 9689 or email dean.rochfort@vpa.vic.gov.au.

If you have any questions about this letter, please contact Dr Jane Homewood, Executive Director, Statutory Planning Services, DELWP, on (03) 8683 0975 or email jane.homewood@delwp.vic.gov.au.

Thank you for raising this matter with me.

Yours sincerely



HON RICHARD WYNNE MP
Minister for Planning

18 / 10 / 2020

REPORT NO:	GE490
REPORT TITLE:	Allocation of Councillor Portfolios
SOURCE:	Megan Kruger, Manager Governance
DIVISION:	Corporate Services
FILE NO:	HCC12/403
POLICY:	Councillor Portfolios
STRATEGIC OBJECTIVE:	5.3 Provide responsible and transparent governance, services and infrastructure which responds to and supports community needs.
ATTACHMENT:	1. <i>Councillor Portfolio Guidelines Policy</i>

1. SUMMARY OF REPORT:

- 1.1 Portfolios are allocated to Councillors at the beginning of each Council term following the election of a new Council.
- 1.2 This report presents Council with a list of portfolios and recommends Councillors for allocation to each of those portfolios, including some portfolios where the allocation of multiple Councillors is recommended.

2. RECOMMENDATION:

THAT Council allocates portfolios to the Councillors recorded against each portfolio in the following table:

Portfolio	Councillor/s	Director
Customer Service	Mayor Cr Dance	Communications, Engagement and Advocacy
Communications and Advocacy	Mayor Cr Misho Cr Kurt	Communications, Engagement and Advocacy
Council and Service Planning	Mayor Cr Dance	Communications, Engagement and Advocacy
Organisation and Community Intelligence	Mayor Cr Dance	Communications, Engagement and Advocacy
Governance and Associated Statutory Services	Mayor Cr Moore Cr Dance	Corporate Services
Finance and Procurement Services	Cr Misho Cr Moore	Corporate Services
Information Technology - Digital	Cr Kurt	Corporate Services
Asset Management and Infrastructure Development	Cr Sherry	Sustainable Infrastructure and Services

REPORT NO: GE490 (cont.)

Portfolio	Councillor/s	Director
Transport, Roads and Drainage	Cr Overend Cr Haweil	Sustainable Infrastructure and Services
Community Safety	Cr Haweil Cr Overend Cr Bell	Community Services, Corporate Services, Sustainable Infrastructure and Services
Disability Support Services	Cr Sherry	Community Services
Community Development	Cr Haweil Cr Kurt Cr Misho	Community Services
Leisure, Health and Wellbeing	Cr Moore Cr Kurt Cr Hollow	Corporate Services, Community Services
Indigenous Support	Cr Kurt Cr Medcraft	Community Services
Aged Support Services	Cr Sherry	Community Services
Family Support and Health	Cr Moore	Community Services
Early Childhood Education and Care	Cr Moore	Community Services
Lifelong Learning	Cr Bell	Community Services
Youth Services	Cr Bell	Community Services
Facilities Management/Facilities Hire	Cr Haweil	Community Services
Arts and Culture	Cr Kurt Cr Bell	Community Services
Economic Development	Cr Medcraft Cr Misho Cr Kurt	Planning and Development
Interface and Growth	Cr Haweil	Planning and Development
Parks and Open Space	Cr Jackson Cr Hollow Cr Dance	Sustainable Infrastructure and Services, Planning and Development
City Development - Statutory	Cr Medcraft Cr Sherry Cr Dance	Planning and Development
City Development - Strategic	Cr Medcraft Cr Sherry	Planning and Development

REPORT NO: GE490 (cont.)

Portfolio	Councillor/s	Director
	Cr Dance	
City Amenity and Appearance	Cr Medcraft Cr Overend	Sustainable Infrastructure and Services
Waste Management	Cr Medcraft	Sustainable Infrastructure and Services
Environment	Cr Jackson Cr Dance	Sustainable Infrastructure and Services
Friends of Aileu	Cr Haweil Cr Kurt	Community Services

3. **LEGISLATIVE POWERS:**

Local Government Act 2020

4. **FINANCIAL IMPLICATIONS:**

There are no financial implications arising from the matters contained within this report.

5. **ENVIRONMENTAL SUSTAINABILITY CONSIDERATIONS:**

There are no environmental sustainability considerations arising from the matters contained within this report.

6. **CLIMATE CHANGE ADAPTATION CONSIDERATIONS:**

There are no climate adaption considerations arising from the matters contained within this report.

7. **CHARTER OF HUMAN RIGHTS APPLICATION:**

The rights protected in the *Charter of Human Rights and Responsibilities Act 2006* are not limited by the matters contained within this report.

8. **COMMUNITY CONSULTATION:**

Community consultation is not required prior to Council allocating portfolios to Councillors, as per the recommendations contained in this report.

9. **DISCUSSION:**

9.1 The primary role of a portfolio Councillor is to be a spokesperson within the Council Chamber for those issues falling within their allocated portfolio.

9.2 The portfolios that are recommended to be allocated to Councillors for this term of Council are as follows:

- (a) Customer Service;
- (b) Communications and Advocacy;
- (c) Council and Service Planning;
- (d) Organisation and Community Intelligence;
- (e) Governance and Associated Statutory Services;
- (f) Finance and Procurement Services;
- (g) Information Technology – Digital;
- (h) Asset Management and Infrastructure Development;
- (i) Transport, Roads and Drainage;

REPORT NO: GE490 (cont.)

- (j) Community Safety;
- (k) Disability Support Services;
- (l) Community Development;
- (m) Leisure, Health and Wellbeing;
- (n) Indigenous Support;
- (o) Aged Support Services;
- (p) Family Support and Health;
- (q) Early Childhood Education and Care;
- (r) Lifelong Learning;
- (s) Youth Services;
- (t) Facilities Management/Facilities Hire;
- (u) Arts and Culture;
- (v) Economic Development;
- (w) Interface and Growth;
- (x) Parks and Open Space;
- (y) City Development – Statutory;
- (z) City Development – Strategic;
- (aa) City Amenity and Appearance;
- (bb) Waste Management;
- (cc) Environment;
- (dd) Friends of Aileu

- 9.3 Allocating portfolio responsibilities to individual Councillors, and to multiple Councillors where this is considered by Council to be appropriate, allows Councillors to develop a more detailed knowledge of the projects and issues that sit within their allocated portfolios.
- 9.4 Councillors are guided by Council's *Councillor Portfolio Guidelines Policy*, which is provided as Attachment 1 of this report. This policy outlines the objectives and role expected of portfolio holders.
- 9.5 The *Councillor Portfolio Guidelines Policy* state that Portfolio Councillors will regularly meet with the relevant Director, and senior management where appropriate, so that the Portfolio Councillor will have a more significant understanding of the portfolio activities and issues than Councillors generally. This will also allow Portfolio Councillors to be kept informed of topical issues, proposed legislative changes and industry and technology updates.
- 9.6 Portfolios that are allocated to the Mayor are held by the sitting Mayor. These portfolios do not sit with the individual Councillor who holds the office of Mayor throughout the Council term, they will change when the Councillor who holds the office of Mayor changes.

10. DISCUSSION:

It is recommended that Council allocates portfolios to Councillors for this term of Council.



COUNCILLOR PORTFOLIO GUIDELINES POLICY

Policy Reference No.	POL/158
File No.	HCC12/403
Strategic Objective	5.3 Provide responsible and transparent governance, services and infrastructure which responds to and supports community needs
Adopted by Council	June 2000
Re-Adopted	4 July 2016
Date for Review	4 July 2021
Responsible Officer	Manager Governance
Department	Corporate Services

COUNCILLOR PORTFOLIO GUIDELINES POLICY

1 POLICY STATEMENT

The main role of Portfolio Councillors is to be a spokesperson within the Council Chamber for those issues falling within their allocated portfolio. Portfolio Councillors should adhere to the Requesting Advice or Information - Protocols as contained in the Code of Conduct for Councillors, and not be involved in the day to day operation of the portfolio area.

2 PURPOSE

2.1 The purpose of this policy is to clarify expectations and set clear directions for the role of Portfolio Councillors. This includes defining the relationship between a Portfolio Councillor and the staff within the portfolio area, their involvement in policy discussion, and their role in representing Council on issues that fall within their portfolio.

2.2 The Portfolio Councillor's main role will be in the Council Chamber advising on topical issues and advocating for their portfolio area by requesting Council action or resources.

3 SCOPE

This policy applies to all Councillors of Hume City.

4 OBJECTIVE

4.1 That Council will be better informed by the Portfolio Councillor who will have a more significant understanding of the portfolio activities and issues than Councillors generally.

4.2 The Portfolio Councillor will be able to speak with knowledge of specific issues and activities of his/ her portfolio at Council meetings, and publicly if requested to by the Mayor.

4.3 To utilise the ability to greater inform Councillors on specific areas of Council's functions to maximise the use of all Councillor's time, whilst ensuring Council has at its disposal all relevant information for making decisions.

4.4 The Portfolio Councillor will contribute to officer's understanding of Council's preferences and direction for matters falling within the portfolio.

5 POLICY IMPLEMENTATION

5.1 Portfolio Councillors will regularly meet with the relevant Director (and senior management as deemed appropriate) to be informed on the topical issues, proposed legislative changes and industry and technology updates.

5.2 Portfolio Councillors will be well informed of controversial legislation or proposals and will endeavour to have a detailed understanding of issues.

5.3 Portfolio Councillors will not independently of Council set policy, or give instructions on report preparation. Portfolio Councillors may discuss the proposed content of reports with Directors or senior officers but the report preparation and content will always

Policy Reference No:	POL158	Responsible Officer:	Manager Governance
Date of Re/Adoption:	4 July 2016	Department:	Governance
Review Date:	4 July 2021		

COUNCILLOR PORTFOLIO GUIDELINES POLICY

remain the officer's responsibility, and it should be prepared free from improper influence or direction from the Portfolio Councillor.

5.4 Portfolio Councillors are not responsible for the performance outcomes of their portfolio area.

5.5 The communication of information to Councillors in general should not change, with all Councillors being kept well-informed of major controversial or topical issues requiring all Councillors' knowledge for possible future Council action. Portfolio holders may advise what extra information should be communicated to Council. This may be via memo, briefing note or information report to Council.

5.6 There is no provision for a Portfolio Councillor to sign any documents as a portfolio holder.

5.7 Any press releases or media enquiries will be handled by the Mayor and Portfolio Councillors will only speak to an issue if requested by the Mayor.

6 DEFINITIONS AND ABBREVIATIONS

6.1 Nil.

7 RELATED DOCUMENTS

7.1 Code of Conduct for Councillors

Date Adopted	June 2000
Date Re-Adopted	4 July 2016
Review Date	4 July 2021

Policy Reference No:	POL158	Responsible Officer:	Manager Governance
Date of Re/Adoption:	4 July 2016	Department:	Governance
Review Date:	4 July 2021		

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REPORT NO:	GE491
REPORT TITLE:	Rate Arrears as at 31 December 2020
SOURCE:	Fadi Srour, Chief Financial Officer
DIVISION:	Corporate Services
FILE NO:	HCC05/737
POLICY:	-
STRATEGIC OBJECTIVE:	5.3 Provide responsible and transparent governance, services and infrastructure which responds to and supports community needs.
ATTACHMENT:	1. <i>Debt Collection Process</i>

1. SUMMARY OF REPORT:

- 1.1 This report provides the current status of rates arrears / (outstanding) as at 31 December 2020.
- 1.2 Up until the COVID-19 Pandemic commenced, a more proactive approach towards the collection of rate arrears had been occurring with all available options of debt recovery being explored.
- 1.3 However, with the onset of the COVID-19 Pandemic, the focus has shifted away from debt recovery with a greater emphasis being placed on debtor management.

2. RECOMMENDATION:

- 2.1 That the Rate Arrears Report as at 31 December 2020 be received and noted.
- 2.2 That Council continues to support the community during this difficult time with the implementation of realistic payment plans and the introduction of the COVID-19 hardship waiver of \$50 for residential owners.

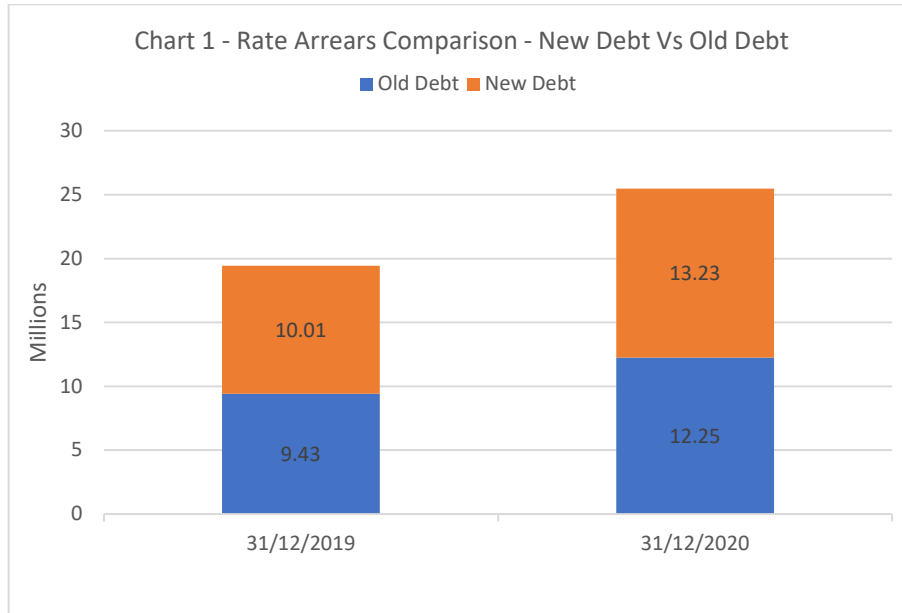
3. LEGISLATIVE POWERS:

Rates are raised and payment is required pursuant to the Sections contained in Part 8 of the Local Government Act 1989 including Section 181 which provides for the sale of rateable properties where rates remain unpaid for more than three years.

4. FINANCIAL IMPLICATIONS:

- 4.1 As at 31 December 2020, the total amount of rates outstanding was \$25.48m comprising rates raised in previous years (old debt) of \$12.25m (48.1%) and rates raised in 2020/21 (new debt) of \$13.23m (51.9%).
- 4.2 By way of comparison, at 31 December 2019, the total amount of rates outstanding was \$19.44m comprising \$9.43m (49%) of old debt and \$10.01m (51%) of new debt. Refer to Chart 1 below.

REPORT NO: GE491 (cont.)



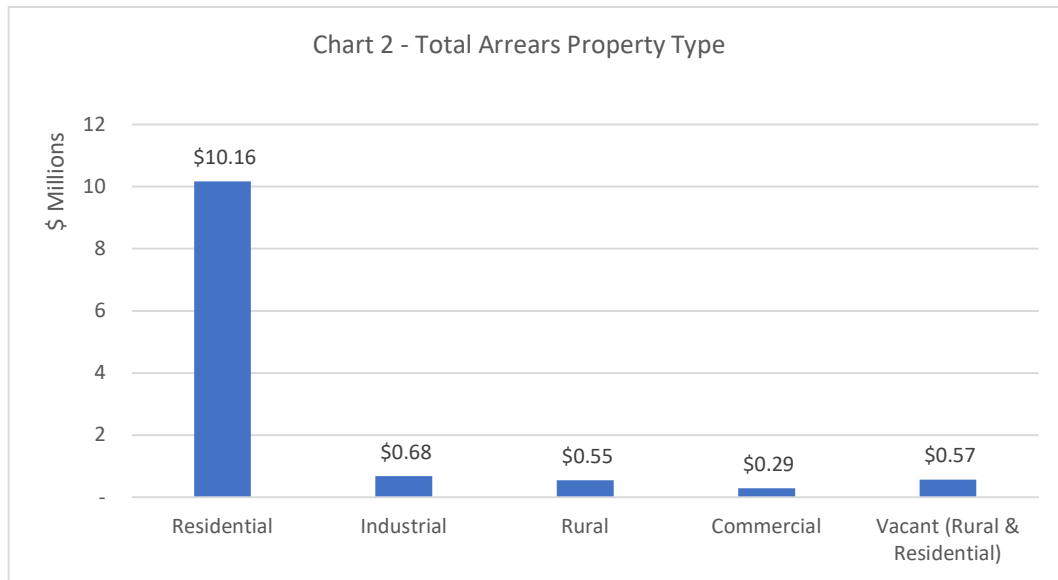
- 4.3 The breakdown of the rate arrears as at 31 December 2020 of \$12.25m compared to \$9.43m as at 31 December 2019 is provided below.

Table 1				
Charge Type	2020/21		2019/20	
	Arrears \$	% of Total	Arrears \$	% of Total
General Rates	10,797,394	88.32%	8,075,106	85.55%
Penalty Interest	761,582	6.23%	814,064	8.62%
Debt Recovery	496,371	4.06%	408,442	4.33%
Bin Charges	169,977	1.39%	141,564	1.50%
Total	12,225,324		9,439,176	

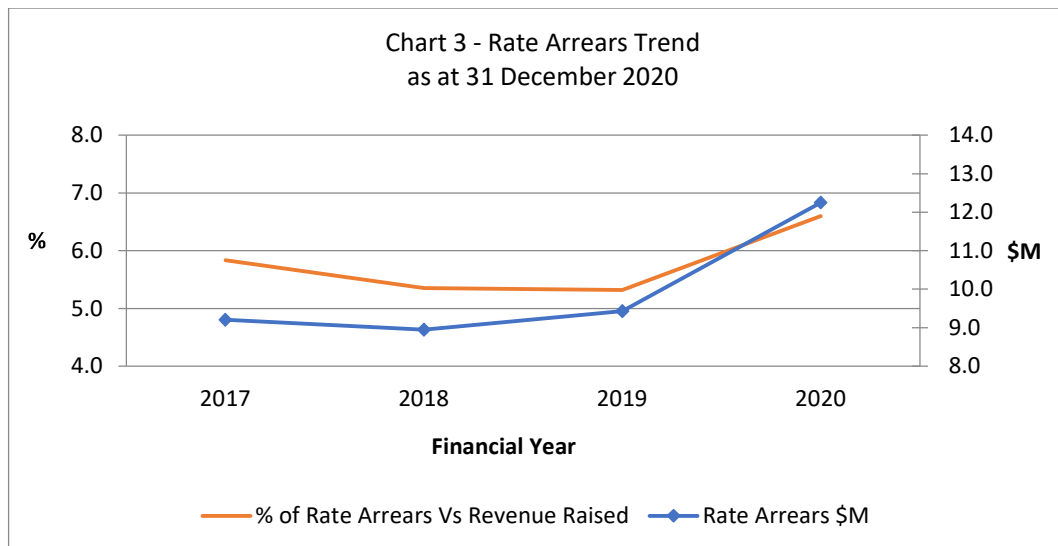
- 4.4 Chart 2 – Analysis of the rates arrears by property type.

The following chart illustrates the rate arrears as at 31 December 2020 of \$12.25m by property type. As outlined below, the majority (83%) of the rate arrears relates to residential properties.

REPORT NO: GE491 (cont.)



- 4.5 Chart 3 - The chart below indicates the effect of the COVID-19 Pandemic on the collection of rate arrears compared to previous rating periods as it shows that not only has the value of arrears increased, but so too has the % of rate arrears versus rate revenue raised which was previously trending lower. A one-time COVID-19 hardship waiver of \$3.1m has been applied to the 2020/21 council rates.



- 4.6 Of significant impact was the 2,837 COVID-19 arrangement applications received between April and December 2020, of which 1,396 are still active totalling \$2.45m. Further prominent COVID-19 statistics:
- 4.6.1 A total of 481 applications for financial hardship have been received so far for this past quarter (October to December 2020).
 - 4.6.2 99% of applications were submitted via the website without any contact centre assistance.
 - 4.6.3 A total of 26% have opted to defer their rates, 6% adjusted a current arrangement and 68% created a new payment plan.

REPORT NO: GE491 (cont.)

4.6.4 93% of all arrangements end before our recommended end date of 31 May 2021.

4.7 Table 2 and 3 below show that the top five suburbs with active COVID-19 payment arrangements were Craigieburn (400), Mickleham (171), Sunbury (171), Greenvale (148) and Roxburgh Park (123). Total active COVID-19 arrangements are 1,396.

Table 2 – COVID-19 Payment Arrangements by Suburb				
Suburb	Number of rateable assessments	Number of payment arrangements	Total arrears \$	Average payment arrangement \$
Attwood	1,127	21	40,724	1,939
Broadmeadows	5,038	53	75,773	1,430
Bulla	297	1	4,966	4,966
Campbellfield	4,611	30	134,518	4,484
Clarkefield	15	-	-	-
Coolaroo	1,395	11	44,298	4,027
Craigieburn	21,695	400	758,541	1,896
Dallas	2,305	23	28,690	1,247
Diggers Rest	180	2	30,620	15,310
Fawkner	127	1	185	185
Gladstone Park	3,396	29	42,496	1,465
Greenvale	7,127	148	261,172	1,765
Jacana	889	13	18,937	1,457
Kalkallo	1,884	54	50,483	935
Meadow Heights	4,783	82	136,327	1,663
Melbourne Airport	3	-	-	-
Mickleham	6,214	171	219,607	1,284
Oaklands Junction	152	2	4,683	2,342
Roxburgh Park	7,065	123	208,489	1,695
Somerton	510	1	3,814	3,814
Sunbury	16,313	171	260,272	1,522
Tullamarine	4,159	31	64,708	2,087
Westmeadows	2,847	26	48,530	1,867
Wildwood	105	2	5,603	2,801
Yuroke	64	1	2,000	2,000
Total	92,334	1,396	2,445,436	1,751

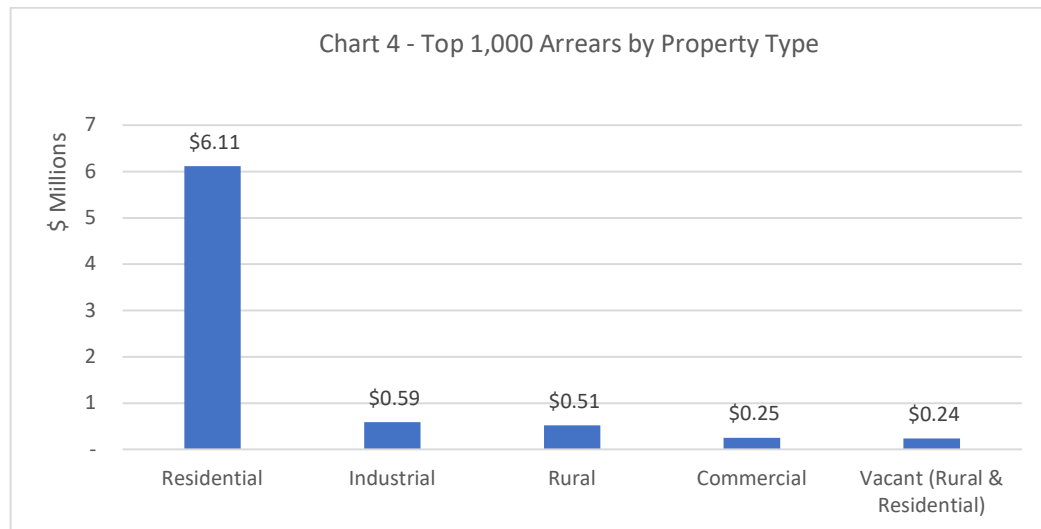
REPORT NO: GE491 (cont.)

Table 3 – COVID-19 payment arrangements by property type			
Suburb	Number of payment arrangements	Total arrears \$	Average payment arrangement \$
Residential	1,268	1,748,889	1,379
Vacant land	66	64,584	979
Industrial	27	190,952	7,072
Commercial	14	281,294	20,092
Rural house and land	21	159,717	7,606
Total	1,396	2,445,436	1,751

- 4.8 Table 4 and Chart 4 – analysis of the largest 1,000 debts as at 31 December 2020. The largest 1,000 debts represent approximately 30% of the total debt and 1% of all rateable properties.

Table 4 – Analysis of largest 1,000 Debts				
Property Type	Average Capital Improved Value \$	No of Assessments	Average Outstanding \$	Total Outstanding \$
Residential house and land	548,609	865	7,068	6,114,195
Industrial	2,161,214	56	10,510	588,599
Rural house and land	3,430,568	37	13,895	514,118
Commercial	4,642,000	15	16,904	253,553
Vacant rural and residential	2,599,815	27	8,867	239,422
Total		1,000	*7,710	7,709,887

**this result is not a sum of the values directly above it but rather a simple average of the total outstanding divided by the number of assessments.*



- 4.9 Previous rate arrears reports have recommended 371 properties to be considered for a rate recovery sale. Of these, 306 have since paid in full, 65 properties (\$896,963.39) remain on payment arrangements or have further legal action on hold.

REPORT NO: GE491 (cont.)

4.10 During the current pandemic no further action will be taken on those properties previously recommended for sale.

5. ENVIRONMENTAL SUSTAINABILITY CONSIDERATIONS:

There are no environmental sustainability implications in respect to this report.

6. CLIMATE CHANGE ADAPTATION CONSIDERATIONS:

There are no climate change adaptation implications in respect to this report.

7. CHARTER OF HUMAN RIGHTS APPLICATION:

The Charter of Human Rights and Responsibility has been considered and the recommendations of this report give no rise to any matters.

8. COMMUNITY CONSULTATION:

Not applicable.

9. DISCUSSION:

9.1 Reminder final notices and in some cases debt collector letters have been sent to a number of debtors across all categories alerting owners of Council's intention to collect all outstanding balances. These notices also indicate that failure to pay the outstanding balance may result in the commencement of legal proceedings.

9.2 In addition, letters are sent to debtors whose rate arrears are approaching three years alerting them to Council's intention to collect these amounts. This letter also explains to the ratepayer that once rates become outstanding for three years, Council is within its right to sell their property in accordance with Section 181 of the Act.

9.3 Interest is currently not being charged on outstanding debts until after the 31st of March 2021.

9.4 There are currently 1,069 (\$4.51m in arrears) properties with Council's debt collection agency which are at different stages of the debt collection process as follows. Action on these properties is also currently on hold during the Pandemic.

Debt Collection Process – Stage	Number of Properties
Demand Letter	622
Field Call	0
Summons Issued or Served	267
Judgment Entered or Obtained	85
Warrant Issued or Executed	84
Summons for Oral Examination Issued or Complete	11

10. CONCLUSION:

10.1 Effective March 2020 Council placed debt collection activities on hold and will not proceed with further collection proceedings until after 31 March 2021. However, ratepayers are encouraged to enter into payment plans or apply for short term deferments of their rates if they are impacted by COVID-19 or are experiencing financial difficulty.

10.2 During the current pandemic the use of electronic communication utilising Webforms have proven pivotal in managing the increased short-term hardship and arrangement applications.

10.3 COVID-19 has had a significant impact on Council's ability to collect overdue rates, however by continuing to stay engaged with the community and the initiatives that Council will be undertaking we will continue to ensure that our rate payers are supported during this difficult time.

Debt Collection Process	
Action	Description
Reminder Letter	Issued by the Rates Department and is the first stage of the debt collection process.
Final Demand	Issued by the Rates Department if there is no response to the reminder letter.
Demand Letter	Issued by Australian Receivables Limited (ARL) or Maddocks Lawyers Collections (ML&C) (Debt Collection Agencies) if there is no response to the final demand.
Summons Issued	Complaint is sent to court for issuing
Field Call	A representative of ARL / MLC visits the property outside of office hours in an attempt to make contact with the debtor and make them aware of the situation and request payment or contact with Council.
Rental Demand	If payment or contact has not been made and the property is tenanted, Council may issue a rental demand.
Summons Served	Complaint is served on owners of the property.
Judgment Entered	If payment or contact has not been made, judgement is entered. This will now affect the credit rating of the owner.
Warrant Issued	This is a warrant to seize property. If all above steps have been taken, and no contact or payments have been made, a Sheriff will attend the owner's address and request payment. If the defendant refuses to pay or enter into an arrangement to pay, the Sheriff has the authority to seize certain goods to the value of the demand.
Arrangement	At any of these stages, the ratepayer can ring and organise to pay outstanding Council rates in weekly, fortnightly or monthly instalments. If this arrangement is maintained, further action will not be taken.
Letter to Mortgage	Where a bank has a mortgagee over the property, a letter is sent requesting payment or assistance in settling the debt.
Summons for Oral Examination	Requires a person to attend Court and answer questions about income, assets and debts under oath. An instalment order can be made by the Court to clear the debt.
Section 181 Demand	This section provides for Council with the ability to sell a property at public auction for recovery of rates and charges, certain legal processes must be actioned including but not limited to at least three years unpaid rates, unsuccessful legal action being taken and no current repayment arrangement in place.

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REPORT NO:	GE492
REPORT TITLE:	Quarterly Budget Report - December 2020
SOURCE:	Fadi Srouf, Chief Financial Officer
DIVISION:	Corporate Services
FILE NO:	HCC19/898
POLICY:	-
STRATEGIC OBJECTIVE:	5.3 Provide responsible and transparent governance, services and infrastructure which responds to and supports community needs.
ATTACHMENT:	1. <i>Quarterly Budget Report</i>

1. SUMMARY OF REPORT:

The quarterly budget report provides information to the community and stakeholders on the financial performance and position of Council as at 31 December 2020, as required on a quarterly basis under section 97 – Quarterly budget report of the *Local Government Act*, 2020.

2. RECOMMENDATION:

That the quarterly budget report for the six months ended 31 December 2020 be received and noted.

3. FINANCIAL IMPLICATIONS:

3.1 The financial statements consist of three main reports:

1. the Income Statement including explanation of material variances
2. the Balance Sheet
3. the Statement of Cash Flows

3.2 Two additional reports have also been included that show the level of payments that Council makes directly to businesses, community groups, individuals and employees within Hume.

3.3 From a governance and accountability perspective, the quarterly budget reports include key financial data with commentary on variance analysis. The reporting of this key financial data on a quarterly and annual basis adds to the commitment Council has made to govern in an open manner and be accountable to residents for the management of resources and funding

4. DISCUSSION:

Key Financial Information

4.1 Income Statement [Attachment 1]

4.1.1 The Income Statement measures how well Council has performed from an operating or recurrent nature. It reports revenue and expenditure from the activities and functions undertaken with the net effect being the resulting surplus figure.

4.1.2 Attachment 1 identifies that Council has generated \$196m in revenue and \$136.5m in expenses. This has resulted in a surplus of \$59.5m which is \$3.9m above budget for the six months ended 31 December 2020. This favourable variance is largely due to staff vacancies in Council which has been partly offset by an increase in agency staff costs.

REPORT NO: GE492 (cont.)

4.1.3 Council's Revenue Base

- (a) The majority of Council's revenue is derived from rates and charges. During the financial year ended 30 June 2020, rates income was \$194m. This equated to 41.1% of Council's total revenue of \$471.9m.
- (b) For the six months ended 31 December 2020, rates revenue was \$100.6m which equates to 51.3% of total revenue. Therefore, Council continues to be reliant on its rates revenue as a major source of income.

4.1.4 For the six months ended 31 December 2020, the major items of revenue earned by Council include:

- | | |
|----------------------------------|-----------|
| (a) Rates and charges | \$ 100.6m |
| (b) Contributions - non-monetary | \$ 42.6m |
| (c) Grants – operating | \$ 24.9m |
| (d) Contributions - monetary | \$ 6.5m |
| (e) User fees | \$ 5.6m |
| (f) Statutory fees | \$ 5.3m |

4.1.5 Council's Expense Base

- (a) The majority of Council's expenses relates to employee benefits. During the financial year ended 30 June 2020, employee benefits were \$114.5m. This equated to 41.3% of Council's total expenses of \$277.2m.
- (b) For the six months ended 31 December 2020, employee benefits were \$58.9m which equates to 43.1% of total expenditure.

4.1.6 For the six months ended 31 December 2020, the major items of expenditure incurred by Council include:

- | | |
|-----------------------------------|----------|
| (a) Employee costs | \$ 58.9m |
| (b) Materials and services | \$ 40.0m |
| (c) Depreciation and amortisation | \$ 27.4m |

4.2 Balance Sheet *[Attachment 2]*

4.2.1 The Balance Sheet is a statement at a point in time which shows all the resources controlled by Council and the obligations of Council. The aim of the Balance Sheet is to summarise the information contained in the accounting records relating to assets, liabilities and equity in a clear and intelligible form.

4.2.2 The major item on the Balance Sheet consists of property, infrastructure, plant and equipment. These fixed assets made up 90% of Council's total asset base in 2019/20 (\$3.86b of total assets of \$4.3b). As at 31 December 2020, fixed assets made up 89.9% of Council's total asset base (\$3.9b of total assets of \$4.34b).

4.2.3 The impact of sound financial management can be seen in the ratepayer equity of \$4.24b which reflects the strong financial position of Council. The information contained within the Balance Sheet also demonstrates that liquidity is strong as demonstrated by the favourable cash balance. Council's assets are increasing, which is largely due to developer contributed assets and a substantial capital works program. All these factors have led to favourable key ratios as identified in this report.

REPORT NO: GE492 (cont.)

4.3 Statement of Cash Flows [Attachment 3]

- 4.3.1 The Statement of Cash Flows shows what was actually received and paid by Council, not what was owed or what was recorded. This is largely why it is different to the Income Statement which shows what income was raised and payments incurred during the same period.
- 4.3.2 For example, Council may make a purchase of some goods/services today but may not make payment for those goods/services for another 30 days (in accordance with Council's credit terms). However, as the goods/services have already been provided, the accounting standards require that the cost of these goods/services be recorded in the Income Statement as soon as they have been provided.
- 4.3.3 Another reason for the difference between the surplus figure reported in the Income Statement and the Cash Flow Statement is the Depreciation and Amortisation recorded as expense, \$27.4m at 31 December 2020, (in the Income Statement) with no resulting cash payments.
- 4.3.4 For the six months ended 31 December 2020, Council's cash position was \$340.1m which is \$75m favourable to budget. This is largely due to commencing the year with a higher than expected cash balance primarily as a result of a larger than expected carried forward in the capital works program, less capital works expenditure in the first half year impacted by COVID-19 stage 4 restrictions and a favourable operating result.

4.4 Buying Local [Attachment 4]

- 4.4.1 The Buying Local report highlights the level of payments made by Council to businesses, community groups and individuals within the municipality. The report includes payments for grants and contributions, materials and services, building and utility costs and contractor and other services. For the six months ended 31 December 2020, Council made payments to local suppliers totalling \$8.7m. Significantly, the level of local expenditure as a proportion of Council's total expenditure (including capital works) was 13.7% as at 31 December 2020.
- 4.4.2 It should be noted that the report only includes payments to suppliers whose mailing address is listed within Hume. Therefore, there is the possibility that the level of payments made to local suppliers is in fact, higher.

4.5 Employees Residing within Hume [Attachment 5]

- 4.5.1 The Employees Residing within Hume report highlights the level of salaries paid to employees who reside within Hume as well as the number of employees who reside within Hume.
- 4.5.2 For the six months ended 31 December 2020, Council paid salaries to employees residing within Hume totalling \$23.6m, representing 40% of total employee benefits. In addition, for the six months ended 31 December 2020, there were 953 employees residing within Hume representing 58.4% of total employees.

4.6 Financial Ratios

The following financial ratios are required to be included in Council's financial report at year-end. Although their value may be limited as key financial indicators, they do provide information on trends.

REPORT NO: GE492 (cont.)

Ratios	Six months to 31- Dec-20 2020/21	Six months to 31-Dec-19 2019/20	Twelve months to 30-Jun-20 2019/20
<u>Debt Servicing Ratio (Target < 3%)</u> This ratio measures the extent to which long-term debt is impacting on the annual total income of Council and identifies the capacity of Council to service outstanding debt. The ratio expresses the amount of interest paid as a percentage of Council's total revenue. (The lower the ratio the better).	0.00%	0.01%	0.00%
<u>Debt Commitment Ratio (Target < 15%)</u> This ratio identifies Council's debt redemption strategy and expresses the percentage of rate revenue utilised to pay interest and redeem debt principal. (The lower the ratio the better).	0.16%	0.17%	0.18%
<u>Revenue Ratio (Target 65% - 70%)</u> This ratio identifies Council's reliance on rates as a source of income. (The lower the ratio the better).	51.30%	49.39%	41.11%
<u>Debt Exposure Ratio</u> This ratio identifies Council's exposure to debt and expresses the total indebtedness to total realisable assets. (The lower the ratio the better).	5.66%	3.14%	6.41%
<u>Working Capital Ratio (Target 100% - 150%)</u> This ratio identifies Council's ability to meet current liabilities and enables an assessment of Council's liquidity and solvency. The ratio compares the current assets to current liabilities. (The higher the ratio the better).	620.72%	946.61%	496.02%

4.7 Revised budget statement

- 4.7.1 As required under section 97(3) – Quarterly budget report of the Local Government Act, 2020, the Chief Executive Officer is required to include a statement as to whether a revised budget is, or may be, required.
- 4.7.2 As at 31 December 2020 the Chief Executive Officer is of the opinion that no revision to the budget is required. Council will continue to monitor revenue and expenditure to determine if a revision is required.

5. CONCLUSION:

The quarterly budget report has been prepared on an accrual basis and in accordance with accounting practices, including an Income Statement, Balance Sheet and Statement of Cash Flows. Council's financial performance is ahead of expectations.

Attachment 1

INCOME STATEMENT

For the six months ended 31 December 2020

		Six months to 31/12/2020 ACTUAL 2020/21 \$'000	Six months to 31/12/2020 BUDGET 2020/21 \$'000	Six months to 31/12/2020 VARIANCE 2020/21 \$'000	Six months to 31/12/2020 VARIANCE 2020/21 %	Twelve months to 30/06/2020 ACTUAL 2019/20 \$'000
	Ref					
Income						
Rates and charges	1	100,557	98,515	2,042	2%	194,002
Statutory fees and fines	2	5,271	6,442	(1,171)	(18%)	14,285
User fees	3	5,628	8,501	(2,873)	(34%)	23,036
Grants - operating	4	24,941	23,049	1,892	8%	48,605
Property rental		1,191	1,103	88	8%	1,963
Interest and other income		850	893	(43)	(5%)	5,235
Total income		138,438	138,503	(65)	(0%)	287,126
Expenses						
Employee costs	5	58,865	67,365	8,500	13%	114,495
Materials and services	6	39,995	43,701	3,706	8%	81,877
Utility costs	7	2,706	3,287	581	18%	6,490
Bad and doubtful debts	8	212	-	(212)	(100%)	637
Depreciation and amortisation	9	27,410	26,193	(1,217)	(5%)	51,298
Finance costs	10	22	84	62	74%	821
Other expenses		2,340	2,628	288	11%	7,358
Total expenses		131,550	143,258	11,708	8%	262,976
Underlying surplus		6,888	(4,755)	11,643	(245%)	24,150
Less						
Capital works expensed		-	-	-	-	3,605
Assets renewed	11	1,339	-	(1,339)	(100%)	6,903
Loss on disposal of financial assets		-	-	-	-	310
Reimbursements to developers for WIK/LIK		3,584	3,676	92	3%	3,401
Add						
Net gain on disposal of property, plant, equipment and infrastructure	12	4,542	2,360	2,182	92%	56
Net gain on property development		-	-	-	-	26
Fair value adjustments for investment property		-	-	-	-	2,971
Contributions - non-monetary	13	42,624	52,284	(9,660)	(18%)	162,141
Contributions - monetary	14	6,517	5,711	806	14%	15,494
Grants - capital		3,882	3,685	197	5%	4,080
Surplus for the year		59,530	55,609	3,921	7%	194,698
Other comprehensive income						
Net asset revaluation increment		111	120	(9)	(8%)	200,272
Comprehensive result		59,641	55,729	3,912	7%	394,971

Attachment 1 (cont.)

INCOME STATEMENT

For the six months ended 31 December 2020

Explanation of material variations (YTD Actual versus YTD Budget)

Item	Ref	Explanation
Rates and charges	1	\$2m favourable to budget predominantly due to processing 5,955 new supplementary rates notices.
Statutory fees and fines	2	\$1.2m unfavourable to budget predominantly due to the contraction of activities due to the COVID-19 pandemic; including subdivisions, \$492k and local laws, \$799k.
User fees	3	\$2.9m unfavourable to budget predominantly due to the contraction of activities due to the COVID-19 pandemic; including Leisure Services, \$1.38m. Family & Children Services income was also \$1.37m unfavourable mainly due to not raising invoices for term 3 and 4 fees as this was funded by the Department of Education and Training (DET), lost income in the gap fees in Long Day Care (LDC) due to stage 4 restrictions and residual low utilisation resulting from COVID-19.
Grants - operating	4	\$1.9m favourable to budget predominantly due to the additional funding received for COVID-19 in LDC, \$679k and Home care packages, \$485k, unbudgeted grant income received for outdoor dining project, \$500k and 'Culturally and Linguistically Diverse' (CALD) Communities Taskforce, \$330k.
Employee costs	5	\$8.5m favourable to budget predominantly due to staff vacancies (112 EFT) and the COVID-19 impact on the forced shutdown of Council non-essential services. This is partially offset by an unfavourable variance in agency staff of \$1.8m.
Materials and services	6	\$3.7m favourable to budget predominantly due to reduction in contractor expense due to COVID-19 including Parks, \$2.2m, Building Maintenance, \$427K and Local Laws \$411k.
Utility costs	7	\$0.6m favourable to budget predominantly due to not operating Council services and facilities at full capacity as the COVID-19 pandemic caused the temporary shutdown of some services.
Bad and doubtful debts	8	\$0.2m unfavourable to budget including Statutory Planning and Building infringements \$80k and Local Laws \$75k as Council does not budget for bad debts.
Depreciation and amortisation	9	\$1.2m unfavourable to budget predominantly due to the increase in the value of infrastructure assets from the 2019/20 revaluation resulting in higher depreciation in 2020/21.
Finance costs	10	\$62k favourable to budget due to the deferral of photocopier lease renewal contracts.
Assets renewed	11	\$1.3m unfavourable to budget as Council does not budget for assets disposed of through the capital works renewal program.
Net gain on disposal of property, plant, equipment and infrastructure	12	\$2.2m favourable to budget predominantly due to the disposal of a major road reserve along Craigieburn Rd to VicRoads as part of the Craigieburn Rd duplication.
Contributions - non-monetary	13	\$9.7m unfavourable to budget predominantly due to the impact of COVID-19 on subdivision activities.
Contributions - monetary	14	\$0.8m favourable to budget predominantly due to unbudgeted developer cash contributions as a result of a delay in finalising 'works-in-kind' agreements in Greenvale Central North and Lockerbie PSP precincts.

Attachment 2
BALANCE SHEET

As at 31 December 2020

	Six months to 31/12/2020 ACTUAL 2020/21 \$'000	Six months to 31/12/2019 ACTUAL 2019/20 \$'000	Twelve months to 30/06/2020 ACTUAL 2019/20 \$'000
Assets			
Current assets			
Cash and cash equivalents	194,772	70,800	215,465
Other financial assets	145,278	225,369	110,478
Trade and other receivables	32,811	26,128	33,314
Non-current assets classified as held for sale	890	-	2,778
Other assets	137	281	1,602
Total current assets	373,888	322,578	363,637
Non-current assets			
Trade and other receivables	81	81	81
Property, plant, equipment and infrastructure	3,903,187	3,545,951	3,864,846
Right-of-use assets	633	966	770
Investment property	45,559	45,566	46,622
Intangible assets	16,640	18,164	16,640
Equity Investment	-	310	-
Total non-current assets	3,966,100	3,611,038	3,928,959
Total assets	4,339,988	3,933,616	4,292,596
Liabilities			
Current liabilities			
Trade and other payables	12,191	1,817	37,427
Trust funds and deposits	13,049	1,365	3,060
Provisions	34,667	30,728	32,504
Lease liabilities	328	167	320
Total current liabilities	60,235	34,077	73,311
Non-current liabilities			
Trust funds and deposits	10,676	9,699	9,781
Provisions	32,073	32,040	32,000
Lease liabilities	320	795	464
Total non-current liabilities	43,069	42,534	42,245
Total liabilities	103,304	76,612	115,556
Net assets	4,236,684	3,857,005	4,177,040
Equity			
Accumulated surplus	2,125,376	1,952,952	2,055,288
Asset revaluation reserve	1,965,584	1,765,201	1,965,473
Other reserves	145,724	138,852	156,279
Total equity	4,236,684	3,857,005	4,177,040

Attachment 3

STATEMENT OF CASH FLOWS

For the six months ended 31 December 2020

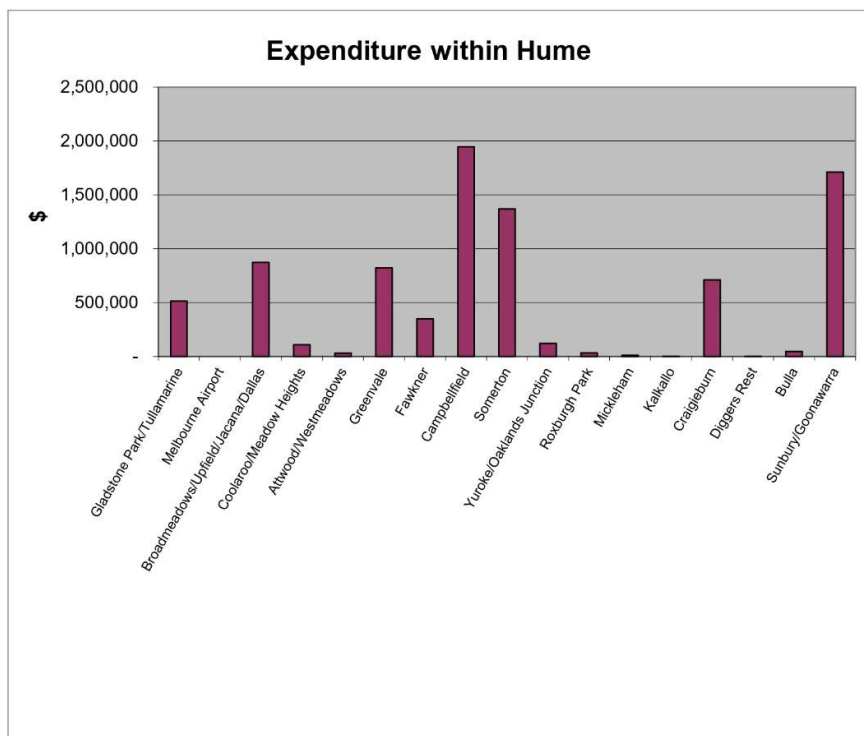
	Six months to 31/12/2020 ACTUAL 2020/21 \$'000	Six months to 31/12/2020 BUDGET 2020/21 \$'000	Twelve months to 30/06/2020 ACTUAL 2019/20 \$'000
Cash flows from operating activities			
Rates and charges	99,351	97,308	191,782
Statutory fees and fines	5,119	6,442	13,680
User fees	10,212	13,166	22,309
Grants - operating	24,941	23,049	49,634
Grants - capital	3,997	3,799	8,864
Contributions - monetary	3,707	5,711	14,545
Interest received	967	1,040	6,101
Other income	1,092	1,400	2,343
Net trust movement	10,885	-	3,217
Net GST refund	(23)	-	14,530
Employee costs	(60,879)	(69,431)	(112,751)
Materials and services	(53,260)	(57,423)	(105,619)
Short-term, low value and variable lease payment	(305)	(403)	(476)
Other payments	(9,808)	(10,201)	(10,257)
Net cash provided by operating activities	35,995	14,458	97,901
Cash flows from investing activities			
Payments for property, plant, equipment	(28,424)	(61,580)	(68,045)
Proceeds from sales of property, plant,	6,697	6,455	647
Proceeds from property development	-	-	24
Net cash provided by / (used in) investing activities	(21,727)	(55,125)	(67,374)
Cash flows from financing activities			
Interest paid	(9)	(15)	(39)
Repayment of lease liabilities	(152)	(179)	(303)
Net cash used in financing activities			
Net increase/(decrease) in cash and cash equivalents	(161)	(194)	(342)
Cash and cash equivalents at the beginning of the financial year	325,943	305,930	295,758
Cash and cash equivalents at the end	340,050	265,069	325,943

Attachment 4

**Buying Local
Expenditure by Locality**

As at 31 December 2020

Postcode	Suburb	Amount \$
3043	Gladstone Park/Tullamarine	514,152
3045	Melbourne Airport	-
3047	Broadmeadows/Upfield/Jacana/Dallas	873,199
3048	Coolaroo/Meadow Heights	110,605
3049	Attwood/Westmeadows	31,522
3059	Greenvale	823,803
3060	Fawkner	349,498
3061	Campbellfield	1,947,336
3062	Somerton	1,370,107
3063	Yuroke/Oaklands Junction	121,665
3064	Roxburgh Park	32,881
3064	Mickleham	12,031
3064	Kalkallo	449
3064	Craigieburn	710,630
3427	Diggers Rest	288
3428	Bulla	47,621
3429	Sunbury/Goonawarra	1,712,783
Total		8,658,570



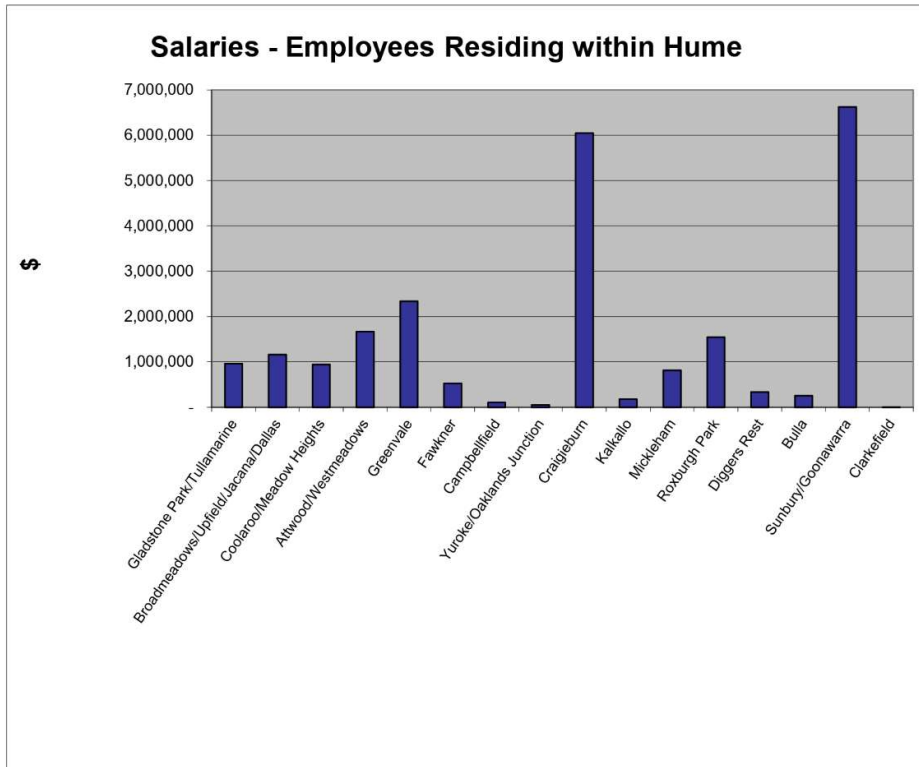
Attachment 5

Employees Residing within Hume

As at 31 December 2020

Postcode	Suburb	Gross Salaries \$
3043	Gladstone Park/Tullamarine	965,377
3047	Broadmeadows/Upfield/Jacana/Dallas	1,163,891
3048	Coolaroo/Meadow Heights	945,227
3049	Attwood/Westmeadows	1,665,645
3059	Greenvale	2,340,951
3060	Fawkner	527,186
3061	Campbellfield	107,546
3063	Yuroke/Oaklands Junction	52,136
3064	Craigieburn	6,050,748
3064	Kalkallo	179,708
3064	Mickleham	813,722
3064	Roxburgh Park	1,544,758
3427	Diggers Rest	336,030
3428	Bulla	253,701
3429	Sunbury/Goonawarra	6,625,864
3430	Clarkefield	1,968
Total		23,574,457

Total number of employees paid that reside within Hume was 953.



REPORT NO:	GE493
REPORT TITLE:	Approved Contracts Report 1 October 2020 - 31 December 2020 and Chief Executive Officer's use of temporary increase in delegation - 22 December 2020 to 1 February 2021
SOURCE:	Fadi Srour, Chief Financial Officer
DIVISION:	Corporate Services
FILE NO:	HCC04/13
POLICY:	Procurement Policy
STRATEGIC OBJECTIVE:	5.3 Provide responsible and transparent governance, services and infrastructure which responds to and supports community needs.
ATTACHMENTS:	Nil

1. SUMMARY OF REPORT:

- 1.1 This report provides Council with a summary of contracts approved by Council and contracts and extensions approved by the Chief Executive Officer (CEO) and Directors under delegated authority for the period 1 October 2020 – 31 December 2020.
- 1.2 The report also provides details of contracts approved under the temporary increase in the limitation on the power delegated to the CEO to award a contract not exceeding the value of \$2.0 million (including GST) for the period 22 December 2020 to 1 February 2021.

2. RECOMMENDATION:

That the report be noted.

3. LEGISLATIVE POWERS:

- 3.1 Legislative Powers to enter into contracts are contained in section 186 of the *Local Government Act* 1989.
- 3.2 *Local Government Act* 2020 – Section 11 – Council is given the power to delegate any of its powers, duties and functions, other than for stipulated exceptions, under the Act as specified in section 9 of this report.

4. FINANCIAL IMPLICATIONS:

All approved contracts listed in this report have been provided for in Council's Annual Budget and for multi-year contracts in the Strategic Resource Plan.

5. ENVIRONMENTAL SUSTAINABILITY CONSIDERATIONS:

There are no environmental sustainability implications in respect to this report.

6. CLIMATE CHANGE ADAPTATION CONSIDERATIONS:

There are no climate change adaptation considerations in this report.

7. CHARTER OF HUMAN RIGHTS APPLICATION:

There is no charter of human rights application in respect to this report.

8. COMMUNITY CONSULTATION:

Not applicable.

REPORT NO: GE493 (cont.)

9. DISCUSSION:

9.1 Contracts approved under delegated authority by the Chief Executive Officer

9.1.1 Council has, via an Instrument of Delegation, delegated to the CEO, the power to enter into contracts up to a value of \$500,000.

9.1.2 For the period 1 October 2020 – 21 December 2020, the CEO approved the following contracts up to the value of \$500,000.

Contract No.	Description	Awarded Supplier	Date Approved
30 20 3155	Elizabeth Drive, Sunbury (between Budburst Drive & Backhaus Avenue) - Service Road Construction	Viscaria Pty Ltd	8/10/2020
30 20 3115	Design and Construct - Pedestrian Bridge over Moonee Ponds Creek, Pascoe Street, Westmeadows	GR Design and Construct Pty Ltd	21/10/2020
30 20 3150	Condition Audit of Buildings	Macutex Pty Ltd	4/11/2020
30 20 3175	Microsoft Arrangement (MAV NPN 2.17-3)	Crayon Australia Pty Ltd	5/11/2020
30 20 3165	Design Documentation, Contract Administration and Project Management of Leo Dineen Pavilion in Tullamarine	Dav Arc Pty Ltd	12/11/2020
30 20 3124	Design and Project Support - Spillway at Spavin Drive Lake, Sunbury	Engeny Water Management Pty Ltd	23/11/2020
30 20 3108	Progress Reserve, Coolaroo - Detail Design	Hansen Partnership Pty Ltd	26/11/2020
30 20 3161	Jensen Road, Broadmeadows - Road Reconstruction	MJ Construction Group Pty Ltd	26/11/2020
30 20 3140	Provision of Grass Cutting/Slashing and Rubbish Removal Services	Urban Maintenance Systems Pty Ltd	7/12/2020
30 20 3140	Provision of Grass Cutting/Slashing and Rubbish Removal Services	Traianon Contractors Pty Ltd	7/12/2020
30 20 3140	Provision of Grass Cutting/Slashing and Rubbish Removal Services	Rivergum Parks & Gardens	7/12/2020
30 20 3140	Provision of Grass Cutting/Slashing and Rubbish Removal Services	Sanpoint Pty Ltd t/as JD Total	7/12/2020
30 20 3140	Provision of Grass Cutting/Slashing and Rubbish Removal Services	JMT Enviro Pty Ltd	7/12/2020
30 20 3140	Provision of Grass Cutting/Slashing and Rubbish Removal Services	Hume Turf and Machinery	7/12/2020
30 20 3141	Craigieburn Bowling Green Reconstruction	Agturf Pty Ltd	7/12/2020
30 20 3159	Calivil Street Dallas - Road Reconstruction	Patten Pavement Services Pty Ltd	17/12/2020

9.1.3 The following contract was approved by the CEO under the temporary increase in his delegation amended by Council at its meeting of 21 September 2020.

REPORT NO: GE493 (cont.)

Contract No.	Description	Awarded Supplier	Date Approved
30 20 3129	Bamburgh Street Jacana – Road Reconstruction	Presta & Sons Pty Ltd	7/12/2020

- 9.1.4 Council, at its meeting of 21 December 2020, resolved to amend its instrument of delegation to the CEO for the period 22 December 2020 to 1 February 2021 for a temporary increase in the limitation of the power delegated to the CEO to award a contract not exceeding the value of \$2.0 million (including GST).

- 9.1.5 For the period 22 December 2020 to 1 February 2021, the CEO approved the following contracts under the temporary increase to his delegation.

Contract No.	Description	Awarded Supplier	Date Approved
30 20 3174	Roxburgh Park Recreation Centre Upgrade	Harris HMC Interiors Pty Ltd	24/12/2020
30 20 3131	Cavendish Street Broadmeadows – Drainage Upgrade	Presta Construction Group Pty Ltd	27/01/2021

9.2 Contracts approved under delegated authority by Directors

- 9.2.1 Council's Procurement Policy provides for Directors to undertake procurement up to the value of \$100,000.

- 9.2.2 For the period 1 October 2020 – 31 December 2020, Directors approved the following contracts up to the value of \$100,000.

Contract No.	Description	Awarded Supplier	Date Approved
30 20 3153	Supply and Installation of Office Desks Broadmeadows Office	Advanced Print Scan Solutions Pty Ltd	19/11/2020
30 20 3160	Design of Mickleham North Active Open Space (Merrifield AOS) Pavilion	Davidson Architecture	4/12/2020
30 20 3146	Design Services - Gladstone Park Tennis Pavilion Refurbishment	Davidson Architecture	14/12/2020

9.3 Contract extensions approved under delegated authority by Directors

- 9.3.1 For the period 1 October 2020 – 31 December 2020, the following contract extensions were approved by Directors for contracts approved by Council which had the approval of contract extensions delegated to Directors for review and approval.

Contract No.	Description	Awarded Supplier	Date Extension Approved
30 15 2518	Provision of Cleaning Services for Public Buildings	Makkim Pty Ltd	14/10/2020
30 19 3022	Provision of Mulching Services	Hume Contracting Pty Ltd	20/10/2020
30 18 2850	Provision of Painting Services	Petrolo Asset Maintenance Pty Ltd	26/10/2020
30 15 2559	Supply and Delivery of Educational Materials, Room Furniture, Equipment and Resources	Modern Teaching Aids Pty Ltd	4/11/2020
30 15 2559	Supply and Delivery of Educational Materials, Room Furniture, Equipment and Resources	Bellbird Kid-Z Educational Resources	4/11/2020

REPORT NO: GE493 (cont.)

Contract No.	Description	Awarded Supplier	Date Extension Approved
30 15 2559	Supply and Delivery of Educational Materials, Room Furniture, Equipment and Resources	Arrow Group Australia Pty Ltd t/as STE Products	4/11/2020
30 17 2768	Supply of Trucks - MAV NPN04-13	Daimler Trust and Bus Australia Pacific Pty Ltd	4/11/2020
30 17 2768	Supply of Trucks - MAV NPN04-13	Fuso Truck and Bus	4/11/2020
30 17 2768	Supply of Trucks - MAV NPN04-13	Hino Motor Sales Australia Pty Ltd	4/11/2020
30 17 2768	Supply of Trucks - MAV NPN04-13	Isuzu Australia Ltd	4/11/2020
30 17 2768	Supply of Trucks - MAV NPN04-13	IVECO Trucks Australia	4/11/2020
30 17 2768	Supply of Trucks - MAV NPN04-13	Mack Trucks	4/11/2020
30 17 2768	Supply of Trucks - MAV NPN04-13	MAN Automotive imports Pty Ltd	4/11/2020
30 17 2768	Supply of Trucks - MAV NPN04-13	Mercedes-Benz Trucks	4/11/2020
30 17 2768	Supply of Trucks - MAV NPN04-13	PACCAR DAF	4/11/2020
30 17 2768	Supply of Trucks - MAV NPN04-13	PACCAR Kenworth	4/11/2020
30 17 2768	Supply of Trucks - MAV NPN04-13	Penske Commercial Vehicles-Dennis Eagle Australia	4/11/2020
30 17 2768	Supply of Trucks - MAV NPN04-13	Scania Australia	4/11/2020
30 17 2768	Supply of Trucks - MAV NPN04-13	UD TRUCKS a division of Volvo Group Australia	4/11/2020
30 17 2768	Supply of Trucks - MAV NPN04-13	Volvo Trucks	4/11/2020
30 17 2768	Supply of Trucks - MAV NPN04-13	Western Star Trucks Australia	4/11/2020
30 18 2850	Provision of Painting Services	Hue Painting & Decorating Pty Ltd	18/11/2020
30 18 2850	Provision of Painting Services	N & R McCall Painter and Decorator	18/11/2020
30 18 2850	Provision of Painting Services	MAS Property Services	18/11/2020
30 18 2879	Provision of Valuation Services	Valuelt Pty Ltd	24/11/2020
30 18 2851	Provision of Crack Sealing Services	Supersealing	24/11/2020
30 17 2752	Provision of Pavement and Line Marking Services	Roadmaster Line Marking Pty Ltd	8/12/2020
30 15 2555	Supply and Spreading of Playground Mulch for Hume City Council	Eco Dynamics Services	23/12/2020

9.4 Contracts approved by Council

Council approved the following contracts over \$500,000 for the period 1 October 2020 – 31 December 2020.

Contract No.	Description	Awarded Supplier	Date Approved
30 20 3122	Provision of Delivered Meals	RFK Pty Ltd t/as Community Chef	21/12/2020

REPORT NO: GE493 (cont.)

Contract No.	Description	Awarded Supplier	Date Approved
30 20 3151	Mount Aitken Recreation Reserve - Civil and Landscape Works	Evergreen Civil Pty Ltd	21/12/2020
30 20 3171	Design and Construction of Gladstone Park Reserve Soccer Pavilion	Modular Spaces Pty Ltd	21/12/2020

10. CONCLUSION:

This report summarises contracts and extensions approved either by Council or under delegation by the CEO or a Director for the period 1 October 2020 to 31 December 2020 and the immediate reporting of the use of the temporary increased delegated provided by Council to the CEO. The report ensures transparency of reporting in line with Council's commitment to open and transparent governance.

REPORT NO: GE493 (cont.)

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